

# GILA FOOTHILLS

## PLANNED UNIT DEVELOPMENT

Project Narrative  
Case #: Z-53-22-8

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CITY OF PHOENIX

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- A3. Legal Description







# PART 1 - PUD NARRATIVE

## PLANNED UNIT DEVELOPMENT

The Planned Unit Development (“PUD”) zoning district is authorized by Chapter 6, Section 671 of the Zoning Ordinance of the City of Phoenix (“Phoenix Zoning Ordinance”). A PUD is intended to be a standalone document that sets forth the regulatory framework, including permitted uses, development standards and design guidelines, for a particular project (“PUD Regulations”). The PUD may only modify provisions within the Phoenix Zoning Ordinance and does not modify other City of Phoenix codes, regulations or requirements. A PUD may include substantial background information and narrative discussion, including purpose and intent statements, which are intended to illustrate the overall character and vision for the development. Such statements are not regulatory and are not requirements to be enforced by the City of Phoenix.

The PUD regulations apply to all property within the PUD project boundary. The PUD regulations supersede and replace all applicable Phoenix Zoning Ordinance requirements. If there is a conflict between PUD regulations and the Phoenix Zoning Ordinance, including the design guidelines within the Phoenix Zoning Ordinance, the terms of this PUD shall apply. If a provision is not addressed by the PUD, then the applicable portions of the Phoenix Zoning Ordinance will control.

## 1.2 PURPOSE AND INTENT

The Gila Foothills Planned Unit Development (“PUD”) changes the existing commercial and residential entitlements to promote a mixed-use development located adjacent to the recently completed Congressman Ed Pastor Loop 202 Freeway (“Freeway”).

The PUD encompasses approximately 290 gross acres of land within the 919-acre Tierra Montana PCD located to the north and south of the intersection of Estrella Drive and 51st Avenue, extending from the Freeway to 47th Avenue (the “Site”). The Tierra Montana PCD was approved in 2005 and later amended in 2016 [Z-109-05-7(8), PHO-1-16].

The PUD includes a mixture of land uses such as employment, commercial, health care, an EV-focused auto campus and multi-family. The Site is currently entitled for employment, commercial, and residential uses. The PUD also identifies the location of a planned public trailhead at South Mountain Park with connections to local and regional public trails long overdue in Laveen Village.





## THE GOALS AND OBJECTIVES OF THE GILA FOOTHILLS PUD ARE TO:

- Establish a master plan to create a multi-phased development consisting of a mix of land uses and development options that will complement the area's existing development patterns.
- Guide future development by assigning allowed uses and performance standards to Development Units, and design guidelines to Character Areas.
- Respond to the City of Phoenix Laveen Technology Corridor with a meaningful employment use.
- Provide for branding opportunities for Laveen Village on billboards along the recently completed Freeway.
- Create transition corridors and landscape buffers appropriate to create a seamless integration of the proposed uses.
- Work with the City of Phoenix to locate a publicly accessible trailhead into the South Mountain Regional Park.
- Provide for quality employment opportunities as part of a employment campuses focused on electric vehicles and healthcare services.







EXHIBIT 1 - Site Context

N.T.S. 

## 1.3 EXISTING SITE CONDITIONS

The Site is approximately 290 gross acres and is comprised of Maricopa County Assessor parcels, numbers: 300-07-010E, 300-07-002C, 300-07-011A, 300-03-020B, 300-03-019C, E, N, J, 300-03-021A, 300-03-032A, 300-03-011H, and 300-03-011J.

The Site is currently utilized for agricultural purposes and is bounded on the north and east by a combination of new suburban residential development, and existing rural residential uses in unincorporated Maricopa County. The southeastern edge of the property is adjacent to South Mountain Park. Freeway access to the Site is provided by the Estrella Drive interchange, and 51st Avenue and Estrella Drive (both designated as arterial roadways) that extend through the Site. The Freeway defines the west edge of the property, with the Gila River Indian Community (“GRIC”) beyond. Across the Freeway from the Site, the GRIC Land Use Plan shows Regional Commercial land uses as well as Neighborhood land uses. The Regional Commercial land uses follow along the southern boundary of the Freeway and extend to the existing Gila River Vee Quiva Casino and Hotel.

See Exhibit 1: Context Aerial.



GRIC Land Use Plan



Gila River Vee Quiva Casino and Hotel



## 1.4 GENERAL PLAN LAND USE MAP

The existing land use designations that encompass the Site include the following:

- (Residential) 15+ du/acre – Higher density attached townhouses, condominiums, or apartments
- (Residential) 5 to 10 du/acre – Traditional lot
- (Residential) 3.5 to 5 du/acre – Traditional Lot
- (Residential) 2 to 3.25 du/acre – Traditional Lot
- (Residential) 1 to 2 du/ac – Large Lot
- (Residential) 0 to 1 du/ac – Large Lot
- Commercial
- Resort
- Public/Quasi-Public

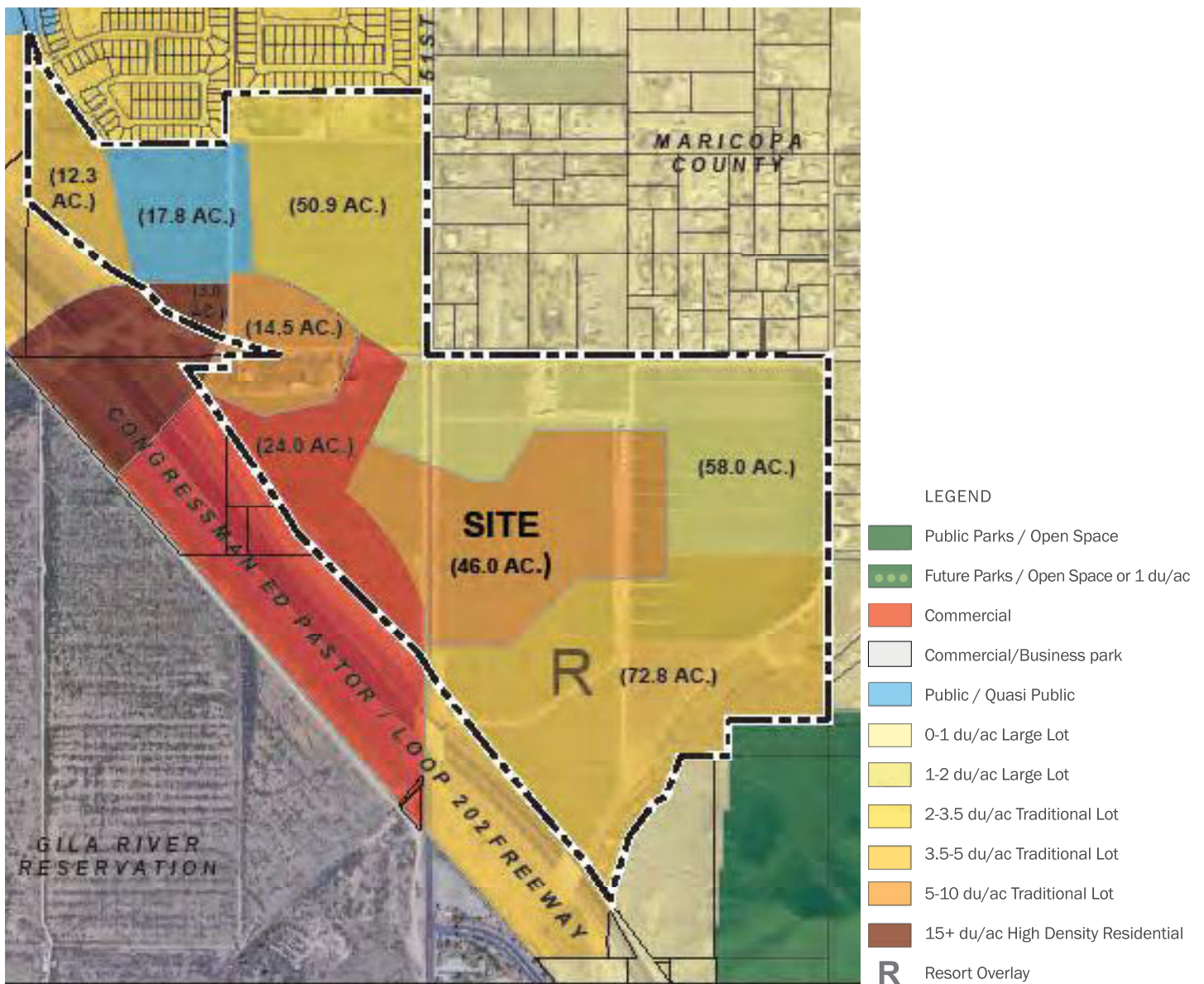


EXHIBIT 2 - Existing City of Phoenix General Plan Land Use Plan

N.T.S.

## 1.5 MINOR GENERAL PLAN AMENDMENT

To facilitate the PUD, a concurrent Minor General Plan Amendment application is being processed with this request to amend the Site's General Plan designation. See Figure 2: Existing General Plan Land Use Map and Figure 3: Proposed General Plan Land Use Map. This proposed General Plan Amendment will establish multiple land use designations across the entirety of the 290-acre Site including:

- 10 to 15 du/ac and 15+ du/ac (80 acres)
- Commercial and Commerce/Business Park (104 acres)
- Commercial, Commerce/Business Park, 10-15 du/ac and 15+ du/ac (66 acres)
- Commercial, 10-15 du/ac and 15+ du/ac (40 acres)

Additionally, the Resort land use designation will overlay 56 acres as shown in Exhibit 3.

The land uses proposed with the PUD request will be consistent with the proposed General Plan land use designations proposed by this Minor GPA and the Tierra Montana Functional Segment Two Master Street Plan.

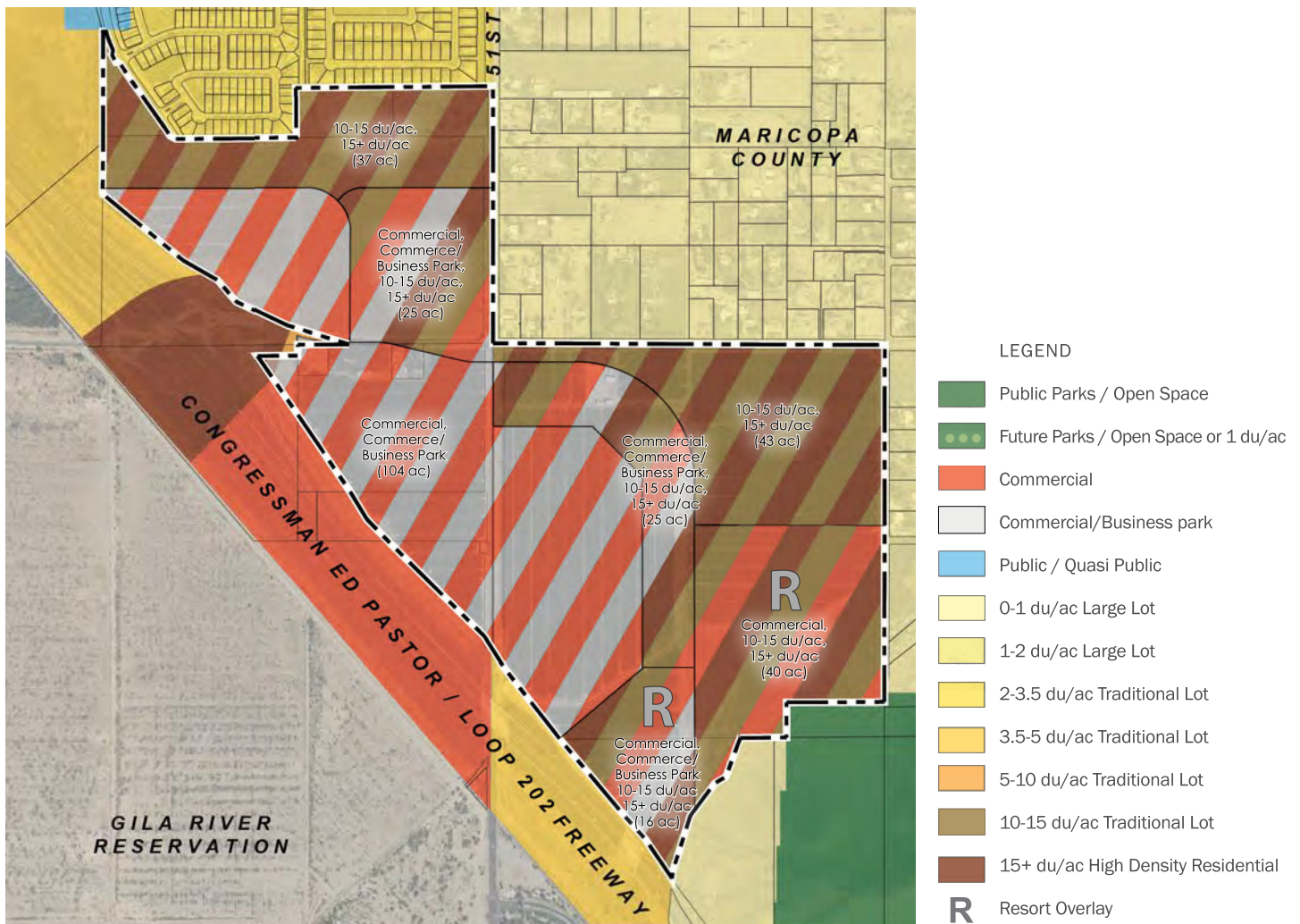


EXHIBIT 3 - Proposed General Plan Amendment

N.T.S.



## 1.6 EXISTING PCD ENTITLEMENTS

The Tierra Montana PCD includes a combination of zoning designations including commercial, large lot residential (1-2 du/ac), High-Density Residential (“HDR”), park/schools, church, and traditional lot residential ranging from 2 – 10 du/ac. Pursuant to the existing Tierra Montana PCD entitlement, approximately 1,700 dwelling units are permitted across the Site. The PCD was established in 2005 per Z-109-05 and later amended in 2016. See Exhibit 4: Existing Zoning Map.

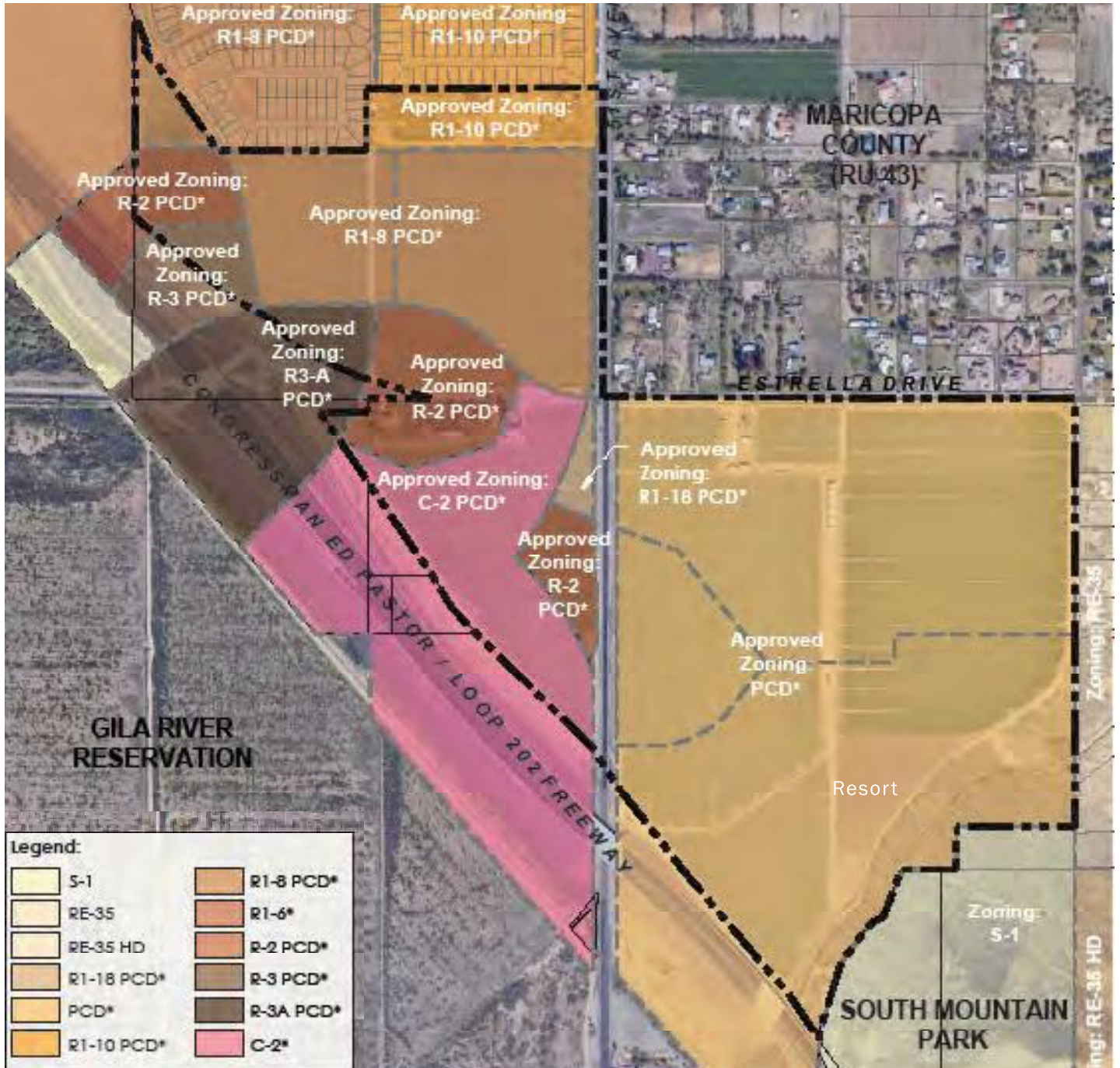


EXHIBIT 4 - Existing Zoning Map

N.T.S.

## 1.7 FREEWAY IMPACT

The Arizona Department of Transportation (“ADOT”) acquired by condemnation approximately 60 acres of the project area as part of the Ed Pastor Loop 202 Freeway development including areas for the Freeway corridor and for retention (PCD Parcel 15). The relocation of the interchange to Estrella Drive ultimately led to a 2016 amendment to the PCD that allowed for a combined park/school Site within Tierra Montana. Those portions of Tierra Montana north of the subject application area have been platted and development is nearing buildout.

As shown in Exhibits 5 and 5.1 Freeway Impact, perhaps the largest unforeseen impact of the Freeway on the PCD was the relocation and redesign of the interchange from the 51st Avenue alignment to Estrella Drive. At the time of the original PCD entitlements of the Site, freeway access was proposed from 51<sup>st</sup> Avenue with traditional linear access ramps. To accommodate this, 51<sup>st</sup> Avenue was planned to curve to the southwest to meet the Freeway at a perpendicular angle. The original PCD entitlements provided for a major commercial and multi-family core at the 51st Avenue interchange. Estrella Drive was planned to continue in a straight alignment and extend under the Freeway without providing Freeway access.

As it exists today, Freeway access is provided from Estrella Drive through two traffic circles. To accommodate this, the Estrella Drive alignment has been adjusted to curve to the north as it approaches the Freeway. 51<sup>st</sup> Avenue now extends in a straight alignment under the Freeway without providing Freeway access.

The redesign and relocation of the interchange and associated street alignments through the Site resulted in a loss of approximately 60-acres of land and bifurcated commercial land uses that are now incorrectly positioned to utilize the originally planned Estrella Drive interchange and therefore requires a revisitation of the Site’s existing zoning.

