



**City of Phoenix**  
PLANNING AND DEVELOPMENT DEPARTMENT

**Staff Report Z-55-25-8**  
**July 30, 2025**

**Central City [Village Planning Committee](#)** August 11, 2025  
**Meeting Date:**  
**[Planning Commission](#) Hearing Date:** September 4, 2025  
**Request From:** [R-3 RSIOD AIOD](#) (Multifamily Residence District, Rio Salado Interim Overlay District, Airport Noise Impact Overlay District) (0.57 acres)  
**Request To:** [A-2 RSIOD AIOD](#) (Industrial District, Rio Salado Interim Overlay District, Airport Noise Impact Overlay District) (0.57 acres)  
**Proposed Use:** Outdoor Storage  
**Location:** Northwest corner of 14th Street and University Drive  
**Owner:** PCI, LLC  
**Applicant:** Kurt Waldier, Gilbert Blilie, PLLC  
**Representative:** Paul Gilbert, Gilbert Blilie PLLC  
**Staff Recommendation:** Approval, subject to stipulations

<a href="#">General Plan Conformity</a>			
<a href="#">General Plan Land Use Map Designation</a>		Commerce / Business Park	
<a href="#">Street Map Classification</a>	14th Street	Local	17-foot west half street
	University Drive	Local	0-foot north half street

***STRENGTHEN OUR LOCAL ECONOMY; MANUFACTURING AND INDUSTRIAL DEVELOPMENT; LAND USE: Support the expansion of industrial zoning in targeted industrial areas.***

The proposal will allow additional industrial development on the subject site, which is in an industrial area surrounded by A-2 zoning.

***CELEBRATE OUR DIVERSE COMMUNITIES AND NEIGHBORHOODS CORE VALUE; CLEAN NEIGHBORHOODS; LAND USE PRINCIPLE: Facilitate the acquisition of vacant, underutilized and blighted parcels for appropriate redevelopment, compatible with the adjacent neighborhood character and adopted area plans.***

The proposal will facilitate the development of vacant land previously used as a Southern Pacific Railroad line. The proposed development, as stipulated, will comply with the Rio Salado Beyond the Banks Plan by providing the Commerce Park Zoning District's landscape and building setback requirements. Additionally, the proposed zoning district will be compatible with the surrounding industrial land uses.

***BUILD THE SUSTAINABLE DESERT CITY CORE VALUE; DESIGN PRINCIPLE: Integrate trees and shade into the design of new development and redevelopment projects throughout Phoenix.***

The proposal, as stipulated, includes enhanced landscape setbacks and shaded designated employee and customer parking spaces. These improvements will create a comfortable pedestrian environment along 14th Street and University Drive and make aesthetic improvements to this industrial area.

#### **Applicable Plans, Overlays, and Initiatives**

[Rio Phx Initiative](#): Background Item No. 5.

[Rio Salado Interim Overlay District](#): Background Item No. 6.

[Rio Salado Beyond the Banks Area Plan](#): Background Item No. 7.

[Airport Noise Impact Overlay District](#): Background Item No. 8.

[Sky Harbor Land Reuse Strategy](#): Background Item No. 9.

[Shade Phoenix Plan](#): Background Item No. 10.

[Complete Streets Guidelines](#): Background Item No. 11.

[Zero Waste PHX](#): Background Item No. 12.

[Monarch Butterfly Pledge](#): Background Item No. 13.

[Conservation Measures for New Development](#): Background Item No. 14.

[Phoenix Climate Action Plan](#): Background Item No. 15.

Surrounding Land Uses and Zoning		
	<u>Land Use</u>	<u>Zoning</u>
<b>On Site</b>	Vacant land	R-3 RSIOD AIOD
<b>North</b>	Truck equipment sales and repair	A-2 RSIOD AIOD
<b>West</b>	Vacant land	A-2 RSIOD AIOD
<b>East (across 14th Street)</b>	Granite fabrication	A-2 RSIOD AIOD
<b>South</b>	Vacant land and warehouse	A-2 RSIOD AIOD

A-2 (Industrial District)		
<u>Standards</u>	<u>Requirements</u>	<u>Met or Not Met</u>
<i>Building Setbacks</i>		
Street (West)	25 feet	Not specified
Side (North)	0 feet	Not specified
Side (South)	0 feet	Not specified
Rear (East)	0 feet	Not specified
<i>Landscaped Setbacks</i>		
Street (West)	20 feet, per Stipulation No. 1	25 feet (Met)
Side (North)	0 feet	0 feet (Met)
Side (South)	0 feet	0 feet (Met)
Rear (East)	0 feet	0 feet (Met)
Maximum Building Height	56 feet; up to 80 feet with use permit	No structures proposed
Parking	1 space per 1.5 warehouse or production workers	12 spaces (Met)

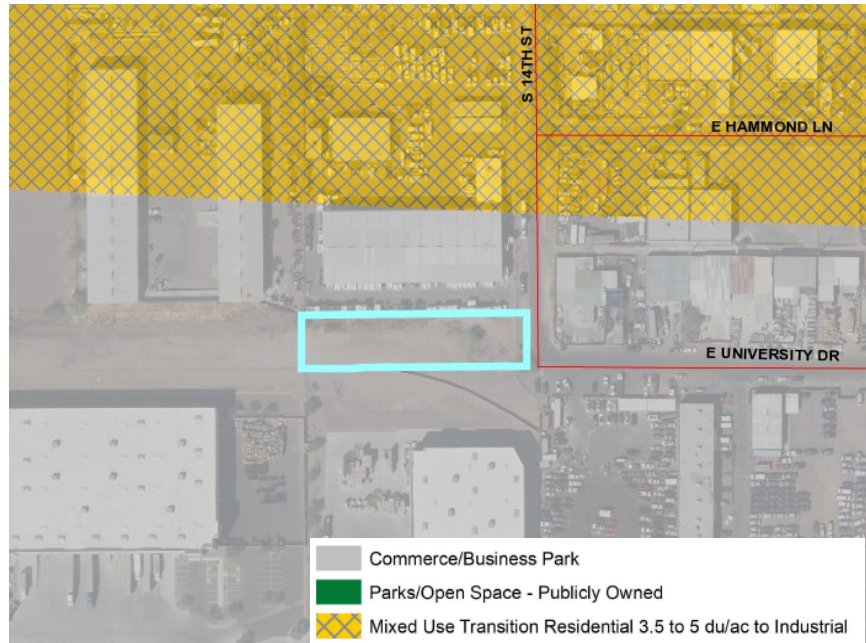
### **Background/Issues/Analysis**

#### **SUBJECT SITE**

1. This request is to rezone 0.57 acres located at the northwest corner of 14th Street and University Drive from R-3 RSIOD AIOD (Multifamily Residence District, Rio Salado Interim Overlay District, Airport Noise Impact Overlay District) to A-2 RSIOD AIOD (Industrial District, Rio Salado Interim Overlay District, Airport Noise Impact Overlay District) for outdoor storage. The subject site is currently vacant land and was previously a Southern Pacific Railroad line.

## GENERAL PLAN

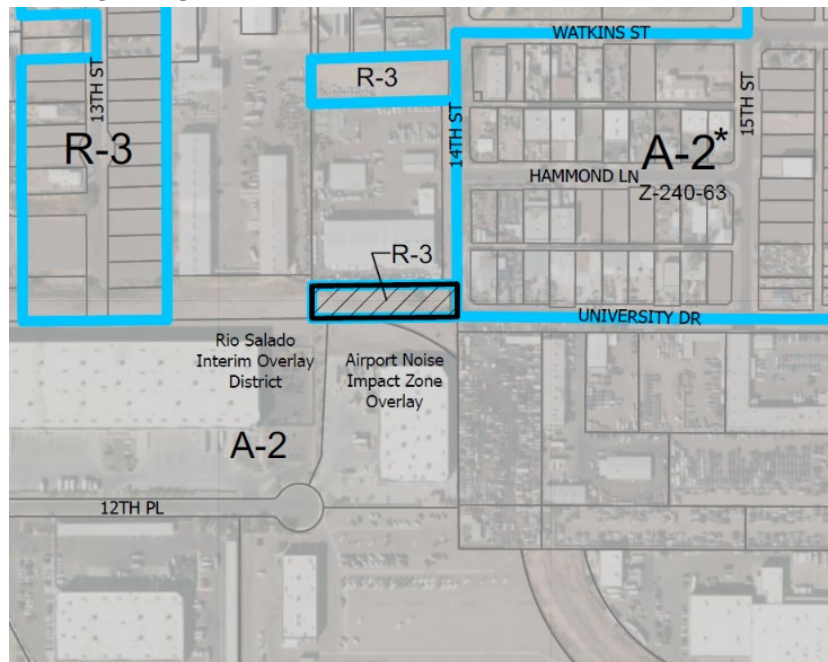
2. The subject site is designated as Commerce / Business Park. The areas to the east (across 14th Street), west, north, and south are designated Commerce / Business Park. The areas to the east (across 14th Street), west, north, and south are designated Commerce / Business Park. The proposed A-2 RSIOD AIOD zoning district is not consistent with the General Plan Land Use Map designation; however, a General Plan Amendment is not required because the site is less than 10 acres in size.



*General Plan Land Use Map; Source: Planning and Development Department*

## SURROUNDING LAND USE AND ZONING

3. To the north the subject site is a truck equipment sales and repair use zoned A-2 RSIOD AIOD. To the west is vacant land zoned A-2 RSIOD AIOD. To the east, across 14th Street, is a granite fabrication use zoned A-2 RSIOD AIOD. To the south is vacant land and a warehouse zoned A-2 RSIOD AIOD.