In addition to updating the various existing zoning districts to respond to the Freeway realignment and land uses proposed by this PUD, the proposed street network has been updated to better accommodate future development and circulation throughout the Site and surrounding area. Final street network design shall be determined by the approved Tierra Montana Functional Segment Two Master Street Plan.



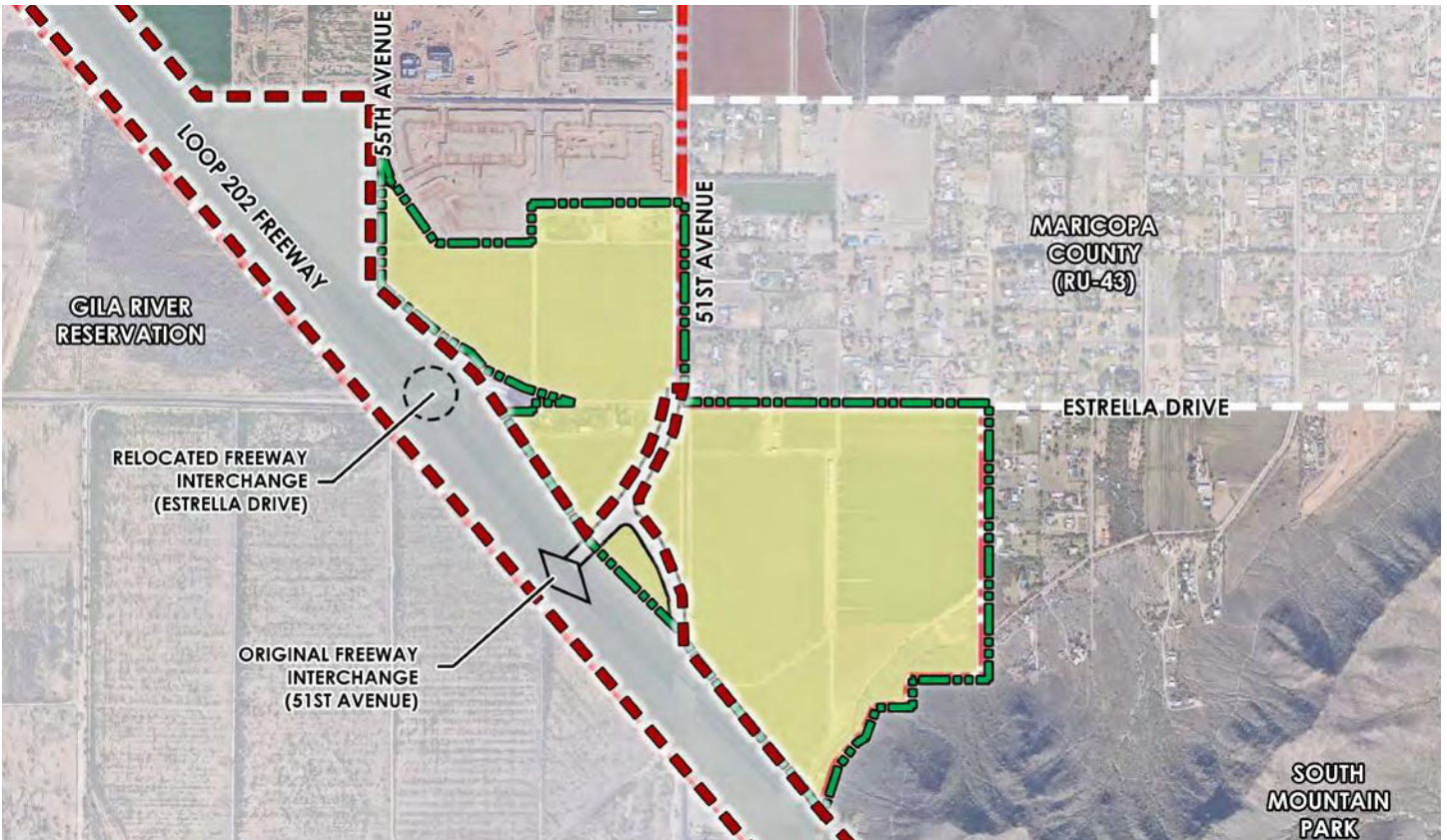


EXHIBIT 5 - 2013 ADOT Freeway Alignment

N.T.S. ⓘ

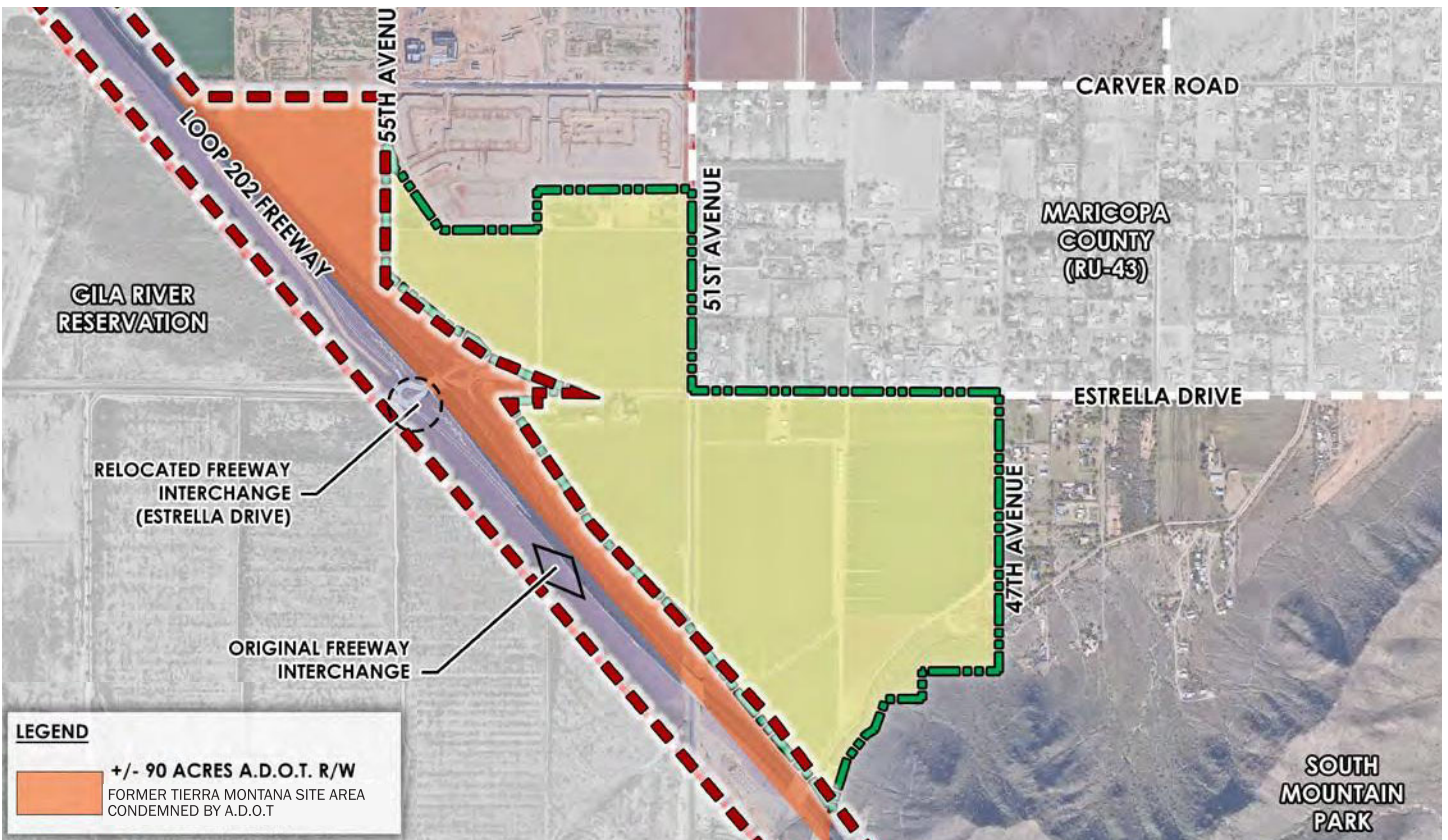


EXHIBIT 5.1 - Final ADOT Freeway Alignment

N.T.S. ⓘ



## 1.8 PROPOSED PUD REZONING

As shown in Exhibit 6 Proposed Zoning, the entire site is proposed to be zoned PUD for Planned Unit Development to allow development of the site in line with the land uses, development standards, and design guidelines of this document.

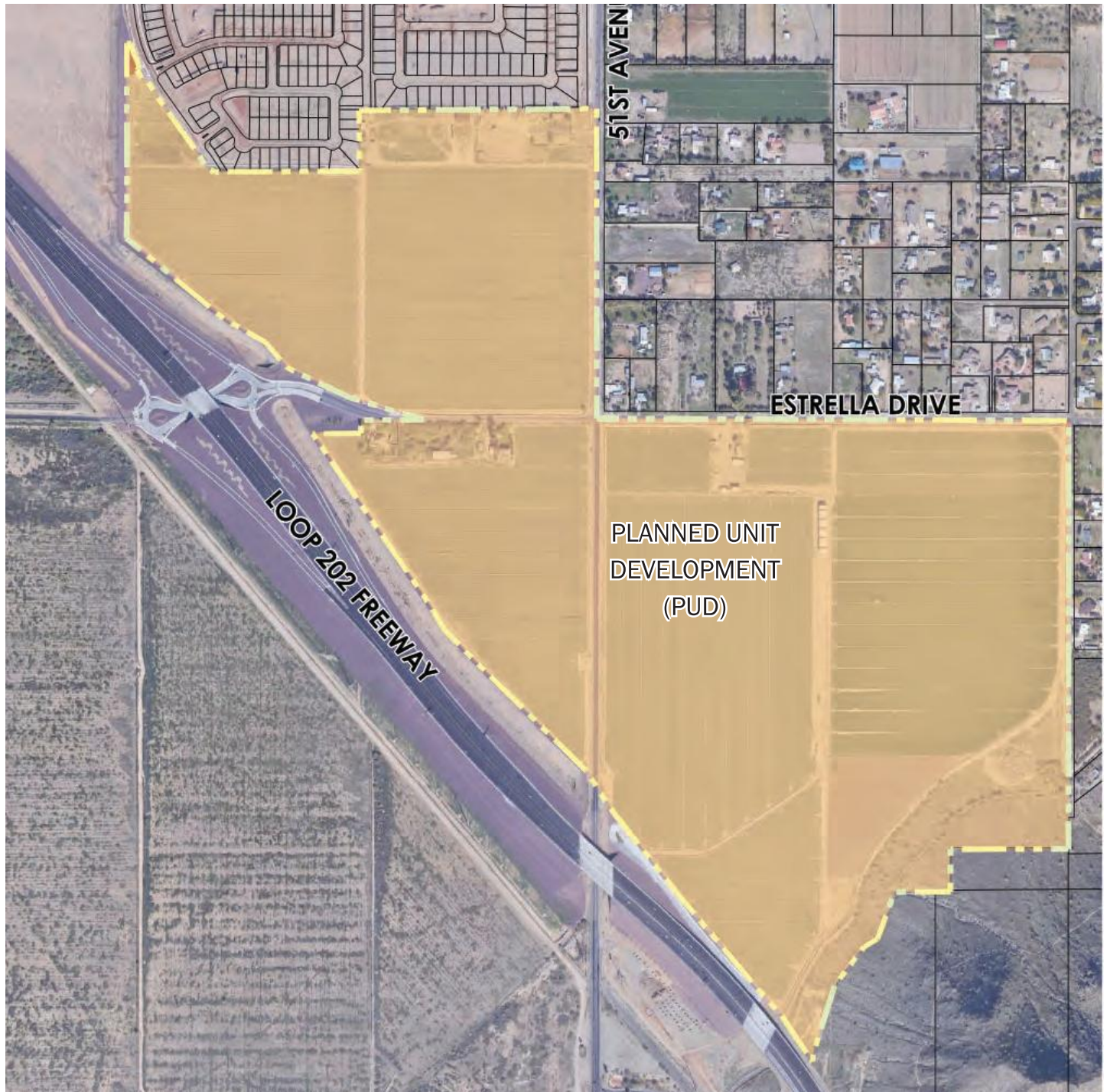


EXHIBIT 6 - Proposed Zoning

N.T.S. ①



## 1.9 FRAMEWORK OF THE PUD

The framework of this PUD has been organized to create three distinct Character Areas and twelve Development Units. A unique set of Design Guidelines is proposed for each Character Area, and land uses, Development Standards, and Landscape Standards are assigned to each Development Unit.

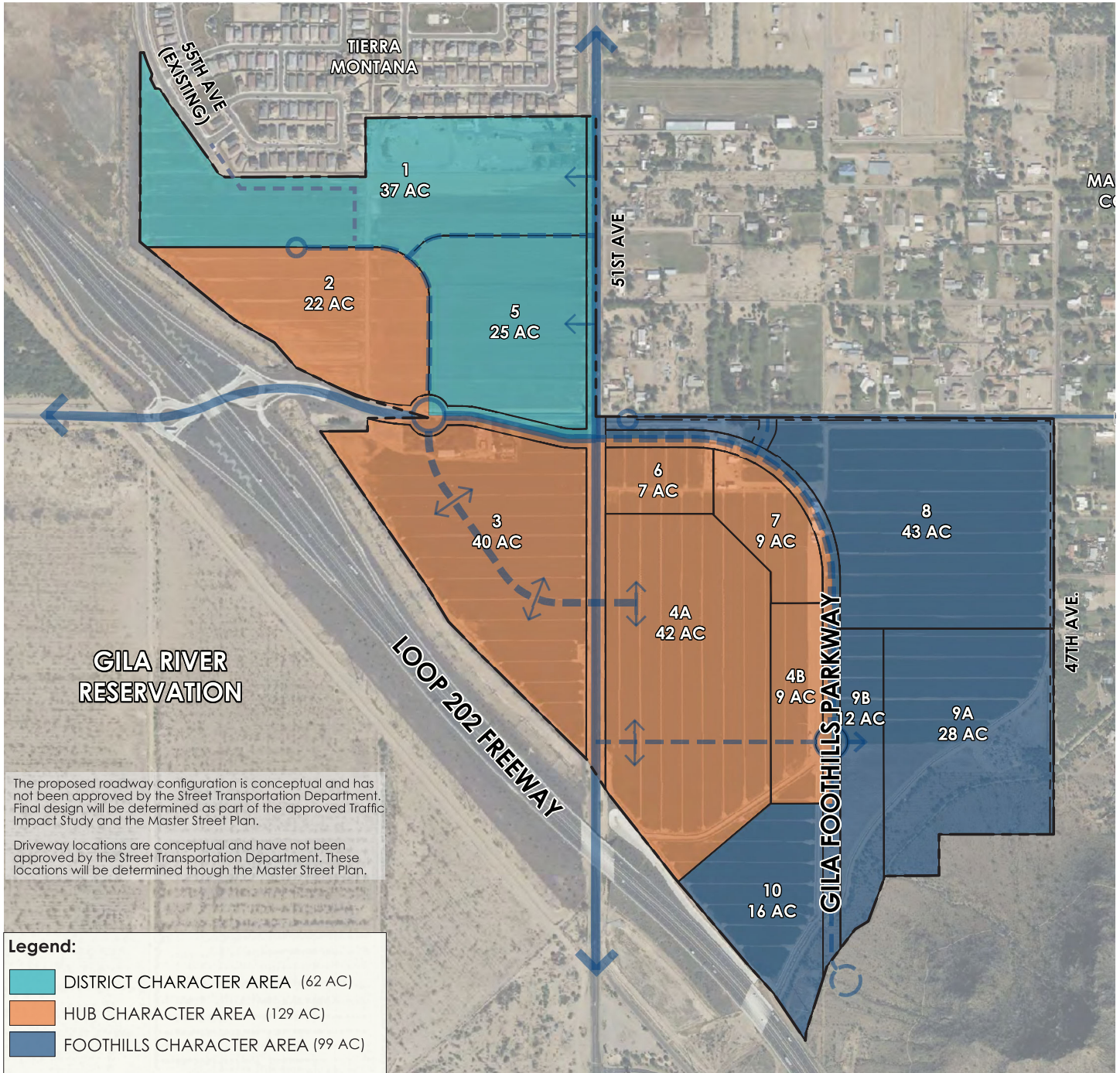


EXHIBIT 7 - Character Areas

N.T.S.

## 2. CHARACTER AREAS

Three Character Areas are proposed across the Site: The Foothills, The District, and The Hub Character Areas. The boundaries of these Character Areas are based on the relationship to surrounding land uses, adjacent roadway types, as well as the unique concerns and priorities voiced by property owners during community outreach. Each Character Area has a unique set of Design Guidelines to guide the aesthetic appearance of buildings and site features. Below is a brief introduction to each Character Area. Design Guidelines are detailed for each Character Area in Part 2.

### The Foothills Character Area

The Foothills Character Area is approximately 99 acres and is generally bounded by Estrella Drive on the north, South Mountain Park on the south, 47th Avenue on the east and the proposed Gila Foothills Drive on the west. The Foothills Character Area is comprised of Development Units 8 through 10.

The Foothills Character Area Design Guidelines call for a modern Sonoran aesthetic that incorporates and refines the building materials, landscaping, and architectural details of the surrounding community. The desert themes relate to the proximity of the character area to the Park, and the Sonoran themes are derived from elements found within the Laveen community. These architectural themes identify building materials, landscaping, and character details found within the surrounding community and applies them in a high-quality manner to residential land uses.

Beyond the Design Guidelines, further components of this document (as mentioned in Section 3 above) focus on carefully transitioning land uses and preserving the existing residential nature and Laveen character of Estrella Drive and 47th Avenue, while also providing for access to a new public trailhead into South Mountain Park and complimentary and vibrant neighborhood-serving commercial uses.

### The District Character Area

The District Character Area is approximately 62 acres and is located between the intersection of Estrella Drive and 51st Avenue and the Tierra Montana neighborhood. It contains Development Units 1 and 5.

Guidelines for The District Character Area encourage a synergy of diverse uses and building design types that promote adjacency and pedestrian continuity, balanced with land use intensity that is indicative of freeway adjacency. Land uses will be connected by a coordinated and comfortable circulation system that engages pedestrians and bicyclists as well as vehicles. Architectural character will borrow from modern rural and agrarian themes that are complementary to nearby residential communities.

Further components of this PUD separate building height and commercial traffic from the surrounding residential areas and extend the existing trail system from The Tierra Montana neighborhood into South Mountain Park.

### The Hub Character Area

The Hub Character Area is approximately 129 acres and primarily located along the Freeway within the Laveen Tech Corridor. The Hub District is the center of employment activity of the Site and surrounding area and may include hospital, associated healthcare uses and an EV-focused auto campus.

Design Guidelines for this area call for subtle Laveen references to be incorporated into the modern design of buildings and site features. Where an EV-focused auto campus is proposed, specific landscape guidelines support the visibility of the vehicle display areas. Additional components of this document aim to establish a sense of arrival along Estrella Drive and promote the visibility of employment and commercial land uses from the Freeway and arterial streets.



## 3. LAND USE PLAN

### 3.1 Land Use Categories

A diversity of land use types is proposed throughout the Site. Exhibit 8 illustrates the general location(s) of each proposed land use category. Where a striped area is shown, more than one land use category is proposed.

The proposed Land Use Plan envisions Employment land uses along the Freeway. Locating employment uses here appropriately utilizes Freeway visibility to contribute to the City of Phoenix’s economic vitality and generate revenue. This Laveen Technology Corridor also establishes a meaningful spatial buffer between the high-intensity Freeway corridor to existing communities to the east. Employment land uses will transition to commercial and multi-family land including defined perimeter landscape buffers and open spaces.