*Location Map; Source: Planning and Development Department*

## PROPOSAL

### 4. Site Plan

The proposal is for four outdoor storage lots. The conceptual site plan, attached as an exhibit, depicts the site layout including the location of the four storage lots, the parking area, and landscape areas. The site will have one point of ingress/egress from 14th Street/University Drive. The proposed rezoning only applies to the northeast portion of the overall site depicted on the conceptual site plan.



Conceptual Site Plan; Source: Kurt Waldier, Gillbert Blilie

## STUDIES AND POLICIES

### 5. Rio Phx Initiative

The Rio Phx initiative is a collaborative effort by the City of Phoenix and various property/business owners and community stakeholders to create a series of land use policy documents that will help promote a 20-mile Rio Salado (Salt River) corridor as a local and regional destination that attracts positive investment for the benefit of businesses, residents, and tourists. This plan aims to be a key implementation component of the regional Rio Reimagined project. This initiative also seeks to identify outstanding needs in the area and opportunities for housing, employment, recreation, art, and ecological restoration. The final aim of this project is also to improve health outcomes for residents.

To support and enhance the Rio Phx Initiative the proposal is required to provide an enhance landscape setback, shielded lighting, enhanced walls, and milkweed or other native nectar species incorporated into the planting materials. These are addressed in Stipulation Nos. 1 through 4.



6. **Rio Salado Interim Overlay District**

The subject site is located within the Rio Salado Interim Overlay District (RSIOD). The district is designed to control open, outdoor land uses and other uses in order to have a positive impact on the Rio Salado Habitat Restoration Project and add to the long-term value of adjacent land. The RSIOD recommends that all industrial development incorporate the Commerce Park zoning district standards. In alignment with the Rio Salado Beyond the Banks Area Plan, staff recommends Stipulation Nos. 1 and 5 to require a 20-foot enhanced landscape and building setback, consistent with the Commerce Park zoning district standards.

7. **Rio Salado Beyond the Banks Area Plan**

In December of 2003 the City of Phoenix adopted the Rio Salado Beyond the Banks Area Plan to identify goals and policies to guide development decisions for an area beyond the banks of the Rio Salado and to complement the Phoenix Rio Salado Habitat Restoration Project (Rio Salado Project).



The Rio Salado Beyond the Banks Area Plan recommends that all industrial development incorporate the Commerce Park zoning district standards. In alignment with the Rio Salado Beyond the Banks Area Plan, staff recommends Stipulation Nos. 1 and 5 to require a 20-foot enhanced landscape and building setback, consistent with the Commerce Park zoning district standards.

8. **Airport Noise Overlay District**

On June 1, 2005, the Phoenix City Council adopted the Airport Noise Overlay District (AIOD), generally bounded by the Southern Pacific Railroad on the north, University Drive on the south, 7th Street on the west, and 16th Street and 24th Street on the east. The Airport Noise Impact Overlay District is intended to promote well managed growth and to protect the health, safety and welfare of persons and residential property in noise impacted areas of Phoenix Sky Harbor International Airport. The AIOD applies to all new residential land uses. The

proposal for A-2, which does not permit residential uses, will not be impacted by the requirements of the AIOD.

9. **Sky Harbor Land Reuse Strategy**

The PHX Land Reuse Strategy (LRS) is the City of Phoenix's program for redeveloping noise land parcels west of Phoenix Sky Harbor. The PHX LRS envisions new and vibrant uses for this area and identifies the best ways to deliver economic benefits to both the community and the airport. The subject site is located in the PHX LRS South Sub-Area, which is designated as an Industrial District. The proposed A-2 zoning district is consistent with the South Sub-Area Industrial District.

10. **Shade Phoenix Plan:**

In November 2024, the Phoenix City Council adopted the Shade Phoenix Plan. The Shade Phoenix Plan prioritizes increasing shade coverage throughout the City to improve health and quality of life. Investing in shade can address the urban heat island effect, clean the air, preserve Sonoran vegetation, and prevent health complications related to prolonged exposure to heat. The Shade Phoenix Plan provides numerous strategies to increase shade including expanding and maintaining existing shade, strengthening tree code enforcement, and developing shade stipulations in rezoning cases.

The proposal, as stipulated, will create a streetscape environment with a shaded designated employee or customer parking and an enhanced landscape setback along the east perimeter of the site. These are addressed in Stipulation Nos. 1 and 8.

11. **Complete Streets Guidelines:**

The City of Phoenix City Council adopted the Complete Streets Guiding Principles. The principles are intended to promote improvements that provide an accessible, safe, connected transportation system to include all modes, such as bicycles, pedestrians, transit, and vehicles. The proposed development, as stipulated, will support walking, bicycling, and transit use by including an enhanced landscape setback along the street frontage. This is addressed in Stipulation No. 1. Additionally, staff recommends Stipulation No. 6 to require pavement treatments that visually contrast parking and drive aisles where pedestrian walkways cross a vehicular path to enhance pedestrian safety.