Following, is a brief explanation of each of the Land Use Categories.

#### **Employment Land Use Category**

The intent of the Employment Land Use Category is to provide opportunities for quality jobs within the Laveen Village. Examples of employment land uses include an EV-focused auto campus and/or a hospital and health care complex. The Employment Land Use Category is proposed along the Freeway.

**In the Employment Land Use Category, residential land uses are prohibited on 100 acres.**

#### **Multi-Family Land Use Category**

This land use category is meant to provide flexibility for multi-family dwelling units up to 40 dwelling units per acre. Height limitations and an overall maximum number of residential units across the entire Gila Foothills PUD

Site provide for a range of multi-family development types to support the employment uses. This may include “build-to-rent” developments of detached apartments that take on the appearance of a single-family homes, traditional garden-style apartments, podium style apartments which are built above parking, or apartments designed to “wrap” around structured parking.

#### **Commercial Land Use Category**

This land use category is located proximate to the arterial-arterial intersection of 51st Avenue and Estrella Drive. Commercial land uses here will provide for community services including a grocery store, retail shops, and a variety of restaurant options that are currently limited in the Laveen area.

#### **Resort Land Use Overlay**

This overlay will allow for resort uses to be developed within Development Units 9A, 9B, and 10 which are otherwise planned for Commercial and Multi-Family, and also Employment on Development Unit 10.

#### **Open Space Land Use Category**

Approximately 20 acres of various open space types are shown on the Land Use Plan. This includes over 7 acres of landscape buffers located along the Tierra Montana neighborhood, 55th Avenue, 51st Avenue, Estrella Drive, and 47th Avenue; an approximately 1-acre open space, and 12.8 acres where development is limited due to an existing WAPA easement.

Additional open spaces will be provided as part of the development of other land uses as detailed in the Development Standards.

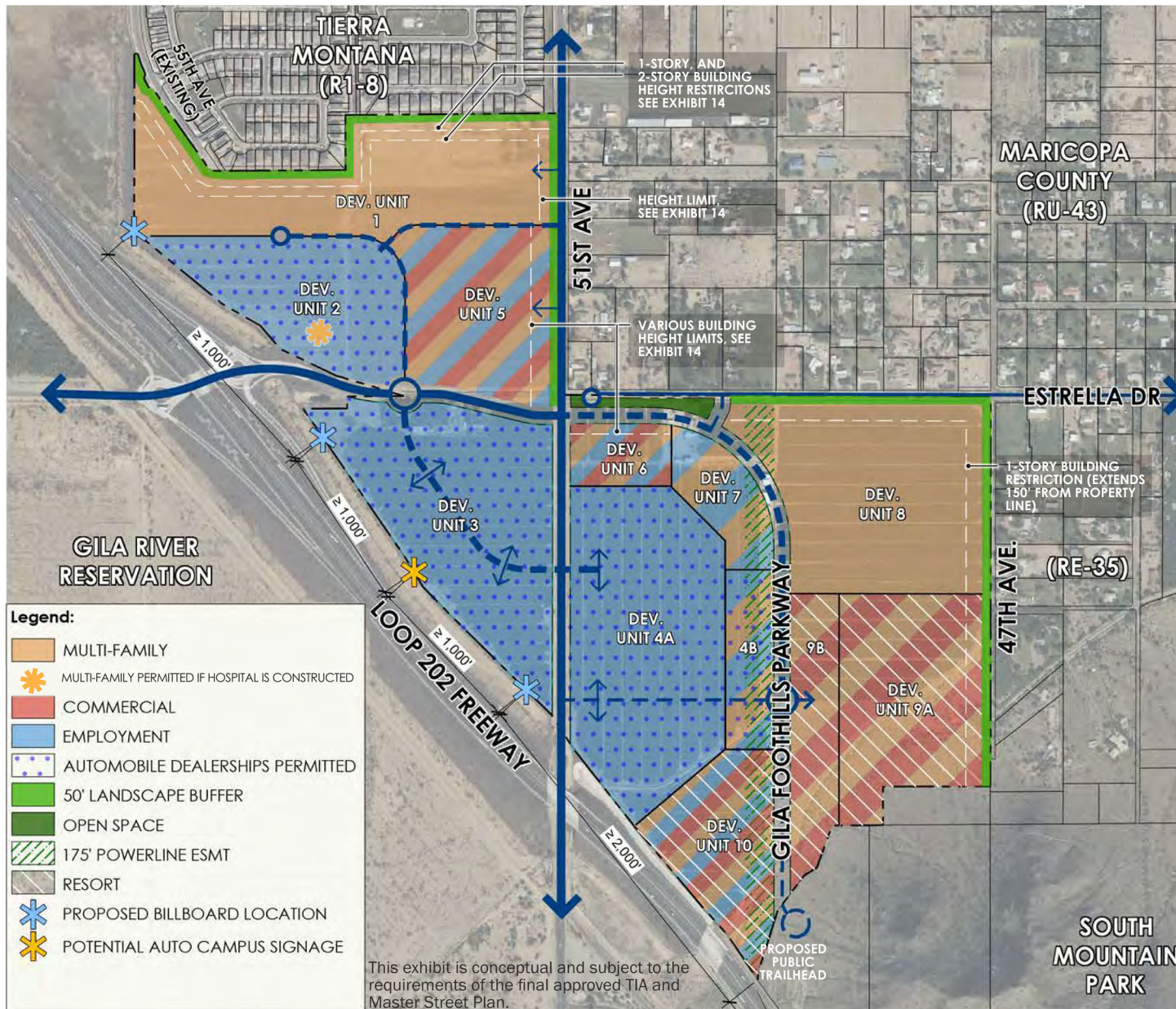


EXHIBIT 8 - Land Use Plan

N.T.S.

## 3.2 LAND USES PER DEVELOPMENT UNIT

As shown in Exhibit 8, each of the proposed land use categories extend across one or more Development Units. Each of these Development Units responds to the characteristics which surround it by having a refined list of associated land uses.

Below, a list of permitted land uses is provided below for each Development Unit. Land uses not included as permitted are prohibited unless determined to be analogous to a permitted land use. An interpretation of analogous uses from the City of Phoenix Zoning Administrator may be requested by a developer or property owner. Analogous uses may be approved administratively by the Zoning Administrator.

## PERMITTED LAND USES

### Development Unit 1

The following uses shall be permitted within Development Unit 1:

- Multifamily.
- All types of office.

### Development Units 2, 3, and 4A

The following uses shall be permitted by-right or with additional standards described below within Development Units 2, 3 and 4A:

- All uses within the C-2 District per Section 623.D of the Phoenix Zoning Ordinance.
- All types of office.
- Motor Vehicle Service and Repair as a standalone use or in conjunction with Vehicle Sales.
- Parking Structure.
- Self Service Storage (Limited to a maximum total of two (2) across the entire PUD site).
- Travel Trailer, Camper Sales.
- Off-premise signage (billboards) subject to Section 705.
- Boat sales.
- Motorcycles sales.



- Automobile Service Station / Gas Station, subject to the following:
  - A total maximum of one (1) Automobile Service Station shall be permitted across the PUD site.
  - The design of buildings, structures, and walls shall be architecturally coordinated and follow the design guidelines of the Character Area they are located within.
  - The design of canopies located over the fueling/charging stations shall be architecturally consistent with the design of the building and shall not be dominated by branded colors or signage. Canopy ceiling lights shall be recessed within or flush with the ceiling.
  - The view of paved areas used for vehicle circulation, parking, or fueling/charging shall be screened from the right-of-way by a minimum three (3) foot tall wall, berm, or alternative screening method. Where a screening wall is proposed, it shall be designed such that it is not conducive to sitting/loitering.
  - The layout and design of Automobile Service Stations/Gas Stations shall follow standards for Crime Prevention Through Design (CPTD).
  - Service areas, storage areas and refuse enclosures must be oriented away from public view and screened from adjacent sites. They shall also be secured sufficiently to deter vandalism or theft and be maintained in a clean and orderly manner.
  - ATMs should be located within the primary retail building when possible. Freestanding and/or exterior wall mounted ATMs are discouraged.
  
- Car Wash, subject to the following:
  - Car washes may not be located within 300 feet from residential uses or zoning districts. This distance shall be measured from the point of the queuing lane closest, building, vacuums, or parking space located closest to the residential district zoning line.
  - A screening wall and landscaping shall be used to screen the view of payment, lighting, circulation area, interior of the car wash building, vacuums, and other similar site appurtenances from any residential use and the public right-of-way.
  - Car Wash buildings must appear to be architecturally enclosed and include doors where vehicles enter and exit the building
  
- Drive-through facilities as an accessory use to a restaurant, subject to the following:
  - Access to the site is to be from an arterial or collector street as defined on the street classification map.

- Securing a use permit if the queuing lane for the drive-through facility is less than 300 feet from a residential district zoning line. This distance shall be measured from the point of the queuing lane closest to the residential district zoning line.
  - All drive through lanes shall be internal to the Site. If portions of a drive-through lane are visible to an arterial street, it shall be screened by a minimum three (3) foot tall wall, berm, or alternative screening method.
  - Landscaping shall screen drive-through or drive-in aisles, payment, and pick-up windows from any residential use and shall be used to minimize the visual impact of menu boards, and directional signs.
  - Pedestrian walkways should avoid intersecting the drive-through drive aisles, but where they do, they shall have clear visibility, and they must be emphasized by enriched paving, such as products similar to stamped concrete, pavers, stone, etc.
  - Drive-through uses within an integrated shopping center shall have an architectural style consistent with the theme established in the center.
  - The architecture of any drive-through use must provide compatibility with surrounding uses in form, materials , colors, scale, etc.
- Restaurants, Bars, and Cocktail Lounges, subject to the following:
    - Patron dancing shall be permitted byright.
    - Outdoor recreation uses, outdoor dining, and outdoor alcoholic beverage consumption shall be permitted as accessory uses only upon securing a Use Permit, if within 500 feet of a residential district zoning line. This distance shall be measured from the exterior wall of the building or portion thereof in which the business is conducted, or proposed use is to be conducted closest to the residential district zoning line.
    - The noise level, measured at any point on the received property, shall not exceed 55 dBa unless a Use Permit is obtained. An occurrence where the sound level increases up to 60 dBa for five continuous seconds or less shall not be deemed a violation of this section so long as there are no more than five occurrences within an hour long interval.
    - Music or entertainment shall be permitted subject to the following:
      - The stage or performance area shall be a maximum of 80 square feet unless a Use Permit is obtained.
- All permitted uses within the Commerce Park, General Commerce Park option (CP-GCP) per Section 626 of the Phoenix Zoning Ordinance, unless otherwise modified or prohibited below:
    - There shall be a maximum of 1 dock door per 20,000 square feet of industrial use to ensure distribution and wholesaling are not a primary use.

- Distribution uses are only allowed as an accessory use, unless related to auto sales.
- Wholesaling and Distribution as primary uses shall be prohibited, unless related to auto sales.
- Automobile Dealership and Retail Sales and Rental subject to the following standards.
  - Body repair and painting done on the lot shall be confined to a closed building.
  - Area used for parking, display, storage, or circulation shall be dust free.
  - Lots shall be properly enclosed with a wall, fence, hedge, plantings, or combinations thereof having a minimum height of 10 inches above the grade at which cars are to be displayed unless a hedge, planting or combinations thereof are approved in lieu of the ten-inch wall and/or the lot is depressed below the elevation of adjacent lots in which case appropriate landscaping may be approved.

## Development Unit 4B

The following uses shall be permitted by-right or with additional standards described below within Development Unit 4B:

- All uses within the C-2 District per Section 623.D of the Phoenix Zoning Ordinance.
- All types of office.
- Multifamily.
- All permitted uses within the Commerce Park, General Commerce Park option (CP-GCP) per Section 626 of the Phoenix Zoning Ordinance, unless otherwise modified or prohibited below:
  - There shall be a maximum of 1 dock door per 20,000 square feet of industrial use to ensure distribution and wholesaling are not a primary use.
  - Distribution uses are only allowed as an accessory use.
  - Wholesaling and Distribution as primary uses shall be prohibited.
- Automobile Dealership and Retail Sales and Rental subject to the following standards.
  - Body repair and painting done on the lot shall be confined to a closed building.
  - Area used for parking, display, storage, or circulation shall be dust free.
  - Lots shall be properly enclosed with a wall, fence, hedge, plantings, or combinations thereof having a minimum height of 10 inches above the grade at which cars are to be displayed unless a hedge, planting or combinations thereof are approved in lieu of the ten-inch wall and/or the lot is depressed below the elevation of adjacent lots in which case appropriate landscaping may be approved.
  - Any artificial light used for such lot or area shall be so arranged as to reflect light away from the adjoining premises, lot/and or streets and shall be located as not to be a nuisance to adjacent property and streets.



## Development Units 5 and 6

The following uses shall be permitted by-right within Development Unit 5 and 6:

- All uses within the C-2 District per Section 623.D of the Phoenix Zoning Ordinance.
- All types of office.
- Multifamily
- Automotive Service Station / Gas Station
  - A total maximum of one (1) Automobile Service Station shall be permitted across the PUD site.
  - The design of buildings, structures, and walls shall be architecturally coordinated and follow the design guidelines of the Character Area they are located within.
  - The design of canopies located over the fueling/charging stations shall be architecturally consistent with the design of the building and shall not be dominated by branded colors or signage. Canopy ceiling lights shall be recessed within or flush with the ceiling.
  - The view of paved areas used for vehicle circulation, parking, or fueling/charging shall be screened from the right-of-way by a minimum three (3) foot tall wall, berm, or alternative screening method. Where a screening wall is proposed, it shall be designed such that it is not conducive to sitting/loitering.
  - The layout and design of Automobile Service Stations/Gas Stations shall follow standards for Crime Prevention Through Design (CPTD).
  - Service areas, storage areas and refuse enclosures must be oriented away from public view and screened from adjacent sites. They shall also be secured sufficiently to deter vandalism or theft and be maintained in a clean and orderly manner.
  - ATMs should be located within the primary retail building when possible. Freestanding and/or exterior wall mounted ATMs are discouraged.

## Development Unit 7

The following uses shall be permitted by-right within Development Unit 7:

- Multifamily.
- All uses within the C-2 District per Section 623.D of the Phoenix Zoning Ordinance.
- All types of office.

## Development Unit 8

The following uses shall be permitted by-right within Development Unit 8:

- Multifamily.
- All types of office.

## Development Unit 9A

The following uses shall be permitted by-right within Development Unit 9A:

- Multifamily.
- All types of office.
- All uses within the RH District per Section 629 of the Phoenix Zoning Ordinance.

## Development Unit 9B

The following uses shall be permitted by-right or with additional standards described below within Development Unit 9B:

- Multifamily.
- All types of office.
- Alcohol Sales.
- All uses within the RH District per Section 629 of the Phoenix Zoning Ordinance.
- All uses within the C-1 District per Section 622 of the Phoenix Zoning Ordinance.
- Restaurants, Bars, and Cocktail Lounges, subject to the following:
  - The noise level, measured at any point on the received property, shall not exceed 55 dBa unless a Use Permit is obtained. An occurrence where the sound level increases up to 60 dBa for five continuous seconds or less shall not be deemed a violation of this section so long as there are no more than five occurrences within an hour long interval.
  - Patron dancing shall be permitted byright.
  - Outdoor recreation uses, outdoor dining, and outdoor alcoholic beverage consumption shall be permitted as accessory uses only upon securing a Use Permit, if within 500 feet of a residential district zoning line. This distance shall be measured from the exterior wall of the building or portion thereof in which the business is conducted, or proposed use is to be conducted closest to the residential district zoning line.
  - Music or entertainment shall be permitted subject to the following:
    - The stage or performance area shall be a maximum of 80 square feet unless a Use Permit is obtained.

## Development Unit 10

The following uses shall be permitted by-right or with additional standards described below within Development Unit 10:

- Multifamily.
- All types of office.
- Resort hotel.
- All uses within the RH District per Section 629 of the Phoenix Zoning Ordinance.
- All uses within the C-1 District per Section 622 of the Phoenix Zoning Ordinance.
- Alcohol Sales as an accessory use.
- Restaurants, Bars, and Cocktail Lounges, subject to the following:
  - The noise level, measured at any point on the received property, shall not exceed 55 dBa unless a Use Permit is obtained. An occurrence where the sound level increases up to 60 dBa for five continuous seconds or less shall not be deemed a violation of this section so long as there are no more than five occurrences within an hour long interval.
  - Patron dancing shall be permitted byright.
  - Outdoor recreation uses, outdoor dining, and outdoor alcoholic beverage consumption shall be permitted as accessory uses only upon securing a Use Permit, if within 500 feet of a residential district zoning line. This distance shall be measured from the exterior wall of the building or portion thereof in which the business is conducted, or proposed use is to be conducted closest to the residential district zoning line.
  - Music or entertainment shall be permitted subject to the following:
    - The stage or performance area shall be a maximum of 80 square feet unless a Use Permit is obtained.



## PROHIBITED LAND USES

Additional land uses are further prohibited within all Development Units or within specific Development Unit(s) as listed below:

### All Development Units

The following uses shall be prohibited within all Development Units:

- Wholesaling, unless related to vehicle sales.
- Distribution, unless related to vehicle sales.
- Adult bookstore, adult novelty store, adult theatre, adult live entertainment establishment, or erotic dance or performance studio.
- Travel Center (Truck Stop)  
*A Travel Center/Truck Stop is defined as a building or buildings providing facilities used for commercial truck drivers during stopovers at the facility and may include overnight parking, maintenance of semi-trucks or commercial vehicles, showering facilities and laundry facilities.*

### Development Units 9B and 10

The following uses shall be prohibited within Development Units 9B and 10:

- Restaurants with a drive-thru

### Development Units 1-4b and 7-10

The following uses shall be prohibited within Development Units 1, 4B and 7-10:

- Automotive Service Station

### Development Units 2-4a

The following uses shall be prohibited within Development Units 2-4a:

- Residential dwelling units, with the exception of those permitted in Development Unit 2 following the construction of a hospital.