12. **Zero Waste Phoenix PHX:**

The City of Phoenix is committed to its waste diversion efforts and has set a goal to become a zero waste city, as part of the city's overall 2050 Environmental Sustainability Goals. One of the ways Phoenix can achieve this is to improve and expand its recycling and other waste diversion programs.

Section 716 of the Phoenix Zoning Ordinance establishes standards to encourage the provision of recycling containers for multifamily, commercial, and mixed-use developments meeting certain criteria. The application materials indicated that recycling services will not be provided because the use is not anticipated to produce waste.

13. **Monarch Butterfly Pledge:**

In April 2021, Mayor Kate Gallego signed the [National Wildlife Federation's Mayor's Monarch Pledge](#). This pledge commits the city to take action to support the monarch butterfly population. In the United States, loss of milkweed habitat is a major factor in the decline of the monarchs. Arizona has at least 29 species of milkweed native to the state. Adult monarchs feed on the nectar of many flowers, but they breed only where milkweeds are found. To support the monarch butterfly population, Stipulation No. 4 addresses the planting of milkweed shrubs, or other native nectar plant species, on the subject site.

14. **Conservation Measures for New Development:**

In June 2023, the Phoenix City Council adopted the Conservation Measures for New Development policy as part of a resolution addressing the future water consumption of new development (Resolution 22129). This resolution addresses the future water consumption of new development to support one of the City's Five Core Values in the General Plan which calls for Phoenix to - Build the Sustainable Desert City. The Conservation Measures for New Development policy includes direction to develop standards for consideration as stipulations for all rezoning cases that will address best practices related to water usage in nine specific categories. This is addressed in Stipulation Nos. 7 through 9.

15. **Phoenix Climate Action Plan**

In October 2021, the Phoenix City Council approved the Climate Action Plan. The Climate Action Plan will serve as a long-term plan to achieve greenhouse gas emissions reductions and resiliency goals from local operations and community activities as well as prepare for the impacts of climate change. This plan contains policy and initiatives regarding stationary energy, transportation, waste management, air quality, local food systems, heat, and water. Goal W2 (Water), Action W2.4, pertains to the implementation of the [Greater Phoenix Metro Green Infrastructure \(GI\) and Low Impact Development Details for Alternative Stormwater Management](#) to benefit the environment, promote water conservation, reduce urban heat, improve the public health, and create additional green spaces. This goal is addressed in Stipulation No. 7, which requires a minimum of one GI technique for stormwater management to be implemented in this development.



#### COMMUNITY CORRESPONDENCE

16. As of the writing of this report, no letters of support or opposition have been received for the request.

#### INTERDEPARTMENTAL COMMENTS

17. The Street Transportation Department requires that a minimum 30 feet of right-of-way be dedicated and constructed along the west side of 14th Street and the south side of University Drive (along the transition from 14th Street to University Drive), that a minimum four-foot sidewalk be provided along the west side of 14th Street and the south side of University Drive, and that the developer replace and construct all improvements in the right-of-way with all required elements and to ADA standards. These are addressed in Stipulation Nos. 10 through 13.

#### OTHER

18. The site is located in a larger area identified as being archaeologically sensitive. If further review by the City of Phoenix Archaeology Office determines the site and immediate area to be archaeologically sensitive, and if no previous archaeological projects have been conducted within this project area, it is recommended that archaeological Phase I data testing of this area be conducted. Phase II archaeological data recovery excavations may be necessary based upon the results of the testing. A qualified archaeologist must make this determination in consultation with the City of Phoenix Archaeologist. In the event archaeological materials are encountered during construction, all ground disturbing activities must cease within a 33-foot radius of the discovery and the City of Phoenix Archaeology Office must be notified immediately and allowed time to properly assess the materials. These are addressed in Stipulation Nos. 14 through 16.
19. Staff has not received a completed form for the Waiver of Claims for Diminution in Value of Property under Proposition 207 (A.R.S. 12-1131 et seq.), as required by the rezoning application process. Therefore, a stipulation has been added to require the form be completed and submitted prior to final site plan approval. This is addressed in Stipulation No. 17.
20. Development and use of the site are subject to all applicable codes and ordinances. Zoning approval does not negate other ordinance requirements. Other formal actions such as, but not limited to, zoning adjustments and abandonments may be required.

#### **Findings**

1. The requested A-2 zoning district is compatible with the surrounding zoning districts.

2. The proposal, as stipulated, will provide Commerce Park building and landscape setbacks in alignment with the Rio PHX initiative, Rio Salado Interim Overlay District, and the Rio Salado Beyond the Banks Area Plan.
3. The proposal will develop a vacant property and as stipulated will be compatible with surrounding land uses.