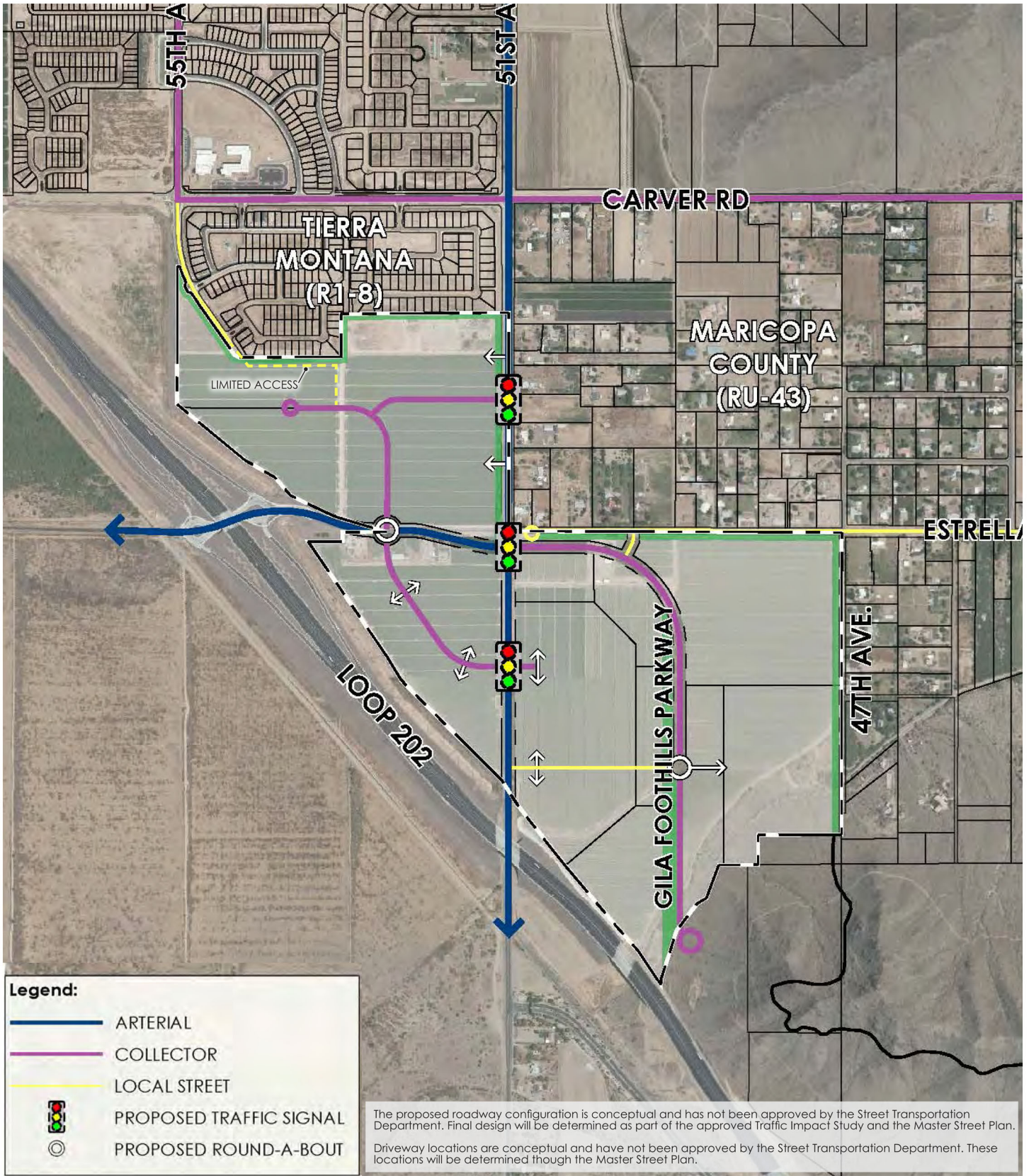


EXHIBIT 9 - Conceptual Circulation Plan

N.T.S.



### 3.3 ROADWAY CIRCULATION

There are graphics within the PUD that illustrate the intent of streetscape landscaping and street cross sections. These graphics shall be treated as conceptual and for illustrative purposes only. Final determination of these details will be determined by the Streets Transportation Department at the time of the Master Street Plan and site plan submittals.

The circulation of the Site has been thoughtfully designed to protect the rural character of surrounding residential neighborhoods to the northeast and the suburban neighborhoods to the northwest. A traffic circle with monumentation is proposed to contribute to a sense of arrival as soon as vehicles exit the Freeway. Collector streets are proposed to facilitate commercial traffic from the existing arterial alignments of Estrella Drive (West of 51<sup>st</sup> Avenue) and 51<sup>st</sup> Avenue. The anticipated design of each street is outlined.

#### 51<sup>st</sup> Avenue

Half street improvements meeting the City of Phoenix typical arterial street section standards will be provided on the western half of 51<sup>st</sup> Avenue for the segment north of Estrella Drive. These improvements will generally match existing half-street improvements already constructed in association with Tierra Montana to the north. South of Estrella Drive, 51<sup>st</sup> Avenue will ultimately be widened to a full four-lane roadway in conformance with typical City of Phoenix standards.

#### 47<sup>th</sup> Avenue

The goal of this PUD is to preserve the existing residential character of 47<sup>th</sup> Avenue. No points of vehicular ingress or egress associated with the Gila Foothills community (except for emergency access required by the City of Phoenix) will be constructed on 47<sup>th</sup> Avenue. To further protect this neighborhood street, a 50-foot landscape buffer is proposed along the west side of 47<sup>th</sup> Avenue.

#### Estrella Drive, Gila Foothills Parkway, & County Lane

One of the primary goals of the Gila Foothills planned circulation network is to ensure that traffic impacts to adjacent neighborhoods are diminished to the greatest extent possible. The proposed Circulation plan seeks to protect existing properties that front onto Estrella Drive east of 51<sup>st</sup> Avenue by eliminating through traffic movements along this corridor.

The current 51<sup>st</sup> Avenue and Estrella Drive intersection will be shifted south, effectively disconnecting the east-west traffic pattern. East of the intersection, Estrella Drive will transition into the new Gila Foothills Parkway alignment that will curve southward, ultimately connecting to the proposed public trailhead.

A small north-south connecting roadway named County Lane will be built in conjunction with the Gila Foothills Parkway improvements and will provide a local-only linkage between Gila Foothills Parkway and the existing Estrella Drive (the section east of 51<sup>st</sup> Avenue) to maintain existing resident access to both 51<sup>st</sup> Avenue and the Freeway.



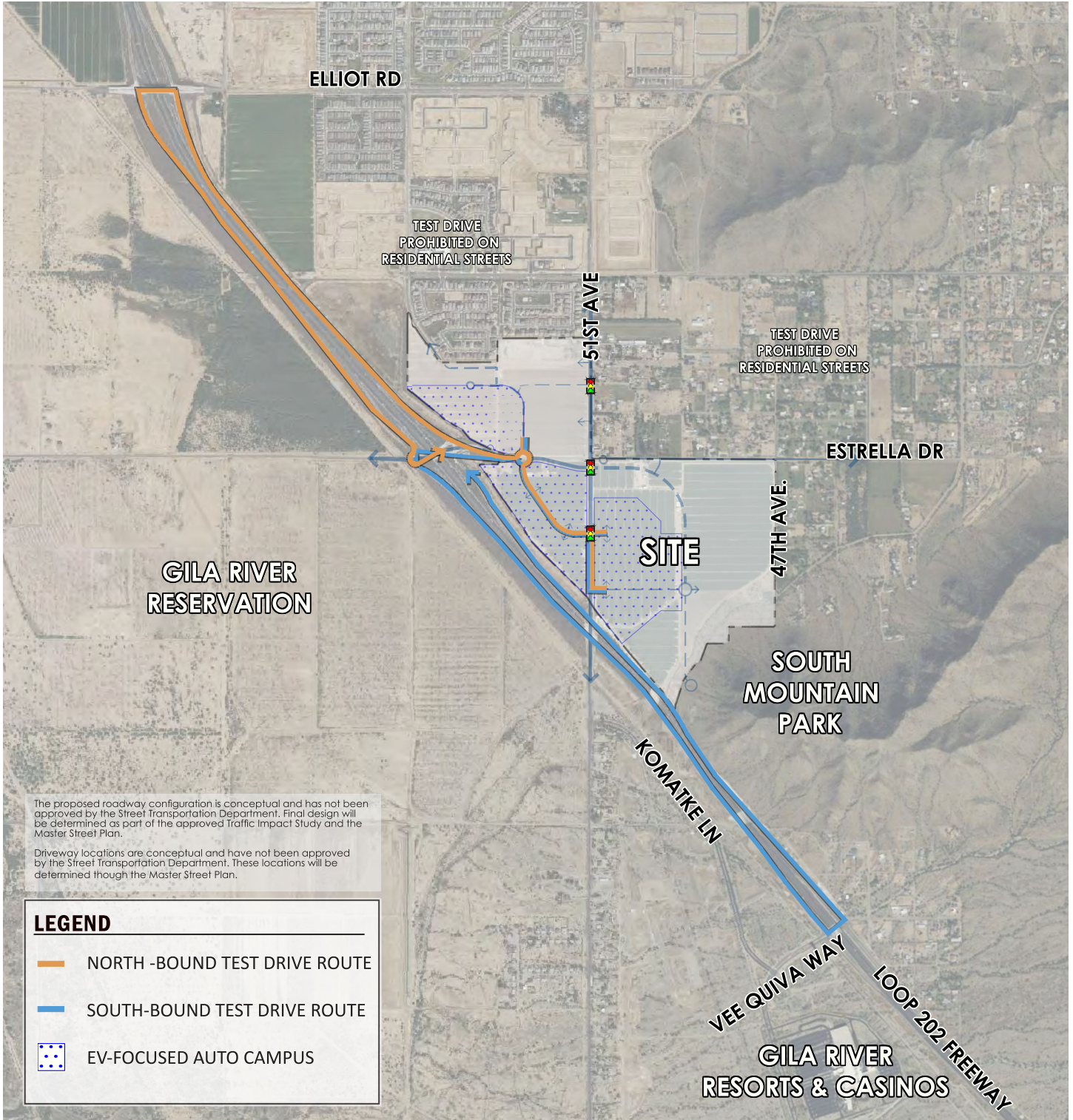


EXHIBIT 10 - Test Drive Routes

N.T.S.

## Test Drive Routes

Routes outside of residential areas have been specifically identified for customers to test drive vehicles. As shown on Exhibit 10: Test Drive Routes, vehicles will travel from the EV-focused auto campus directly on to the Freeway using Estrella Drive. Once on the Freeway, vehicles will travel north or south to turn around at Vee Quiva Way or Elliot Road to travel back to the Site via the Freeway. Following these routes, vehicles will avoid traveling through residential areas.

Test drives will not be permitted on Estrella Drive east of the roundabout, 51<sup>st</sup> Avenue north of Estrella Drive, or on 55<sup>th</sup> Avenue north of the Site.



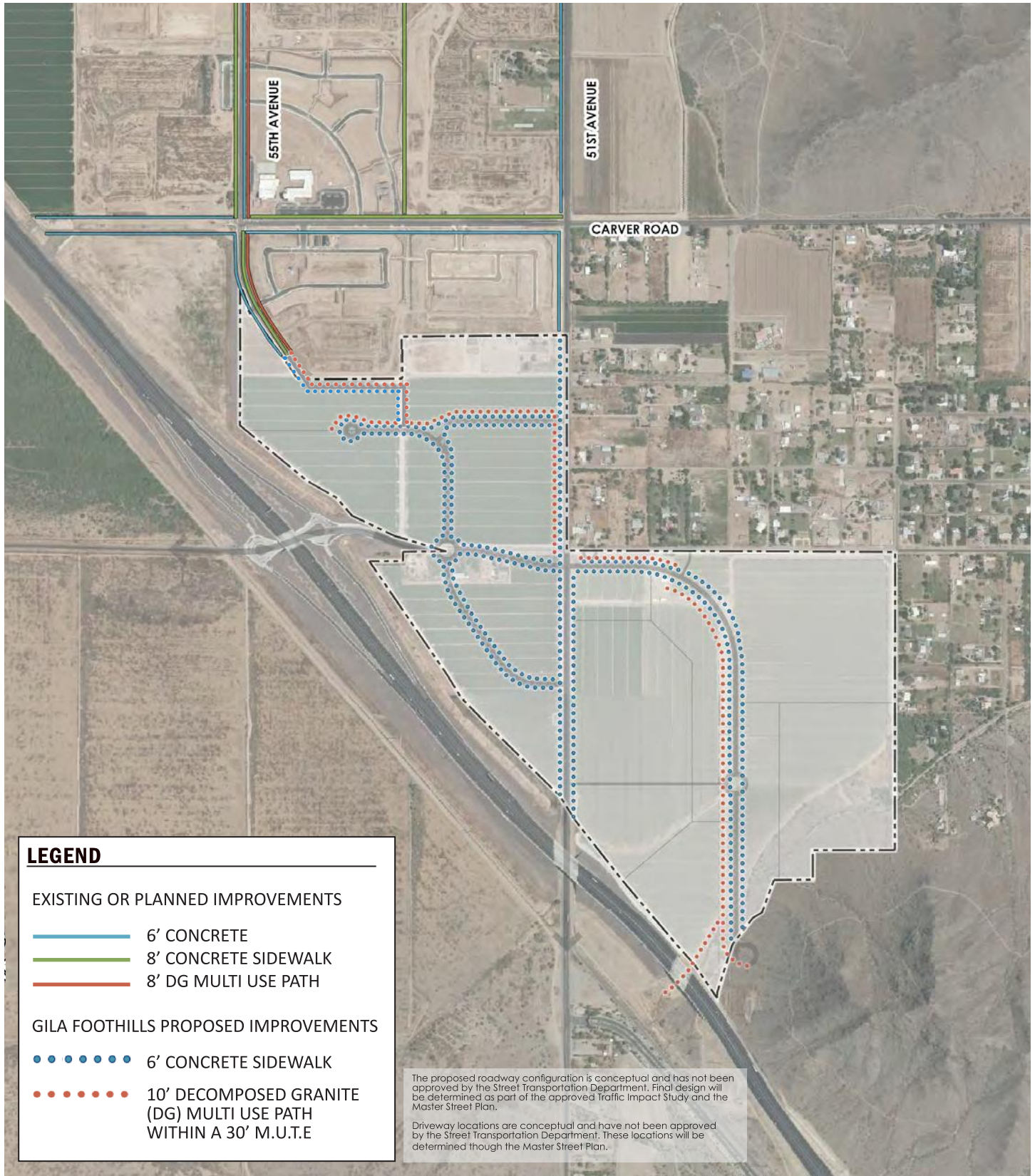


EXHIBIT 11 - Conceptual Trail Plan

N.T.S.



## PEDESTRIAN AND BICYCLE CIRCULATION

Pedestrian circulation and connectivity are essential components of the PUD. The pedestrian and bicycle circulation system will provide safe non-vehicular travel routes highlighted by a planned north-south multi-use trail corridor that will connect from existing improvements in Tierra Montana to the public trailhead proposed at the southern edge of the community. Additionally, the following are the standards relating to pedestrian and bicycle circulation:

- The pathway systems shall provide direct connections between the following Site elements: sidewalks, primary and secondary amenities, all non-residential building entrances and exits, where pedestrian and bike paths cross drive aisles, bicycle facilities and bus stops.
- A system of clearly defined accessible pedestrian and bicycle pathways shall be provided. These pathways will be concentrated in The District Character Area and The Foothills Character Area.
- Enhanced surfacing materials, such as stamped or colored concrete or other pavement treatments, that visually contrast with the adjacent parking and drive aisle surfaces shall be provided to delineate all areas where pedestrian paths cross drive aisles.
- Pedestrian and bicycle circulation is to be directed toward the enhanced pedestrian corridor running along Gila Foothills Parkway to the proposed public trailhead at South Mountain Park.

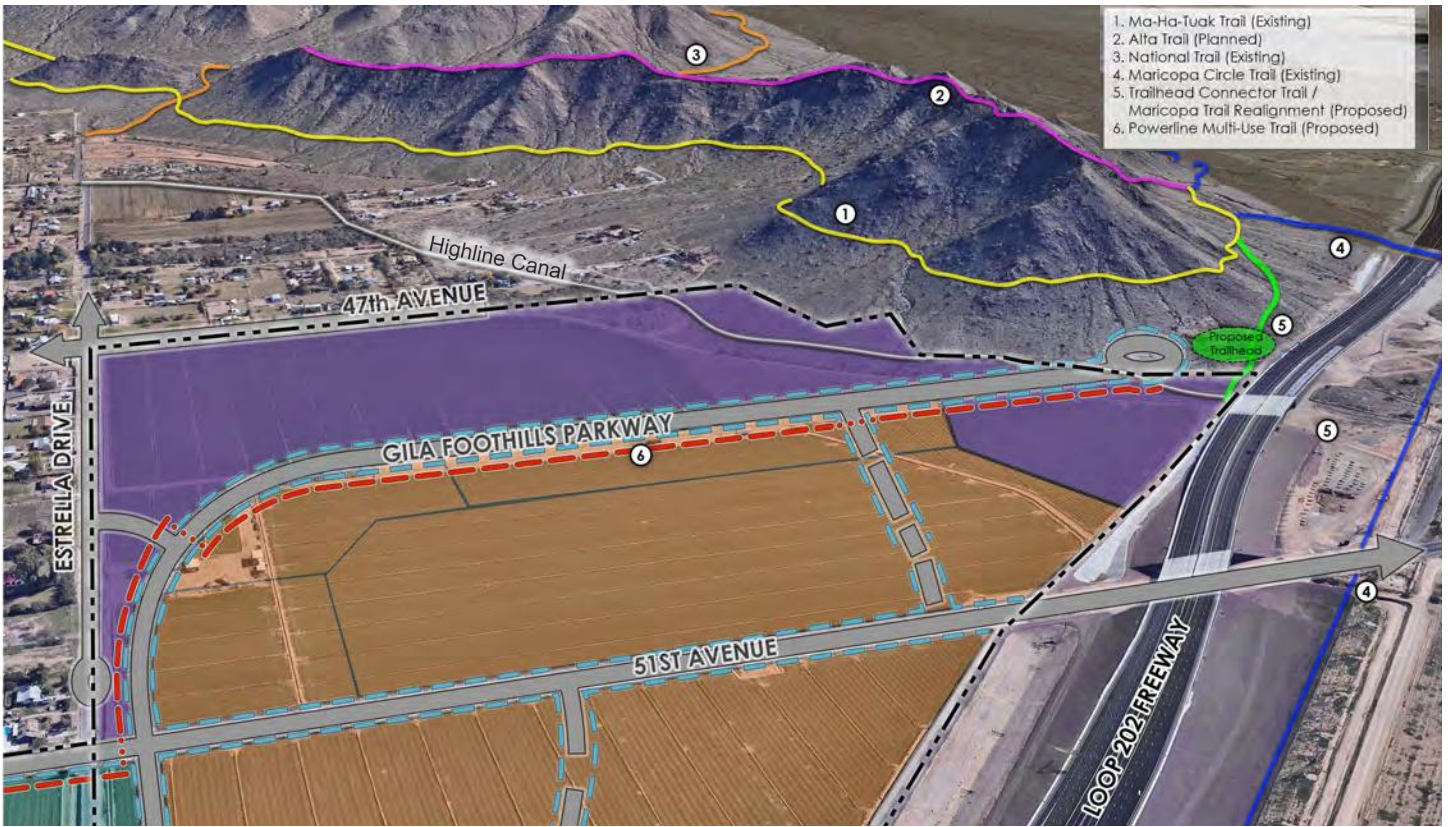


EXHIBIT 12 - Conceptual Trailhead Connectivity

N.T.S.

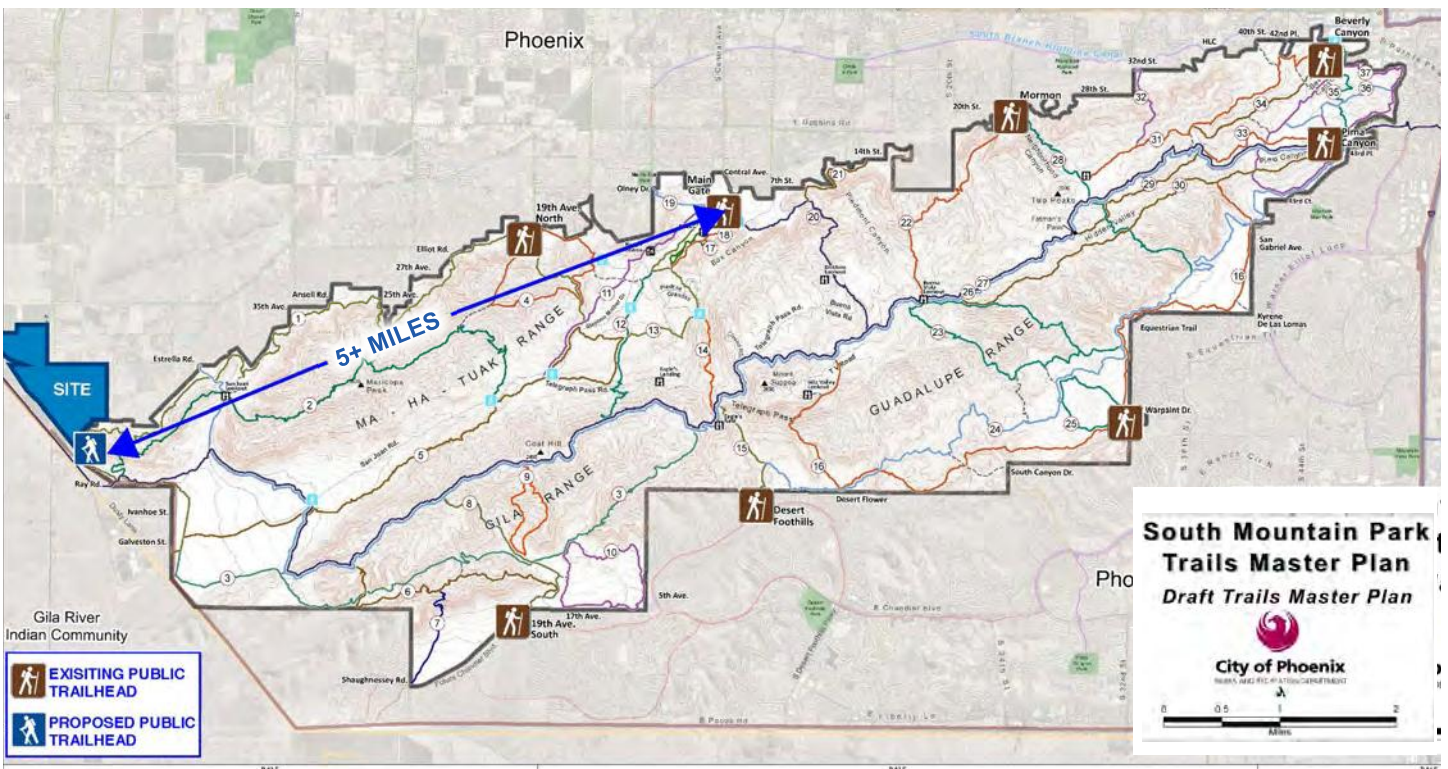


EXHIBIT 13 - Existing and Planned Trailheads

N.T.S.



## Proposed Public Trailhead and Connectivity to South Mountain

The unique location of the PUD near South Mountain Park will provide opportunities for trail connections to a network of existing and future trails and paths that currently cannot be accessed from Laveen.

In cooperation with the City of Phoenix Parks and Recreation Department an analysis has been completed to help identify a location for a public trailhead at the base of South Mountain as part of the PUD that provides much needed access to the western portion of the Park.

This public trailhead will provide the opportunity to connect the existing and planned trails within the South Mountain trail network including the following trails:

- Ma-Ha-Tuak Trail (Existing)
- Alta Trail (Planned)
- National Trail (Existing)
- Maricopa Circle Trail (Existing)
- Trailhead Connector Trail/Maricopa Trail Realignment (Proposed)
- Powerline Multi-Use Trail (Proposed)

This PUD identifies a location on Exhibit 8: Land Use Plan as well as providing Exhibit 12: Conceptual Trailhead Connectivity



General Location of Proposed Public Trailhead



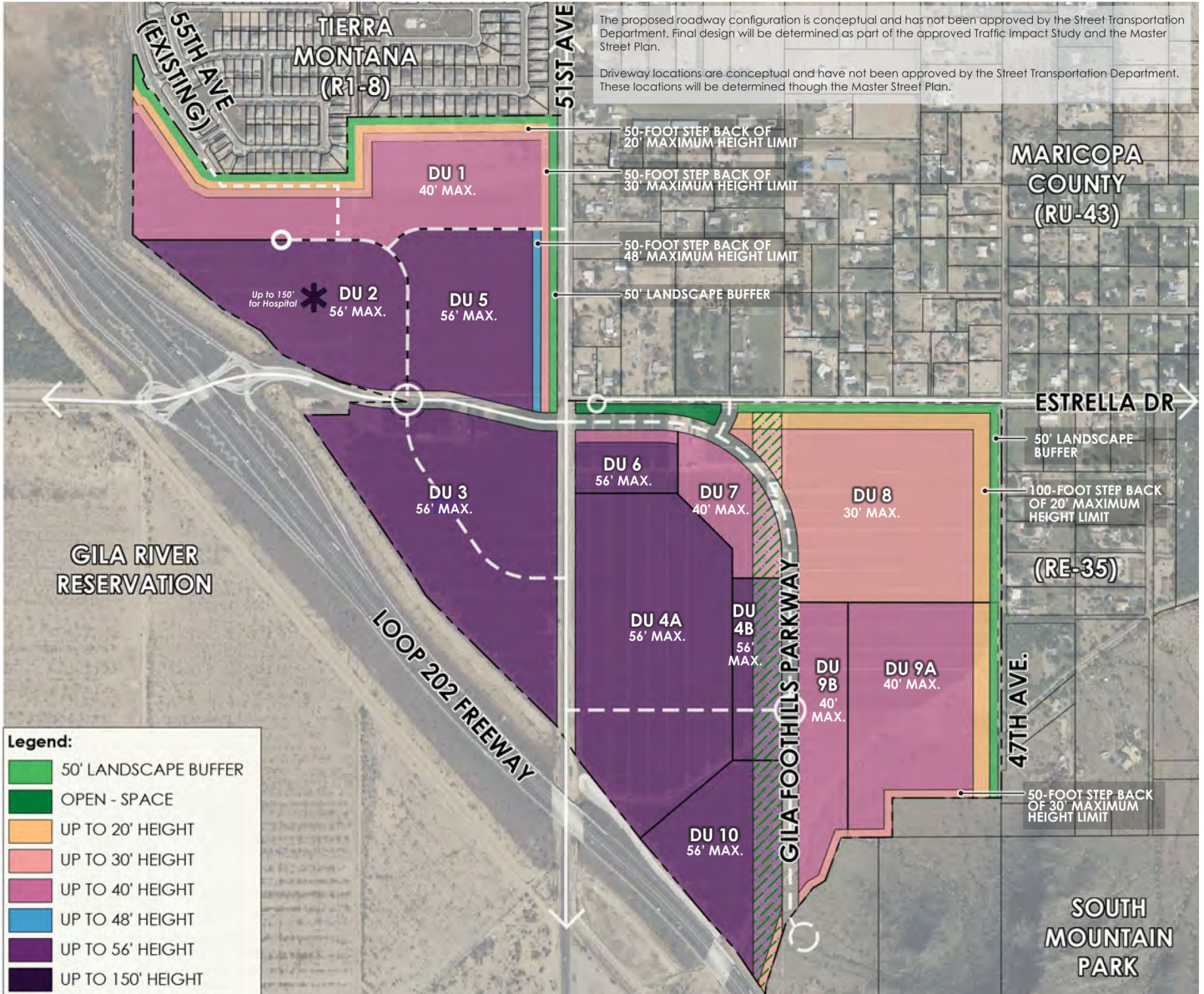


EXHIBIT 14 - Building Height

N.T.S.

### 3.4 BUILDING HEIGHT

This PUD increases the setback distance from existing communities to portions of the Site that allow for greater building heights, limits maximum building heights in The Foothills and The District Character Areas, and concentrates allowances for greater building heights in the Hub Character Area to support employment and health care uses. See Exhibit 14, Building Height.

In The Foothills Character Area, no buildings will be permitted within the first 50-feet extending from the property line adjacent to Estrella Drive and 47<sup>th</sup> Avenue. Beyond this 50-foot landscape buffer, building heights are limited to a maximum of one-story and 20-feet in height for the following 50 feet extending into the site from the landscape buffer. Beyond 100 feet from the property line, buildings may be up to 30 to 40 feet in height as shown in Exhibit 14.

In The District Character Area, no buildings will be permitted within the first 50-feet from 51<sup>st</sup> Avenue or from the Tierra Montana neighborhood. Building heights will be limited to a maximum of 20-feet where they are located between 50 and 100-feet from 51<sup>st</sup> Avenue or the Tierra Montana neighborhood. Beyond 100-feet, the maximum building height will be 40-feet in Development Unit 1, and up to 56-feet in Development Unit 5, where employment and commercial uses are permitted at the arterial intersection of Estrella Drive and 51<sup>st</sup> Avenue.

In The Hub Character Area where employment land uses including but not limited to an EV-focused auto campus and/or hospital are proposed, buildings may be a maximum of 56-feet in height. To further encourage the development of a hospital, a hospital located in Development Unit 2 may exceed the maximum 56-foot building height up to a maximum of 150-feet. Maximum building heights and limitations are detailed further in the Development Standards.

### 3.5 LANDSCAPE BUFFERS

The Site is surrounded by a variety of land uses including detached single-family homes, suburban residential lots, South Mountain Park, arterial roadways, and the Ed Pastor Loop 202 Freeway. To separate or transition between land uses, landscape buffers are proposed between the Site and surrounding existing development. As illustrated in Exhibit 14, a 50-foot landscape buffer is proposed along Estrella Drive, 47<sup>th</sup> Avenue, 51<sup>st</sup> Avenue north of Estrella Drive, and the northern property line adjacent to the Tierra Montana neighborhood.

These 50-foot landscape buffers will be planted with a variety of shrubs, ground cover, and trees to provide an enhanced landscape aesthetic that will provide visual open space and shade along adjoining paths or sidewalks. Where the buffer is adjacent to a right-of-way, pedestrian facilities proposed as part of streetscape improvements may meander into landscape buffers so long as they are located within a Multi-Use Trail Easement (“MUTE”) as required by the City of Phoenix.



Public art integrated into the enhanced design of walls, trail features, and ornamental shade features



### 3.6A PUBLIC ART COMPONENT

To further compliment the proposed development, a minimum of one (1) public art project that pays homage to Laveen Village shall be provided within each Character Area that is visually and physically accessible by the general public. Additionally, other community placemaking improvements are encouraged within the Site to further establish a unique character that is supported by the community. This conceptual imagery shows multiple examples of possible public art installations within The Site.



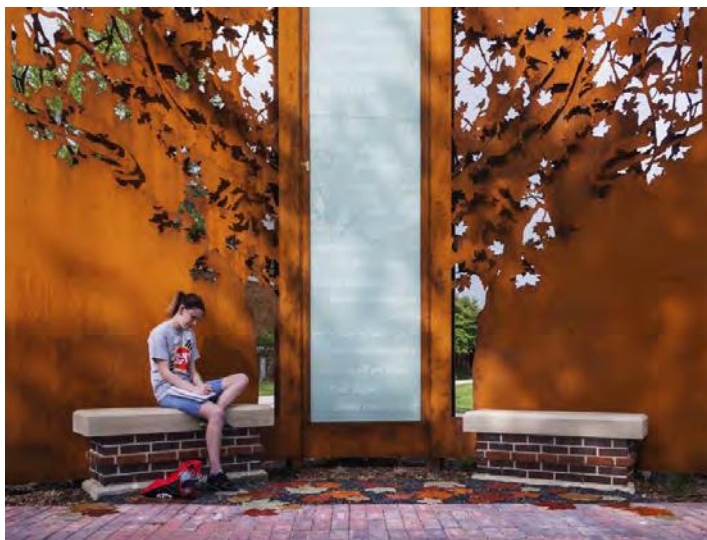
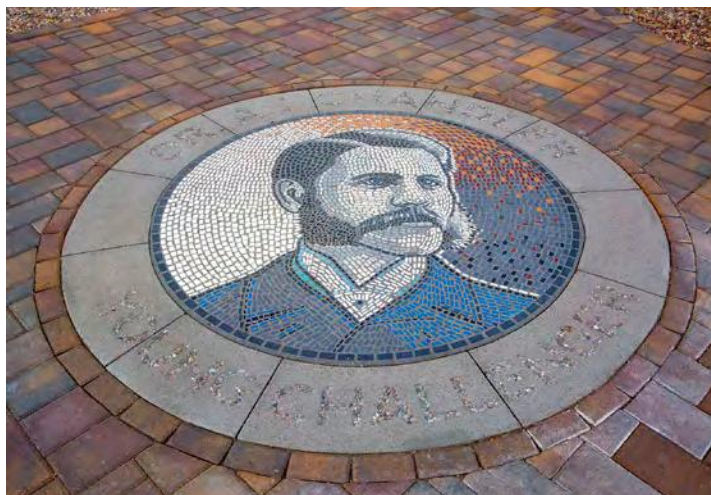
Public art reflective of the agrarian history of an area



Hand-scaled public art







Commemorative public art

Functional public art



### 3.6B KEY OUTDOOR AREAS AND IMPROVEMENTS

A variety of outdoor areas will be provided as part of this project in the form of areas reserved for public art, improvements and landscaping within active and passive outdoor areas, as well as a public trailhead at South Mountain Park. While not all of these areas count towards the City’s calculated minimum open space requirements, they provide a substantial enhancement to the site and surrounding residents. The exhibit below conceptually shows the type and location of these areas.

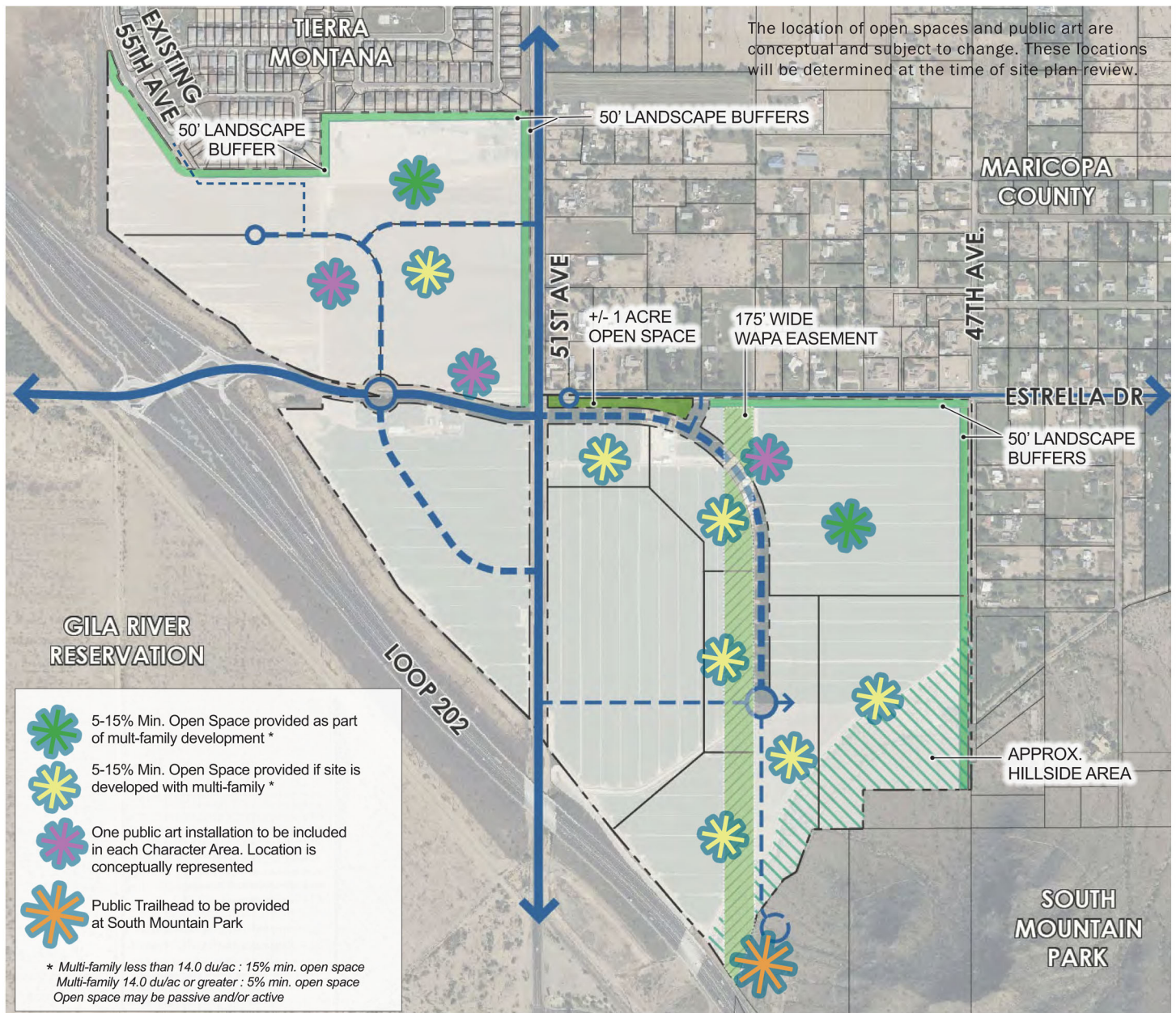


EXHIBIT 15 - Conceptual Outdoor Areas and Improvements Plan

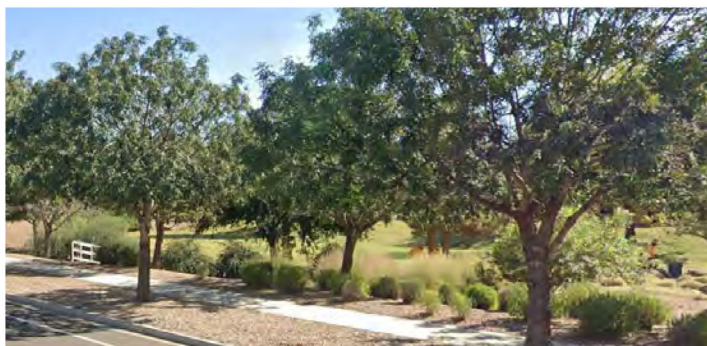
N.T.S. ⓘ

The proposed roadway configuration is conceptual and has not been approved by the Street Transportation Department. Final design will be determined as part of the approved Traffic Impact Study and the Master Street Plan.





50' LANDSCAPE BUFFERS ALONG 47TH AVE., ESTRELLA DR., 51ST AVE., AND THE TIERRA MONTANA NEIGHBORHOOD



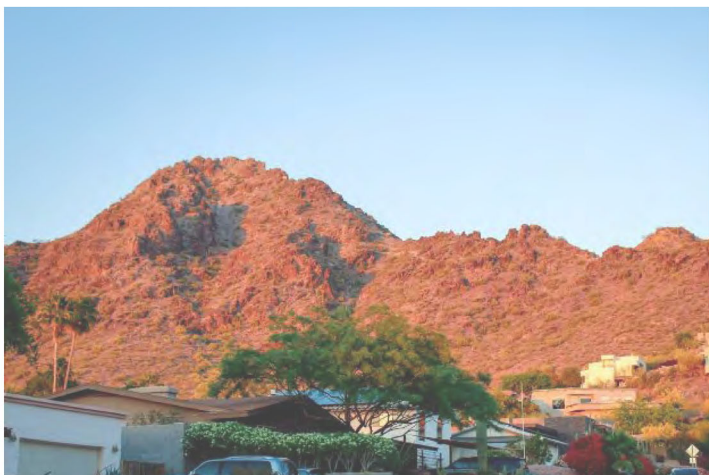
+/- 1 ACRE OPEN SPACE



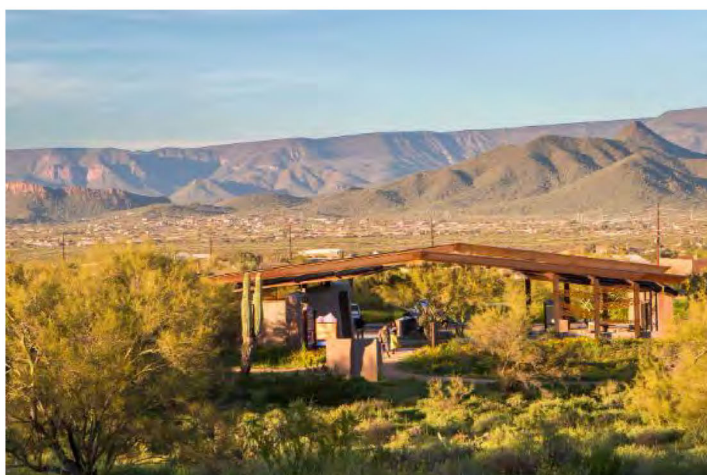
CONCEPTUAL EXAMPLES OF OPEN SPACE WITHIN MULTI-FAMILY DEVELOPMENTS



SHADED MULTI-USE TRAILS



PRESERVED HILLSIDE AREAS



PUBLIC TRAILHEAD AT SOUTH MOUNTAIN



## 4. DEVELOPMENT STANDARDS

Development Standards regulate development, protect surrounding neighborhoods, and preserve the public welfare.