**Stipulations:**

1. A minimum 20-foot-wide landscape setback shall be provided along 14th Street and University Drive. This landscape setback shall be planted with minimum 2-inch caliper trees, planted 20 feet on center, or in equivalent groupings, and five 5-gallon shrubs per tree, as approved by the Planning and Development Department. When possible, the developer shall use existing trees and landscaping to meet the landscaping requirements. Where utility conflicts exist, the developer shall work with the Planning and Development Department on alternative design solutions.
2. Outdoor site lighting shall be fully shielded, as approved or modified by the Planning and Development Department.
3. Fencing and walls oriented towards a public street or trail shall have a finished appearance and contain decorative elements including but not limited to adobe, glass, metal (including rusted metal), stone (stone veneer), wood, or other material consistent with a natural environment, as approved by the Planning and Development Department. Where gates are provided for vehicular access, these gates shall be decorative and may include minor perforations to accommodate wind loads. The perforation shall not exceed 50% of the gate surface area.
4. A minimum of 10% of the required shrubs, shall be a native milkweed or other native nectar species, and shall be planted in groups of three or more, as approved by the Planning and Development Department.
5. If a building is proposed, a minimum 20-foot building setback shall be provided along the east perimeter of the site.
6. If a building is proposed, where pedestrian walkways cross a vehicular path, the pathway shall be constructed of decorative pavers, stamped or colored concrete, or other pavement treatments that visually contrasts parking and drive aisle surfaces, as approved by the Planning and Development Department.

7. A minimum of one green stormwater infrastructure (GSI) element for stormwater management shall be implemented, as approved or modified by the Planning and Development and/or Street Transportation departments. This includes but is not limited to stormwater harvesting basins, bioswales, permeable pavement, etc., per the Greater Phoenix Metro Green Infrastructure and Low Impact Development Details for Alternative Stormwater Management.
8. A minimum of 25% of the surface of the designated employee or customer parking areas shall be shaded, as approved by the Planning and Development Department. Shade may be achieved by structures or by minimum 2-inch caliper, drought tolerant, shade trees, or a combination thereof.
9. Natural turf shall only be utilized for required retention areas (bottom of basin, and only allowed on slopes if required for slope stabilization), as approved by the Planning and Development Department.
10. A minimum 30 feet of right-of-way shall be dedicated and constructed for the west side of 14th Street and the south side of University Drive, to complete the transition from 14th Street to University Drive.
11. A minimum 4-foot wide sidewalk shall be constructed on the west side of 14th Street and the south side of University Drive, adjacent to the development.
12. Replace any broken or out-of-grade curb, gutter, sidewalk, and curb ramps on all streets and upgrade all off-site improvements to be in compliance with current ADA guidelines.
13. All streets within and adjacent to the development shall be constructed with paving, curb, gutter, sidewalk, curb ramps, streetlights, median islands, landscaping and other incidentals, as per plans approved by the Planning and Development Department. All improvements shall comply with all ADA accessibility standards.
14. If determined necessary by the Phoenix Archaeology Office, the applicant shall conduct Phase I data testing and submit an archaeological survey report of the development area for review and approval by the City Archaeologist prior to clearing and grubbing, landscape salvage, and/or grading approval.
15. If Phase I data testing is required, and if, upon review of the results from the Phase I data testing, the City Archaeologist, in consultation with a qualified archaeologist, determines such data recovery excavations are necessary, the applicant shall conduct Phase II archaeological data recovery excavations.

16. In the event archaeological materials are encountered during construction, the developer shall immediately cease all ground-disturbing activities within a 33-foot radius of the discovery, notify the City Archaeologist, and allow time for the Archaeology Office to properly assess the materials.
17. Prior to final site plan approval, the landowner shall execute a Proposition 207 waiver of claims form. The waiver shall be recorded with the Maricopa County Recorder's Office and delivered to the City to be included in the rezoning application file for record.