The Development Standards for Gila Foothills generally adhere to the development standards outlined within the City of Phoenix Zoning Ordinance.

The following tables identify the minimum Development Standards prescribed to each Development Unit.

Unless specifically addressed in the following tables, development standards shall conform to the C-2 zoning district (Section 623) development and landscape standards.

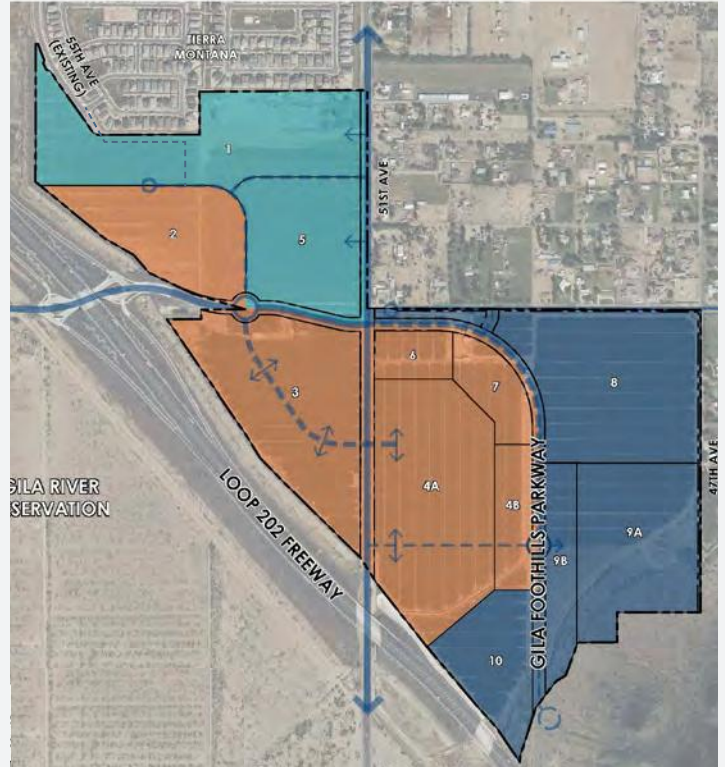


Exhibit 7 is provided above for reference when reviewing Development Standards.

The Development Standards Tables on the following pages identify minimum or maximum criteria for each Development Unit. The tables include column colors which correspond to the respective Character Areas shown above.

## DEVELOPMENT STANDARDS TABLE 1: BUILDING SETBACKS

Character Areas:		DISTRICT		HUB						FOOTHILLS			
Development Units :		1	5	2	3	4A	4B	6	7	8	9A	9B	10
Minimum Building Setback, Adjacent to:	Estrella Drive	NA	25'	25'	25'	NA	NA	NA	NA	50'	NA	NA	NA
	51st Ave	50'	50'	NA	25'	25'	NA	25'	NA	NA	NA	NA	NA
	55th Ave	25'	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA
	47th Avenue	NA'	NA	NA	NA	NA	NA	NA	NA	50'	50'	NA	NA
	Ed Pastor Loop 202 Freeway	NA	NA	15'	15'	15'	NA	NA	NA	NA	NA	NA	15'
	Gila Foothills Parkway	NA	NA	NA	NA	NA	25'	15'	15'	15'	NA	15'	15'
	Property line not adjacent to a public street	5'	5'	5'	5'	5'	5'	5'	5'	5'	5'	5'	5'
	Property line adjacent to interior public or private street	15'	15'	15'	15'	15'	15'	15'	15'	15'	15'	15'	15'
	Property line shared with Tierra Montana Neighborhood	50'	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA



## DEVELOPMENT STANDARDS TABLE 2: LANDSCAPE SETBACKS

Character Areas:		DISTRICT		HUB						FOOTHILLS				
Development Units :		1	5	2	3	4A	4B	6	7	8	9A	9B	10	
Landscape Setback Adjacent to:	Estrella Drive	NA	15' avg. per site plan, min. 5'	15' avg. per site plan, min. 0'	15' avg. per site plan, min. 0'	NA	NA	NA	NA	15' avg. per site plan, min. 5'	NA	NA	NA	
	Gila Foothills Parkway	NA	NA	NA	NA	NA	15' avg. per site plan, min. 0'	15' avg. per site plan, min. 0'	15' avg. per site plan, min. 0'	15' avg. per site plan, min. 5'	NA	15' avg. per site plan, min. 5'	15' avg. per site plan, min. 5'	
	51st Avenue	15' avg. per site plan, min. 5'	15' avg. per site plan, min. 5'	NA	15' avg. per site plan, min. 0'	15' avg. per site plan, min. 0'	NA	15' avg. per site plan, min. 0'	NA	NA	NA	NA	NA	
	55th Avenue	15' avg. per site plan, min. 5'	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	
	47th Avenue	NA	NA	NA	NA	NA	NA	NA	NA	15' avg. per site plan, min. 5'	15' avg. per site plan, min. 5'	NA	NA	
	Ed Pastor Loop 202 Freeway	NA	NA	10' avg. per site plan, 0' min.	10' avg. per site plan, 0' min.	10' avg. per site plan, 0' min.	NA	NA	NA	NA	NA	NA	10' avg. per site plan, min. 5'	
	Property Line not adjacent to public or private street	0'	0' unless adj. to existing residential, then 10'	0' unless adj. to existing residential, then 10'	0'	0' unless adj. to existing residential, then 10'	0' unless adj. to existing residential, then 10'	0' unless adj. to existing residential, then 10'	0' unless adj. to existing residential, then 10'	0' unless adj. to existing residential, then 10'	0' unless adj. to existing residential, then 10'	0' unless adj. to existing residential, then 10'	0' unless adj. to existing residential, then 10'	0' unless adj. to existing residential, then 10'
	Property line adjacent to interior public or private street	15' avg. per site plan, min. 5'	15' avg. per site plan, min. 5'	15' avg. per site plan, min. 0'	15' avg. per site plan, min. 0'	15' avg. per site plan, min. 0'	15' avg. per site plan, min. 0'	15' avg. per site plan, min. 0'	15' avg. per site plan, min. 0'	15' avg. per site plan, min. 0'	15' avg. per site plan, min. 5'	15' avg. per site plan, min. 5'	15' avg. per site plan, min. 5'	15' avg. per site plan, min. 5'
	Property line shared with Tierra Montana Neighborhood	15' avg. per site plan, min. 5'	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA

Footnote:

(1)  
 Vehicle Display Pedestals up to 5-feet in height may encroach into the landscape setback so long as they are no closer than 5-feet to the public ROW. Each vehicle display pedestal shall be no larger than 40' in dimension. Vehicle display pads shall not conflict with sight visibility triangles. Where vehicle display pedestals encroach into the landscape setback, they may not occupy more than 20% of the width of the parcel as measured at the landscape setback.

### DEVELOPMENT STANDARDS TABLE 3: LANDSCAPE BUFFERS

Character Areas:		DISTRICT		HUB						FOOTHILLS			
Development Units :		1	5	2	3	4A	4B	6	7	8	9A	9B	10
Landscape Buffer Adjacent to:	Estrella Drive	NA	NA	NA	NA	NA	NA	NA	NA	50'	NA	NA	NA
	51st Ave	50'	50'	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA
	47th Avenue	NA	NA	NA	NA	NA	NA	NA	NA	50'	NA	50'	NA
	Property line shared with Tierra Montana Neighborhood	50'	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA
<p>Note: Landscape Setbacks may be located within Landscape Buffers. The difference between the landscape setback and the landscape buffer can be counted towards open space.</p>													

### DEVELOPMENT STANDARDS TABLE 4: BUILDING HEIGHT

Character Areas:		DISTRICT		HUB						FOOTHILLS			
Development Units:		1	5	2	3	4A	4B	6	7	8	9A	9B	10
Maximum Buiding Height		40'	56'	56'(1)	56'	56'	56'	56'	40'	30'	40'	40'	56'
<p>Footnote: (1) Hospital uses may be up to 150-feet in height for up to 20% of the area of the Development Unit.</p>													



## DEVELOPMENT STANDARDS TABLE 5: BUILDING STEP BACK

Character Areas:		DISTRICT		HUB						FOOTHILLS			
Development Units:		1	5	2	3	4A	4B	6	7	8	9A	9B	10
Building Step Back, Adjacent to:	Estrella Dr	NA	NA	NA	NA	NA	NA	NA	NA	(1)	NA	NA	NA
	Gila Foothills Parkway	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA
	51st Ave	(6)	(2)	NA	NA	NA	NA	(5)	NA	NA	NA	NA	NA
	55th Ave	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA
	47th Ave	NA	NA	NA	NA	NA	NA	NA	NA	(1)	(1)	NA	NA
	Loop 202 Freeway	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA
	Property line shared with Tierra Montana Neighborhood	(4)	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA
	Property line shared with South Mountain Park	NA	NA	NA	NA	NA	NA	NA	NA	NA	(3)	(3)	(3)

Footnotes:

- (1) 20-foot and one-story maximum building height within 150 feet of the property line.  
(Buildings are not permitted within the 50-foot landscape buffer)
- (2) 30-foot maximum building height within 100 feet of the property line.  
48-foot maximum building height within 150' of the property line.  
(Buildings are not permitted within the 50-foot landscape buffer)
- (3) 30-foot maximum building height within 50' from the property line.
- (4) 20-foot and one-story maximum building height within 100 feet of the property line.  
30-foot and two-story maximum building height within 150 feet of the property line.  
(Buildings are not permitted within the 50-foot landscape buffer)
- (5) 40-foot maximum building height within 50' from the property line.
- (6) 30-foot maximum building height within 100 feet of the property line.  
(Buildings are not permitted within the 50-foot landscape buffer)

## DEVELOPMENT STANDARDS TABLE 6: MISCELLANEOUS

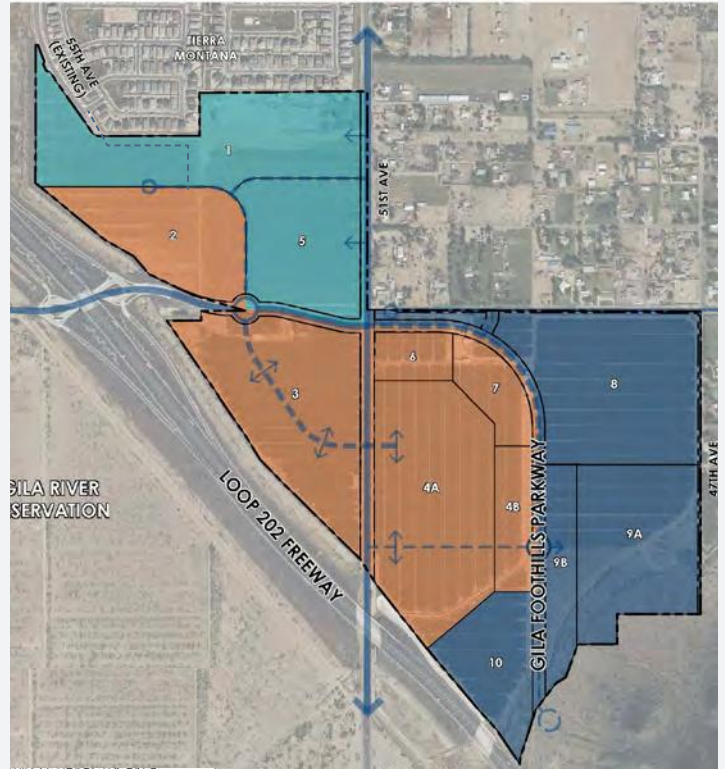
Character Areas:		DISTRICT		HUB						FOOTHILLS			
Development Units:		1	5	2	3	4A	4B	6	7	8	9A	9B	10
Miscellaneous Development Standards	Minimum Open Space	(3)	(3)	NA	NA	NA	(3)	(3)	(3)	(3)	(3)	(3)	(3)
	Maximum Residential Density (1)	40 du/ac per site plan (5)	40 du/ac per site plan (2)(5)	(4b)	NA (4a)	NA (4a)	40 du/ac per site plan (5)	40 du/ac per site plan (5)	40 du/ac per site plan (5)	25 du/ac per site plan (5)	40 du/ac per site plan (5)	40 du/ac per site plan (5)	40 du/ac per site plan (5)
	Maximum Number of Units	Overall PUD total max. residential units shall not exceed 1,700 unless a hospital is constructed.		Overall PUD total max. residential units shall not exceed 1,700 unless a hospital is constructed.	NA (4a)		Overall PUD total max. residential units shall not exceed 1,700 unless a hospital is constructed.			Overall PUD total max. residential units shall not exceed 1,700 unless a hospital is constructed.			
	Parking Standards	Parking shall be provided per Ordinance Section 702, unless modified below or elsewhere within this PUD. Each new auto dealership shall additionally meet the following minimum requirements: <ul style="list-style-type: none"> <li>• Offer for sale and service at least one EV or hybrid automobile.</li> <li>• 1 level 3 charger;</li> <li>• 3 level 2 chargers; and</li> <li>• .5 level 2 service chargers per service bay, excluding oil change or collision bays, not to exceed 10 chargers</li> </ul>											
	Bicycle Standards	<ul style="list-style-type: none"> <li>• Bicycle parking and amenities will be provided per Section 1307-H of the City of Phoenix Walkable Urban Code, Chapter 13 of the City Zoning Ordinance.</li> <li>• Bike parking will comply with ADA requirements and not impede on-site pedestrian access.</li> <li>• A clearance of at least four feet in width will be provided for pedestrian access.</li> <li>• Bicycle racks must be a minimum of 30 inches from a wall or other obstruction.</li> <li>• The minimum length of 72 inches will be provided per bicycle parking space.</li> <li>• With the exception of uses located within an EV-focused auto campus, all nonresidential uses over 5,000 square feet floor area shall provide one bicycle space per 25 vehicle parking spaces, with a maximum of 25 required spaces. Dining and drinking establishments less than 5,000 square feet must provide a minimum of four bicycle spaces in the frontage setback and/or right-of-way if no vehicle parking is provided.</li> <li>• Secure bicycle parking will be provided as part of residential projects that include structured parking.</li> </ul>											
Footnotes: (1) The entire overall PUD Site may be developed with a maximum of 1,700 residential units. Hotel guest rooms do not count towards the maximum 1,700 dwelling unit for the overall PUD Site. Upon Certificate of Occupancy of a hospital in Development Unit 2 an additional 500 dwelling units above the 1,700 cap are permitted within Development Units 2 and 5. (2) Upon Certificate of Occupancy of a Hospital in Development Unit 2, a maximum density of 60 du/ac shall be permitted in this Development Unit. (3) As measured per site plan: Residential uses greater or equal to 14.0 du/ac: 5% of net area; Residential uses less than 14.0 du/ac: 15% of net area; Non-Residential uses: 0% of net area. (4a) Residential dwelling units are prohibited in this Development Unit (4b) Residential dwelling units are prohibited in this Development Unit, unless a hospital is constructed. (5) Except as provided for by Footnote 1, no more than 1,700 residential dwelling units are permitted across the Site.													



## LANDSCAPE STANDARDS

The landscape standards for Gila Foothills establish minimum standards for the integration of the built environment into the Site's landscape while providing a comfortable, accessible and aesthetically pleasing community.

The standards listed the following Landscape Development Standards define planting quantities and sizes for streetscapes, perimeter buffers, parking areas and plantings.



Tables.

The Landscape Standards Tables on the following pages identify minimum landscaping criteria for each Development Unit. The tables include column colors that correspond to the respective Character Areas which are also shown above.



Exhibit DG3 from the Design Guidelines found in Part 2 of this document is provided above for reference when reviewing Landscape Standard Tables.

## LANDSCAPE STANDARDS TABLE 1: LANDSCAPE SETBACKS

Character Areas:		DISTRICT*		HUB*						FOOTHILLS*			
Development Units:		1	5	2	3	4A	4B	6	7	8	9A	9B	10
Landscape Setback Plant Type, Spacing, and Minimum Planting Size, Adjacent to:	Estrella Drive*	NA	(1)	(4)	(4)	NA	NA	NA	NA	(1)	NA	NA	NA
	51st Ave*	(1)	(1)	NA	(4)	(4)	NA	(1)	NA	NA	NA	NA	NA
	55th Ave*	(1)	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA
	47th Avenue*	NA	NA	NA	NA	NA	NA	NA	NA	(1)	(1)	NA	NA
	Ed Pastor Loop 202 Freeway*	NA	NA	(1)	(1)	(1)	NA	NA	NA	NA	NA	NA	(1)
	Gila Foothills Parkway*	NA	NA	NA	NA	NA	(1)	(1)	(1)	(1)	NA	(1)	(1)
	Property line adjacent to other interior public or private street*	(1)	(1)	(3)	(3)	(3)	(3)	NA	NA	NA	NA	NA	NA
	Property line shared with Tierra Montana Neighborhood*	(1)	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA

Note:  
In addition to meeting the minimum Landscaping Standards, all landscaping will also follow applicable Design Guidelines found in Part 2 of this document. Said Design Guidelines include a specific Plant Palette for each Character Area and may call for a particular appearance, application, or treatment of the landscaping.

Footnotes:

(\*)  
Within all landscape setbacks, buffers, and open spaces located within all Development Units and the approx. 1-acre open space south of Estrella Drive will include a minimum of 10% of the required shrubs, shall be a milkweed or other native nectar species, and shall be planted in groups of three or more.

(1)  
Trees:  
20-feet on center or equivalent groupings  
Min. 2-inch caliper (50% of required trees)  
Min. 3-inch caliper or multi-trunk tree (25% of required trees)  
Min. 4-inch caliper or multi-trunk tree (25% of required trees)

Shrubs:  
Min. five (5) 5-gallon shrubs per tree.  
All plants will follow the Hub Character Area Plant Palette which may be found in the Design Guidelines, Part 2 of this document.

(2)  
Trees:  
60-feet on center or equivalent groupings  
Min. 2-inch caliper (50% of required trees)  
Min. 3-inch caliper or multi-trunk tree (25% of required trees)  
Min. 4-inch caliper or multi-trunk tree (25% of required trees)  
All trees included in the Hub Character Area Plant Palette, including palms, may be used to meet this landscape standard.

Shrubs:  
Min. five (5) 5-gallon shrubs per tree  
All plants will follow the Hub Character Area Plant Palette.  
All plants will follow the Hub Character Area Plant Palette which may be found in the Design Guidelines, Part 2 of this document.

(3)  
For sites used for vehicle sales the landscape standards of Footnote (2) apply.  
For sites used for land uses other than vehicle sales the landscape standards of Footnote (1) apply.

(4)  
Trees:  
20-feet on center or equivalent groupings  
Min. 2-inch caliper (50% of required trees)  
Min. 3-inch caliper or multi-trunk tree (25% of required trees)  
Min. 4-inch caliper or multi-trunk tree (25% of required trees)  
All trees included in the Hub Character Area Plant Palette, including Date Palms, may be used to meet this landscape standard.  
Portions of the Landscape Setback occupied by Display Pedestals shall not be considered when calculating the amount of required trees.

Shrubs:  
Min. five (5) 5-gallon shrubs per tree  
All plants will follow the Hub Character Area Plant Palette which may be found in the Design Guidelines, Part 2 of this document.

## LANDSCAPE STANDARDS TABLE 2: ENHANCED STREETScape R.O.W PLANTING

Character Areas:		DISTRICT		HUB						FOOTHILLS			
		1	5	2	3	4A	4B	6	7	8	9A	9B	10
Enhanced Street Landscaping within ROW	Estrella Drive Center Median	NA	(1)	(1)	(1)	NA	NA	NA	NA	NA	NA	NA	NA
	Estrella Drive Between Curb and Sidewalk	NA	(2)	(2)	(2)	NA	NA	NA	NA	NA	NA	NA	NA
	Estrella Drive Min. Sidewalk Shading	NA	75%	25%	25%	NA	NA	NA	NA	NA	NA	NA	NA
	51st Avenue Center Median	(7)	(7)	NA	(3)	(3)	NA	(3)	NA	NA	NA	NA	NA
	51st Avenue Between Curb and Sidewalk	(7)	(7)	NA	(2)	(2)	NA	(2)	NA	NA	NA	NA	NA
	51st Avenue Min. Sidewalk Shading	75%	75%	NA	25%	25%	NA	25%	NA	NA	NA	NA	NA
	Gila Foothills Parkway Center Median (If Median is Feasible)	NA	NA	NA	NA	NA	(5)	(4)	(5)	(5)	NA	(6)	(6)
	Gila Foothills Parkway Between Curb and Sidewalk	NA	NA	NA	NA	NA	(7)	(7)	(7)	(7)	NA	(7)	(7)
	Gila Foothills Parkway Min. Sidewalk Shading	NA	NA	NA	NA	NA	75%	75%	75%	75%	NA	75%	75%
	Other interior public or private street - center median (If median is feasible)	(8)(9)(10)	(9)(10)	(11)	(11)	(11)	(11)	NA	NA	NA	NA	NA	NA
	Other interior public or private street - between curb and sidewalk	(8)(9)(10)	(9)(10)	(14)	(12)	(12)	(12)	NA	NA	NA	NA	NA	NA
	Other interior public or private street - Min. Sidewalk Shading	(8)(9)(10)	(9)(10)	(13)	(13)	(13)	(13)	NA	NA	NA	NA	NA	NA

Notes:

- In addition to meeting the minimum Enhanced Landscaping Standards above, all landscaping will also follow the respective Conceptual Street Sections as ultimately approved by the City of Phoenix Streets Transportation Department, and all other applicable Design Guidelines found in Part 2 of this document. The Design Guidelines include plant palettes that identify the plants recommended in each Character Area. The Design Guidelines also include conceptual imagery that may encourage a greater quantity of plantings or a particular appearance, application, or treatment.
- Dimensions of medians and distances between curbs, sidewalks, appurtenances, etc. to be determined by the approved TIA and Master Street Plan.
- The Planning and Development Department may approve similar tree or plant species at the time of site plan review. Sidewalk shading minimum requirement may be met by trees located in the ROW as well as within the setback.
- The Planning Administrator may approve an alternative planting theme for the median and/or space between curb and sidewalk. The alternative planting theme must be equal or greater in quality of appearance as the standard above, and must be applied to the entire length of the street segment to create a cohesive campus appearance.
- Shade Pockets:  
With the exception of frontages designated as a Gateway Zone (See Exhibit DG3, Landscape Character Zones) where sites are developed and used for vehicle sales, Shade Pockets may be utilized to meet the minimum sidewalk shading requirement. The following minimum standards for Shade Pockets shall apply:
  - A minimum of 1 Shade Pocket per site plan or 1 Shade Pocket for every 300 linear feet of street frontage, which ever results in the greater number of Shade Pockets, shall be provided.
  - Each Shade Pocket must provide a minimum of 300 square feet of shade.
  - Shade Pockets must follow all applicable Hub Character Area Design Guidelines.
  - For frontages where Shade Pockets are implemented, sidewalks shall be shaded a total of no less than 25%.



## Landscape Standards Table 2 Footnotes:

(1)

Trees: Date Palms and Thornless Cascalotes will alternate and be placed 20 feet on center.

Date palms will be a minimum of 15-feet tall at planting.

Thornless Cascalotes will be a minimum of 2-inch caliper at planting.

Shrubs: Min. of five 5-gallon shrubs per tree

Ground Cover: Min. of 75% including coverage from shrubs

All shrubs, plants, and trees will be from the Hub Character Area Plant Palette found in the Design Guidelines.

(2)

Trees: Date Palms placed 20 feet on center.

Date palms will be a minimum of 10-feet tall at planting.

Shrubs: Min. of five 5-gallon shrubs per tree

Ground Cover: Min. of 75% including coverage from shrubs

All shrubs, plants, and trees will be from the Hub Character Area Plant Palette found in the Design Guidelines.

(3)

Where a center median is feasible, the criteria of Footnote 1 will be applied.

(4)

Trees: Date Palms and Ironwood trees will alternate and be placed 20 feet on center.

Date palms will be a minimum of 10-feet tall at planting.

Ironwoods will be a minimum of 2-inch caliper at planting.

Shrubs: Min. of five 5-gallon shrubs per tree

Ground Cover: Min. of 75% including coverage from shrubs

All shrubs, plants, and trees will be from the Foothill Character Area Plant Palette found in the Design Guidelines, or will be a similar species as approved by the Planning and Development Department.

(5)

Trees: Ironwood trees and and Palo Breas will alternate and be placed 25 feet on center.

Trees will be a minimum of 2-inch caliper at planting.

Shrubs: Min. of five 5-gallon shrubs per tree

Ground Cover: Min. of 50% including coverage from shrubs

All shrubs, plants, and trees will be from the Foothill Character Area Plant Palette found in the Design Guidelines, or will be a similar species as approved by the Planning and Development Department.

(6)

Trees: Palo Breas, Blue Palo Verdes, and/or Desert Museum Palo Verdes will be placed 25 feet on center.

Trees will be a minimum of 2-inch caliper at planting.

Shrubs: Min. of five 5-gallon shrubs per tree

Ground Cover: Min. of 50% including coverage from shrubs

All shrubs, plants, and trees will be from the Foothill Character Area Plant Palette found in the Design Guidelines, or will be a similar species as approved by the Planning and Development Department.

(7)

Trees: Shade trees from the respective Character Area Plant Palette will be placed 25 feet on center.

Trees will be a minimum of 2-inch caliper at planting.

Shrubs: Min. of five 5-gallon shrubs per tree

Ground Cover: Min. of 50% including coverage from shrubs

All shrubs, plants, and trees will be from the respective Character Area Plant Palette found in the Design Guidelines or will be a similar species as approved by the Planning and Development Department.

(8)

**Where 55th Avenue extends into the site and terminates at a T-intersection with a proposed internal street, the following Landscape Standards apply:** No landscaped median is required.

Between the curb and sidewalk, the landscape standards of Footnote (7) apply.

Min. Sidewalk Shading: 75%.

(9)

**For the interior street extending from 51st Avenue between DU1 and DU5:**

Between the curb and sidewalk, and for any median, the landscape standards of Footnote (7) apply.

Min. Sidewalk Shading: 75%.

(10)

**For the interior street extending from Estrella Drive and curving between DU2 and DU1& DU5 the following standards apply:**

Between curb and back of sidewalk, the landscape standards of Footnote (7) apply.

For the median, the landscape standards of Footnote (11) apply.

Sidewalk Shading: 25% for vehicle sales where permitted and 75% for all other land uses other than vehicle sales

(11)

Trees: Chinese Pistache and California Fan Palms will alternate and be placed 25 feet on center.

Palms will be a minimum of 10-feet tall at planting.

Shrubs: Min. of five 5-gallon shrubs per tree.

Ground Cover: Min. of 50% including coverage from shrubs.

All shrubs, plants, and trees will be from the Hub Character Area Plant Palette found in the Design Guidelines, or will be a similar species as approved by the Planning and Development Department.

(12)

Trees: Shade trees or palms from the Hub Character Area Plant Palette placed 25 feet on center.

Trees will be a minimum of 2-inch caliper at planting.

Palms will be a minimum of 10-feet tall at planting.

Shrubs: Min. of five 5-gallon shrubs per tree

Ground Cover: Min. of 50% including coverage from shrubs

All shrubs, plants, and trees will be from the Hub Character Area Plant Palette found in the Design Guidelines or will be a similar species as approved by the Planning and Development Department.

(13)

Sidewalk Shading:

25% for vehicle sales where it is a permitted land use.

Shade Pockets may be used to meet the 25% sidewalk shading criteria.

See Table Notes on Shade Pockets.

50% for all other land uses other than vehicle sales.

(14)

For vehicle sales, the landscape standards of Footnote (12) apply.

For all other land uses other than vehicle sales, the landscape standards of Footnote (7) apply.

## LANDSCAPE STANDARDS TABLE 3: MISCELLANEOUS

Character Areas:		DISTRICT		HUB						FOOTHILLS			
Development Standards:		1	5	2	3	4A	4B	6	7	8	9A	9B	10
Miscellaneous Standards	Interior Parking Lot : Landscaping + Plant Types	(1)	(1)	(3)	(2)	(2)	(3)	(1)	(1)	(1)	(1)	(1)	(1)
	Off-Street Parking Screening	(4)	(4)	(4)or (5)	(4)or (5)	(4)or (5)	(4)or (5)	(4)or (5)	(4)or (5)	(4)	(4)	(4)	(4)

Note:  
 In addition to meeting the minimum Landscaping Standards, all landscaping and site improvements will also follow applicable Design Guidelines found in Part 2 of this document. Said Design Guidelines may call for a greater quantity of plantings or a particular appearance, application, or treatment.

Footnotes:

- (1)  
A minimum of 10% of the area will be landscaped.
- (2)  
Minimum 5% interior surface area (exclusive of perimeter landscaping and all required setbacks) for permitted vehicle display areas. Landscape planters located at ends of each row of parking
- Trees:  
Min. 2-inch caliper (60% of required trees)  
Min. 1-inch caliper (40% of required trees)
- Shrubs:  
Min. five (5) 5-gallon shrubs per tree
- (3)  
For sites used for vehicle sales the landscape standards of Footnote (2) apply.  
For sites used for land uses other than vehicle sales the landscape standards of Footnote (1) apply.
- (4)  
Min. 36-inch high screen wall or landscaping.
- (5)  
Min. 10-inch high accent wall or 18-inch high landscaping for Display Areas within parking lots

## 5. SIGNAGE

### On-Premise Signage

A comprehensive sign plan may be submitted for on-premise signage for any future development.

As it relates to on-premise signage, all permanent signs shall be compatible with the design of buildings, reflecting the architectural style, building materials, textures, colors, and landscape elements of the overall project. The following design standards will promote proper and efficient signage that is compatible with the character and environment of the overall PUD.

Signage shall convey a sense of quality, sustainability and style. Creativity is encouraged and some signage regulations may be exceeded in certain circumstances via a comprehensive sign plan for signs that are well designed and have a high degree of character and inventiveness.

Architecturally, building signage shall utilize and/or enhance the architectural elements of the building and be placed to logically relate to the overall composition of the building's façade.

Signs shall enhance the visual environment, protect against visual clutter, promote general safety and ensure compatibility with the character of the area.

### Gateway Signage

To create a sense of arrival into Gila Foothills, monumentation with accessory signage may be permitted within the roundabout on Estrella Drive as approved by the City of Phoenix Street Transportation, and Planning and Development Departments.



## Off-Premise Signage

Given the Site's location adjacent to the Freeway, the PUD will allow for off premise Freeway signs that may advertise both on-site and off-site uses. These proposed Freeway signs will conform to Sections 705 and 705.2 of the Phoenix Zoning Ordinance.

As it relates to off-premise signage, this PUD shall require the following beyond Section 705.2:

- Any off-premise sign shall incorporate design elements and be branded consistent with the respective Character Area Design Guidelines.
- The required pole cover design will incorporate a mixed material façade and column that replicates the look of stone, wood, and metal as conceptually depicted in Exhibit 15 Conceptual Off Premise Freeway Sign Sign Design, design to be approved by Laveen Village.
- An additional screen will be placed within the "V" of the displays. The screen will be branded with the Laveen Village branding identity similar to the pole cover.
- No off-premise sign shall be placed within 2000-feet of South Mountain, as approved by the City of Phoenix Planning and Development Department.
- There shall be two signs with a maximum height of 48-feet and one sign with a maximum height of 55-feet, subject to obtaining a use permit.
- All off-premise digital signs will utilize Planning and Development Department-approved vertical louvers on face(s) to mitigate light trespass to nearby residentially-used properties in general conformance with the VisionIQ Sitaline louver panel details submitted by the applicant. Approval of digital signs is subject to a use permit.

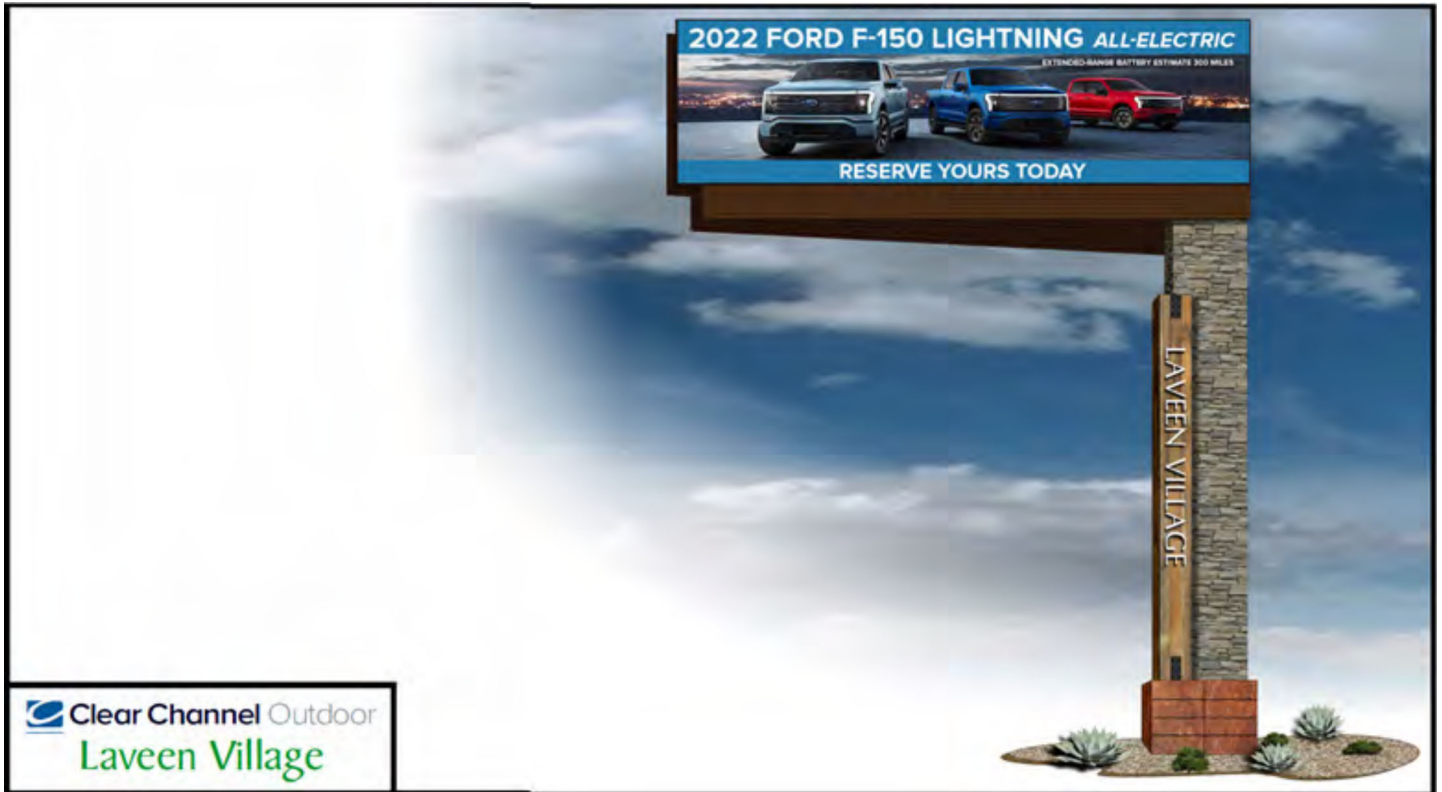


EXHIBIT 16 - Conceptual Off Premise Freeway Sign Design

N.T.S. ⓘ

Despite the following points not being enforced by the City, the landowner is additionally committing to the following restrictions and supporting elements:

- Lease restrictions prohibiting all political and public advocacy messaging.
- Lease allows public safety and community support messaging.
- Established agreements to provide the Laveen Community Council with the equivalent of one, one-week digital spot per month for the display of community support messages. The display time can be accumulated over a calendar year to be used collectively for a larger campaign for a specific time-period. This is subject to space availability.
- The digital spots will be displayed on any available Clear Channel Outdoor displays within the geographic boundary of the Freeway, I-10 west of I-17, and I-17 south of Camelback.
- From the first three (3) years of erection, discount advertising coupons (limited to 10 coupons per calendar year) will be provided to the Laveen Community Council to be distributed at the discretion of the Laveen Community Council to support local, Laveen Village based business that may want to purchase a billboard campaign.

## 6. SUSTAINABILITY

The development is planned as a sustainable development within the community. Energy efficiency in design and long-term operation, coupled with thermal comfort in building design, create a superior experience for customers, employees, property owners and residents. Development parcels must incorporate a minimum of five (5) of the following sustainability principles as approved by the Planning and Development Department;

- Use of cool pavement to treat surfaces of drive aisles, parking areas, sidewalks, paths, or streets.
- Incorporate LED and energy efficient lighting technology into all lighting constructed on the Site, including parking lots and streets.
- Install electric vehicle charging.
- Install electric bicycle charging stations.
- Install rooftop solar panels.
- Provide the following:
  - High-performance windows
  - Insulation
  - HVAC systems
- Utilize shared/reciprocal parking to reduce the overall number of parking spaces required. Use and implementation of shared parking strategies should be considered wherever possible following the requirements provided in Zoning Ordinance Section 702.E.2 (Shared Parking Model).
- Use passive solar building designs that respond to the southwest climate by incorporating materials and design methods suitable for the region.
- Select building materials and colors to reduce quantifiable heat gain, including roof tops.
- Locate shade trees along western building facades to reduce solar heat gain.
- Eliminate turf used only for aesthetic purposes.
- Implement new post COVID-19 design features to promote healthy communities.
- Architectural building elements such as building overhangs and plan variation simulate the flow of air around pedestrian areas and throughout the Site.
- The development will provide a safe, reliable, and efficient storm water management system that preserves the existing drainage ways and protects both human health and the natural environment.
- The development's interior lighting will be controlled by motion sensors to reduce energy consumption.
- Where covered parking is provided, 25% will be covered by solar panels.