**Writer**

Samuel Rogers

July 30, 2025

**Team Leader**

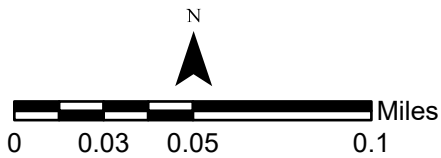
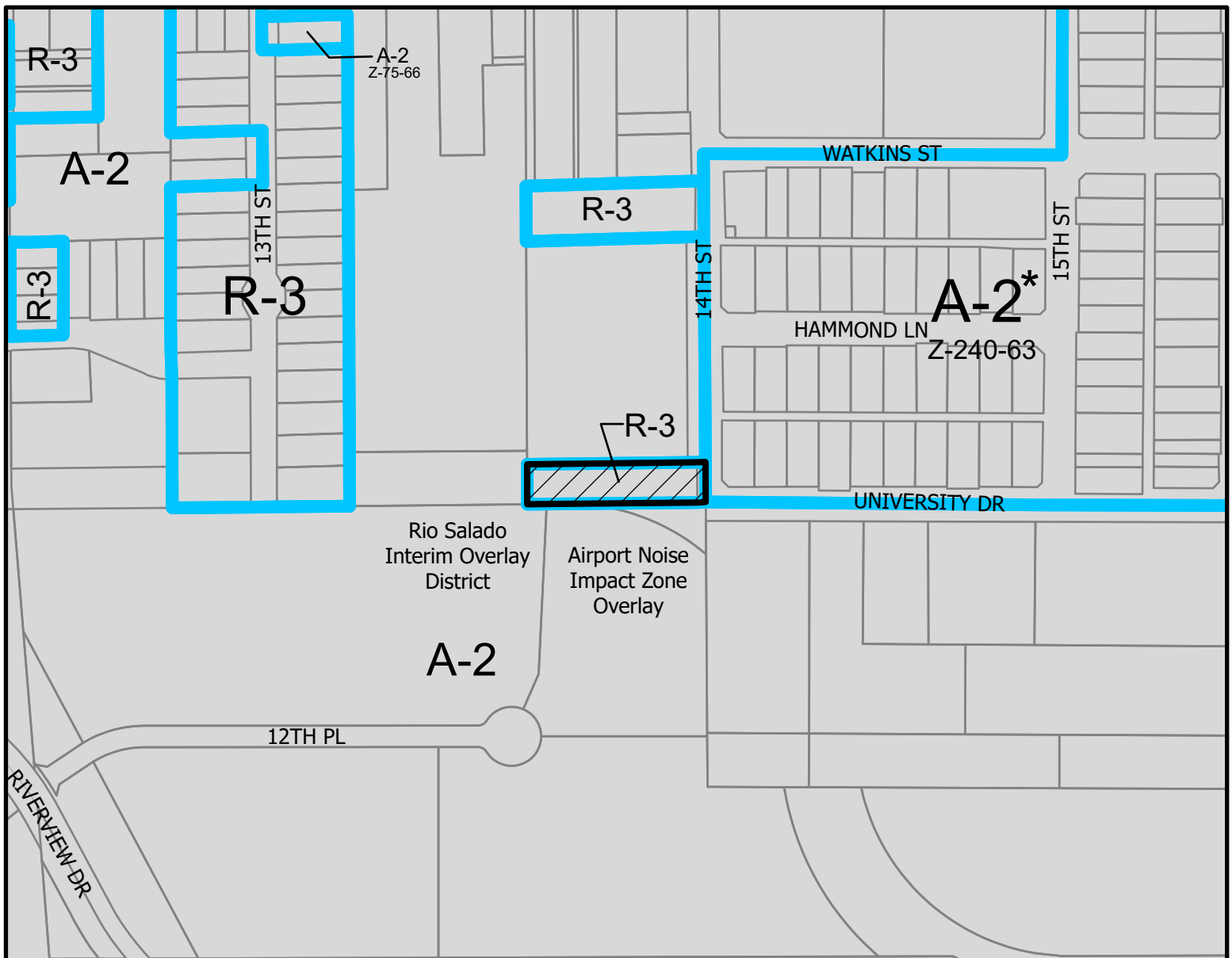
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**Exhibits**

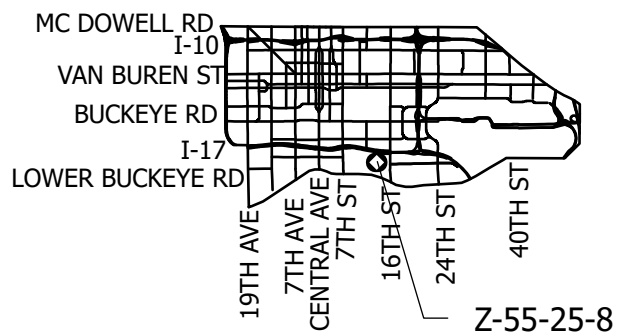
Zoning sketch map

Aerial sketch map

Conceptual Site Plan date stamped May 7, 2025

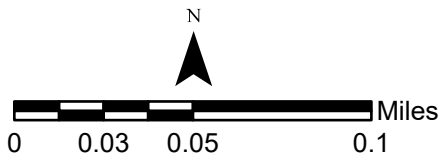
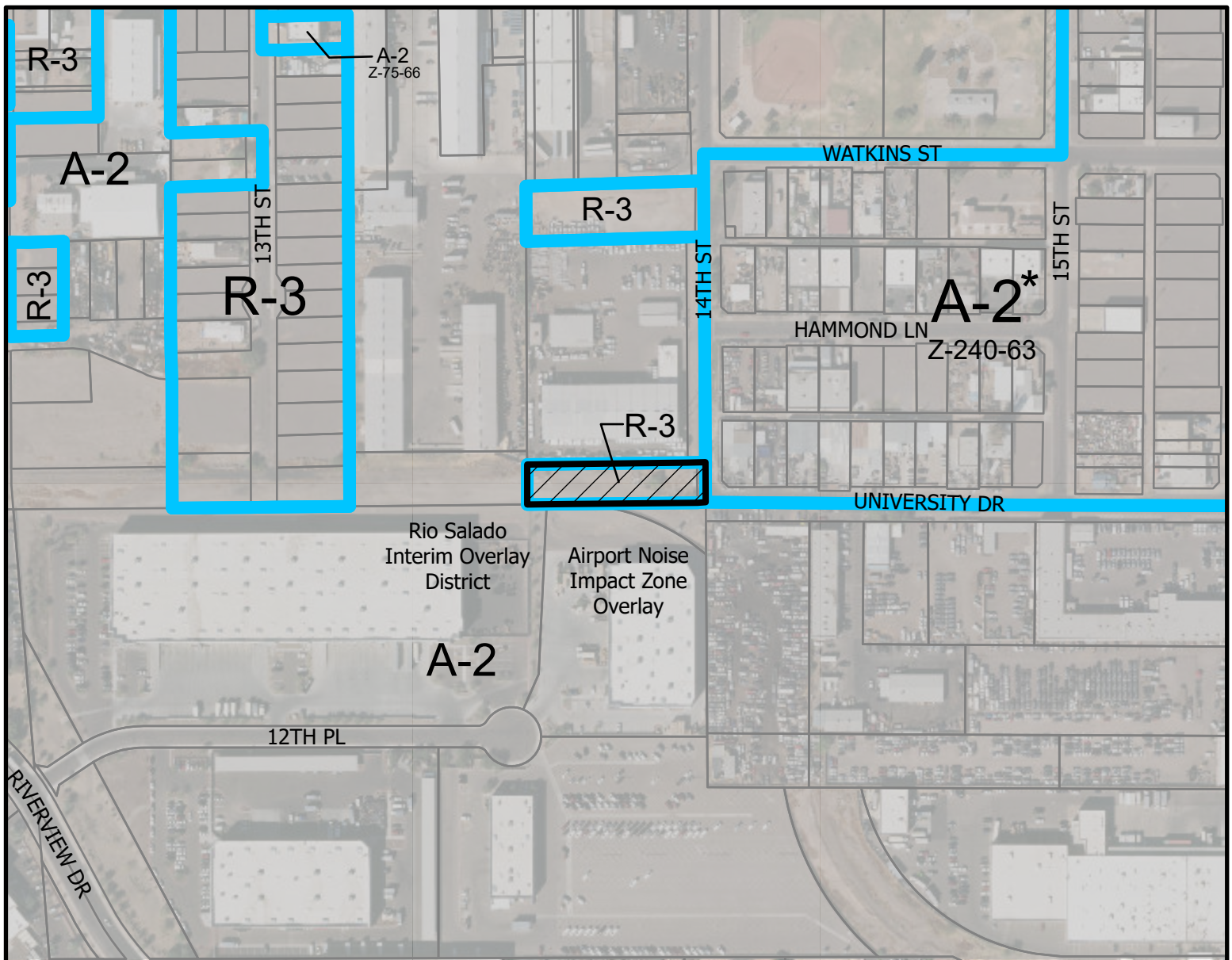


**CENTRAL CITY VILLAGE**  
COUNCIL DISTRICT: 8



APPLICANT'S NAME: <b>Gilbert Blilie, PLLC</b>		REQUESTED CHANGE:										
APPLICATION NO: <b>Z-55-25-8</b>		FROM: <b>R-3 RSIOD AIOD ( 0.57 ac.)</b>										
DATE: <b>5/22/2025</b>		TO: <b>A-2 RSIOD AIOD ( 0.57 ac.)</b>										
REVISION DATES:												
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GROSS AREA INCLUDING 1/2 STREET AND ALLEY DEDICATION IS APPROX. <b>0.57 Acres</b>												
<table border="1"> <tr> <td>MULTIPLES PERMITTED</td> <td>CONVENTIONAL OPTION</td> <td>* UNITS P.R.D OPTION</td> </tr> <tr> <td><b>R-3 RSIOD AIOD</b></td> <td><b>8</b></td> <td><b>10</b></td> </tr> <tr> <td><b>A-2 RSIOD AIOD</b></td> <td><b>N/A</b></td> <td><b>N/A</b></td> </tr> </table>		MULTIPLES PERMITTED	CONVENTIONAL OPTION	* UNITS P.R.D OPTION	<b>R-3 RSIOD AIOD</b>	<b>8</b>	<b>10</b>	<b>A-2 RSIOD AIOD</b>	<b>N/A</b>	<b>N/A</b>		
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\* Maximum Units Allowed with P.R.D. Bonus



**CENTRAL CITY VILLAGE**  
COUNCIL DISTRICT: 8

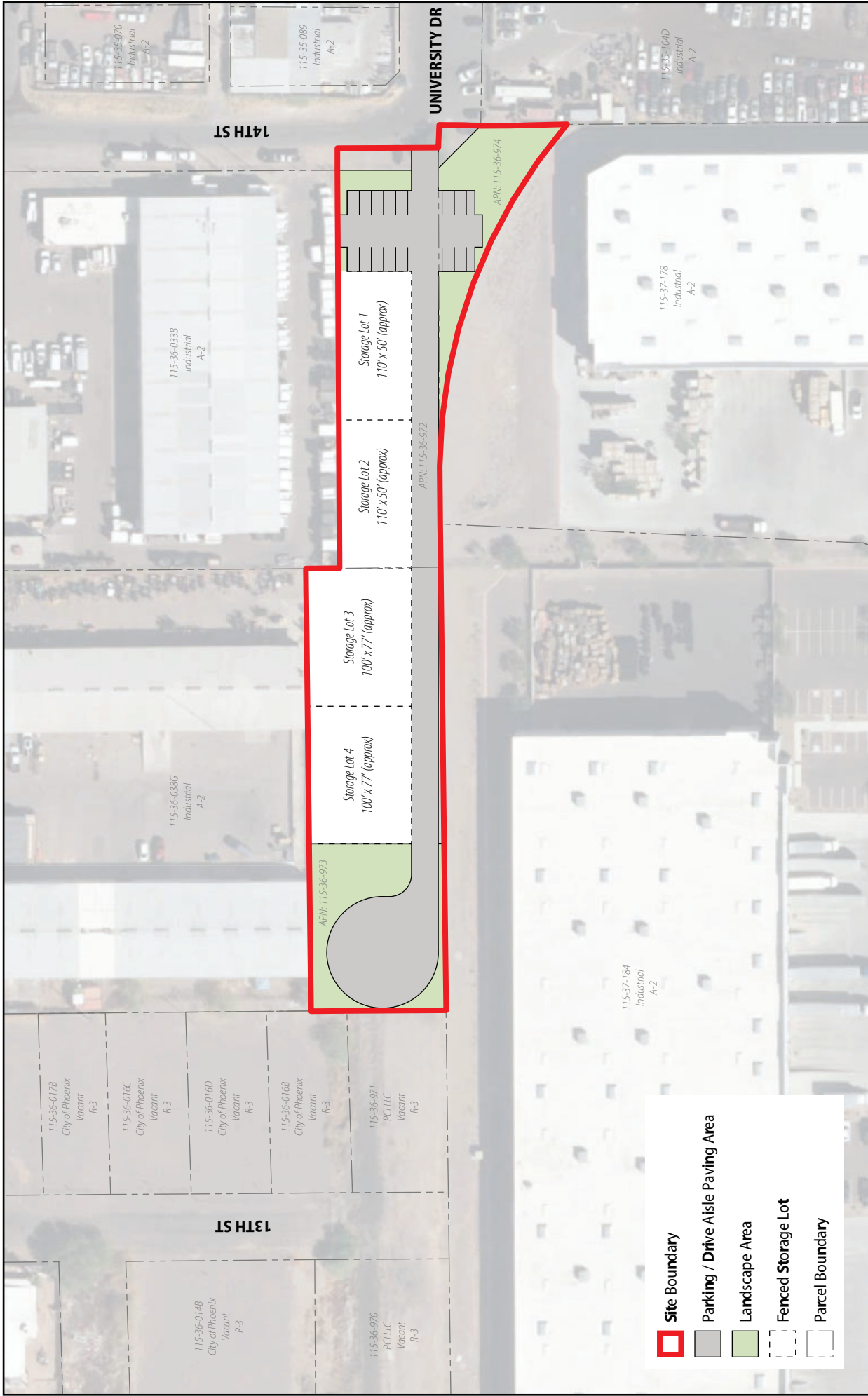


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		* UNITS P.R.D OPTION <b>10</b> <b>N/A</b>	

\* Maximum Units Allowed with P.R.D. Bonus



# PCI 14th St and University



## PROJECT DATA

**Property Owner:**

**APN(s):**

**Site Area:**

PCI LLC

115-36-973, 115-36-972, 115-36-974

76,708 S.F. / 1.76 acres

**Current Zoning:**

A-2, R-3

A-2

**Proposed Zoning:**

**Proposed Use:**

Outdoor Storage

**CITY OF PHOENIX**

**MAY 07 2025**

**Planning & Development  
Department**



Scale: 1" = 100'

0' 25' 50' 100'

Date: 2/1/2025