## Drought Management Commitments

In June 2022, the City of Phoenix declared a Stage 1 Water Alert, which sets in motion an intensive public education and information program centered around water conservation, sustainability and management. This PUD has been designed around a framework that demonstrates progressive commitments by the Gila Foothills Community to implement best management practices for long-term comprehensive water sustainability.

This PUD commits to the following water conservation and sustainability measures for Gila Foothills:

**Irrigation and Use of Smart Controllers.** Common areas that are irrigated will be required to utilize smart irrigation controllers that can be controlled remotely to promote water conservation.

- Automatic drip irrigation systems are required for all non-turf landscaping.
- Automatic irrigation systems designed by a landscape architect or other similar certified designer shall be required for all planting areas in common areas. Fixed risers in such areas are prohibited except in areas where turf is maintained.
- Large overhead and drip irrigation systems should be zoned for exposure, topography and varying water requirements of the plant material.



**Water Leak Detection.** Gila Foothills common areas managed and maintained by a property owners association will undergo regular water leak detection audits to ensure the integrity and efficiency of the irrigation system is maintained.

**Native Drought Tolerant Vegetation/Xeriscape Plans.**

The Gila Foothills PUD contains a curated, localized water-wise plant palette for use throughout the entire community that blends native species with materials that are desert adapted.

**Turf Restrictions.** The Gila Foothills PUD contains turf restriction prohibitions that advance water conservation goals and are a required element of the NWF Partnership.

- Turf is prohibited in all medians and public rights of way.
- Turf is prohibited within common areas adjacent to the South Mountain Park.
- With the exception of the 1-acre open space area proposed at the intersection of Estrella Drive and 51<sup>st</sup> Avenue, turf is prohibited within all areas visible from public right-of-way within the Gila Foothills PUD.
- “Non-functional” turf is discouraged within the Gila Foothills PUD.
- Turf, where allowed, will not be planted on slopes in excess of 5%.



## Energy Diversification EV-Focused Auto Campus

With an ambitious goal to see 280,000 electric vehicles in the city of Phoenix by 2030, the region is rapidly becoming known as the “electric valley” with Phoenix on the cutting edge of the transition to electric vehicles.

The Auto Campus at Gila Foothills will be one of the first auto campuses in the country to focus on the transition to electric vehicles.

To ensure this the focus of the campus is on electrification, each of the new car auto dealerships shall meet the following requirements:

- Offer for sale and service at least one EV or hybrid automobile.
- 1 level 3 charger available to the public during business hours;
- 3 level 2 chargers available to the public during business hours; and
- .5 level 2 service chargers per service bay, excluding oil change or collision bays, not to exceed 10 chargers





## 7. COMPLETE STREETS

The City of Phoenix Complete Streets Design Document was adopted in October of 2018 and is applied throughout the City. As such, Complete Streets vary in their design, function and appearance throughout the City. Consideration of their application should be done in a context-sensitive way in relation to surrounding land uses, street type, available right-of-way, adopted general and specific plans and overall intent of the corridor in coordination with other city codes and ordinances.

The Gila Foothills PUD acknowledges the merit of the Complete Streets design principles and agrees with the context-sensitive application approach as defined by the City.

The following section provides design principles of the City of Phoenix Complete Streets Manual (in bold) followed by an explanation of how this PUD will work to fulfill the design principle or partner with the City on City specific goals and implementation practices towards meeting the design principle:

### **DESIGN FOR CONTEXT:**

#### **The bikeway system should expand and complement the existing network.**

This PUD will provide on-street bike lanes in accordance with the street sections as defined by the approved TIA. These will be consistent with the proposed Complete Street Guidelines. Additional opportunities to incorporate connections within the PUD will be evaluated at the site plan application level.



*Following Complete Streets concepts, the images above show how differently safe and comfortable roadway improvements may look based on who the specific roadway users are, the surrounding land uses, and the existing or future vehicular traffic capacity.*

**DESIGN FOR SAFETY:**

**Streets should be designed to the appropriate speed that provides for safe multi-modal mobility while achieving the desired operation and aligning with the roadway classification and surrounding community characteristics.**

All streets will be designed in accordance with the approved TIA and the City of Phoenix standards. Additional street safety features will be evaluated as needed for future development across the PUD.

To eliminate cut through traffic in the adjacent residential area, a cul-de-sac will be constructed at the end of Estrella Drive east of 51st Avenue. No other changes beyond those associated with the cul-de-sac are proposed to Estrella Drive. These planning and design choices will deter cut-through traffic and commercial traffic from entering the residential area and will create a safer environment for residents.

To achieve the desired operation of 47<sup>th</sup> Avenue to continue to serve as a rural access drive to the adjacent residential properties, no site vehicular-access is proposed from 47<sup>th</sup> Avenue and no right-of-way improvements are proposed on 47<sup>th</sup> Avenue (unless required for emergency access by the City of Phoenix).

**Where practicable, consolidate driveways to minimize modal conflicts and increase opportunities for infrastructure that supports Complete Streets principles.**

Driveways will be partially determined by the TIA. Each driveway will provide the appropriate distance from one another to minimize conflicts.



*Following Complete Streets concepts, the images above show how differently safe and comfortable roadway improvements may look based on who the specific roadway users are, the surrounding land uses, and the existing or future vehicular traffic capacity.*

**DESIGN FOR COMFORT & CONVENIENCE:**

**Shade should be a primary technique in projects to reduce ambient temperatures and reduce direct sunlight exposure for pedestrians and cyclists.**

Shade will be provided in accordance with the required Development Standards throughout the Site in strategic locations where pedestrian and bicycle activity will be the highest.

To limit conflicts with vehicles accessing employment uses in The Hub Character Area, shaded walkways and paths are enhanced and focused in The District and Foothills Character Areas to safely and comfortably connect surrounding neighborhoods through the Site to the new South Mountain Trailhead. 75% sidewalk shading is proposed in the District and Foothills Character Areas.

**DESIGN FOR SUSTAINABILITY:**

**Minimize impermeable surfaces and maximize vegetation on streets outside of vehicular travel lanes. Street designs should capture and beneficially use stormwater wherever practicable.**

Where practical, permeable surfaces will be used to enhance the pedestrian experience and contribute to the sustainability of the Site. These improvements will be coordinated at the site plan level in accordance with the provisions provided in previous sections of this PUD.

**DESIGN FOR COST EFFECTIVENESS:**

**When practicable, identify opportunities to partner and share resources (e.g., driveways, parking, etc.).**

As future development occurs, additional opportunities to consolidate resources will be evaluated further including driveways, plaza spaces, parking, bike parking, and other elements conducive to a pedestrian friendly environment.

Additionally, stormwater management and green infrastructure are elements of complete streets. The future development within this PUD also acknowledges these elements and believes that working with the City during the planning process will help identify context appropriate practices as they relate to green infrastructure roll out.



## 8. INFRASTRUCTURE

In general, Gila Foothills will connect to existing public water and sewer infrastructure constructed by Taylor Morrison as part of Tierra Montana. The connection points and the design of the public infrastructure systems (water, sewer and storm water) are all subject to approval by the City of Phoenix Planning and Development Department.

### PUBLIC WATER

Gila Foothills will connect to the current public water system installed as part of Tierra Montana. The connection points:

- 16-inch water line at Carver Road and 51st Avenue.
- 12-inch water line at 55th Ave and Farmhand Drive.
- 12-inch water line in Estrella Drive.

### PUBLIC SEWER/WASTEWATER

Gila Foothills will connect to existing sewer infrastructure constructed as part of Tierra Montana.

- This peak wastewater flow is estimated at 1.29 MGD.
- The nearest sewer connection is at MH 06-16-205 in 55th Avenue.
- This wastewater flow passes through the 15" alignment on 59th Ave (between Dobbins and Baseline), which is being relieved under the City of Phoenix sewer relief project WS90500286.

### STORMWATER

Storm water requirements:

- Gila Foothills will provide on-site retention for the 100-year, 2-hour rainfall runoff.
- A Cross Retention and Drainage Agreement will be required when adjacent properties desire, or are required to, provide a perpetual, non-exclusive, mutual cross retention and drainage easement for the purposes of storm water retention and drainage.
- For projects that are affected by offsite flows – Submit a Drainage Report developed in accordance with the “Drainage Design Manual for Maricopa County, Volume I” developed by the Flood Control District of Maricopa County.
- Reference will be made to the Laveen Area Drainage Master Plan that may be affecting the site.

### EXISTING IRRIGATION

Any existing Salt River Project irrigation supply ditches and/or irrigation tail water ditches on this site, or in the right-of-way adjacent to this site, will be replaced with an underground pipeline, or abandoned subject to the approval of Salt River Project and/or downstream users.





These are graphics and written descriptions within the Design Guidelines that illustrate the intent of streetscape landscaping and street cross sections. These graphics shall be treated as conceptual and for illustrative purposes only. Final determination of these details will be determined by the Streets Transportation Department at the time of the Master Street Plan and site plan submittals.



# PART 2 - DESIGN GUIDELINES

## 1.1 PURPOSE AND GOALS

These Design Guidelines have been prepared in conjunction with a PUD rezoning request for the 290-acre Gila Foothills property. The PUD zoning allows for a mixed-use development that incorporates employment, commercial, health care and residential uses. These guidelines will establish a comprehensive design approach to all developments throughout all portions of the PUD area.

The PUD breaks the property into three distinct Character Areas and subsequently into individual Development Units within those Character Areas. The Design Guidelines provide an overall vision for the built environment within each Character Area, and focus on a range of associated topics, including but not limited to, site and building design, circulation, landscape design, drainage, lighting, and hillside and perimeter treatments.

As it relates to the PUD and overall success of the proposed land uses and surrounding area, the main contributions of the Design Guidelines are:

- Responding to the Laveen Technology Corridor with Design Guidelines that support a major employment campus within The Hub Character Area.
- Establishing a common design theme to create a sense of unity within each Character Area.
- Within the Foothills and District Character Areas, providing meaningful transitions between the intensity of The Hub Character Area and the surrounding established development.



## 1.2 Site Description

Gila Foothills PUD encompasses approximately 290 acres and sits at the southern tip of the Laveen Village in Phoenix, Arizona. The Site is more specifically bordered by the Freeway to the southwest, South Mountain Park to the southeast, 51st and 47th Avenue to the east, and Estrella Drive and the Tierra Montana neighborhood to the north.



EXHIBIT DG1 - Site Context

N.T.S. 



### 1.3 Guiding Theme and Character

The theme and character of the Site will be guided by the surrounding Laveen Village character while pursuing an individual, modern design aesthetic defined in each Character Area by a refined palette of materials and a building proportion and scale that is complimentary to the development. Each Character Area will incorporate their own individual community features, components, and defining elements focused around a particular design aesthetic and architectural style. As this property sits within the Laveen Technology Corridor as an employment district, all developments will respond to the requirements of this classification with appropriate density and transition to the surrounding neighborhoods.

The Design Guidelines provide the overarching themes necessary to ensure a predictable outcome of high quality, aesthetically pleasing developments.

Intended themes of these Design Guidelines include:

- Establishing complementary yet distinct Character Area design parameters to create a unifying sense of place within each Character Area and for the development as a whole.
- Provide design guidance that complements the character of the surrounding Laveen community and promotes the proximity to the Freeway and South Mountain Park.
- Creating seamless transitions between the Character Areas while promoting walkability and connectivity throughout the Development Units.
- Encourage innovative design to promote development visibility and commercial success that is enduring.



## 1.4 Gila Foothills Character Areas

Gila Foothills is divided into the three following Character Areas:

### A. The Foothills Character Area

- The Foothills Character Area is approximately 99 acres total and sits at the southeast part of the Gila Foothills PUD. This Character Area is more specifically bordered by Gila Foothills Parkway and the Freeway to the west, South Mountain Park to the south, 47th Avenue to the east, and Estrella Drive to the north. The Foothills Character Area is comprised of Development Units 8, 9A, 9B, and 10.
- The Foothills Character Area Design Guidelines call for modern Sonoran architectural aesthetic that incorporates and refines the building materials, landscaping, and architectural details of the surrounding community.
- The Foothills Character Area Design Guidelines focus on seamlessly transitioning internal land uses while also preserving the nature of the surrounding neighborhoods and providing for access to a new South Mountain Park public trailhead.

### B. The District Character Area – Site Description

- The District Character Area is approximately 62 acres in area and is located between Estrella Drive, 51st Avenue and the Tierra Montana neighborhood. It contains Development Units 1 and 5.
- Guidelines for The District Character Area encourage a dynamic variety of diverse building styles that are governed by a modern rural architectural style and connected by a coordinated and comfortable circulation system that engages pedestrians, bicyclists, and vehicles.
- Guidelines for this Character Area aim to distance building height and commercial traffic from the surrounding residential developments and extend the existing trail system south from the Tierra Montana neighborhood.

### C. The Hub Character Area – Site Description

- The Hub Character Area is approximately 129 acres total and sits at the center of the Gila Foothills PUD and within the Laveen Technology Corridor, it is bordered by the Freeway to the southwest, Gila Foothills Parkway and the Foothills Character Area to the east, and the District Character Area and a portion of Estrella Drive to the north. The Hub Character Area is comprised of Development Units 2, 3, 4A, 4B, 6, and 7.
- Design Guidelines for The Hub Character Area focus on providing direction for a clean and modern, branded architectural style and parameters to ensure landscaped and open areas are provided between larger buildings. Product visibility is a fundamental goal for site design with provisions made for low walls, landscaping, and berming to create a comfortable site to streetscape transition.
- The Hub Character Area Design Guidelines further establish a sense of arrival along Estrella Drive streetscape and promote the commercial employment land uses along the Freeway.



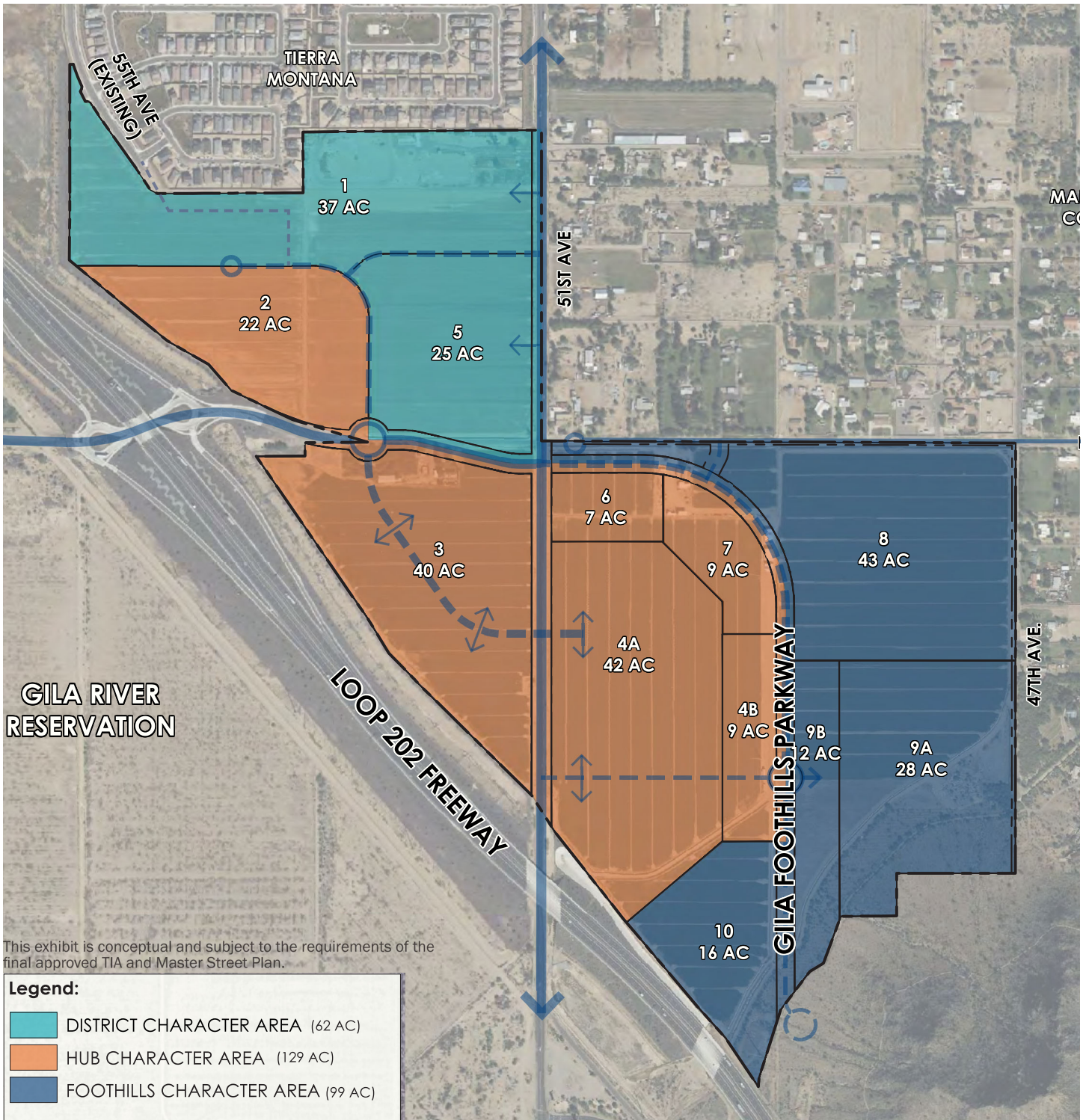


EXHIBIT DG2 - Character Areas

N.T.S.

## 1.5 Gila Foothills Landscape Character Plan

To guide the landscape theming throughout the Gila Foothills Site, Exhibit DG3 Landscape Character Plan identifies zones across all three Character Areas. These zones coordinate with the Design Guidelines and Plant Palette to identify the appropriate plant materials and landscape aesthetic for various roadway corridors and Character Areas within the Site.

### Gateway Zone

The Gateway Character Zone is the primary arrival route into Gila Foothills and will include elevated levels of monumentation, signage & wayfinding, and planting density. The enhanced landscaping within the Gateway Zone will also follow guidance of the Formal Streetscape Zone, uniformity and synergy of the planting design will be emphasized the greatest in this Gateway Zone to ensure that the progression of arrival is distinctive. As outlined in the Hub Character Area Design Guidelines, monumentation, signage and wayfinding, and planting within the roundabout will coordinate with formal planting patterns found elsewhere within the Gateway zone.

### Formal Streetscape Zone

The Formal Streetscape Zone is along arterial roadways within the Hub and District Character Areas. Plantings are characterized with a formal theme-tree progression, and planted at a regular recurring interval. This Zone seeks to create corridors of consistent theming and planting character that will unify adjacent properties with repeating patterns and clearly coordinated planting regimes.

### Natural Streetscape Zone

Natural Streetscape zones are associated with roadways located in the Foothills Character Area, particularly for segments proximate or leading to the South Mountain Park and residential areas. Plantings should seek to be more irregular and representative of the natural environment, and utilize a palette of materials common to the Sonoran Desert. Linear placement of plants and trees are discouraged outside of roadways.

### WAPA Easement Zone

The presence of the high voltage WAPA powerline corridor along Gila Foothills Parkway will create opportunities for passive open space within the Site. Due to the highly restrictive and regulated nature of the corridor, all hardscape and landscape improvements will require WAPA coordination and approval. Improvements may be limited to retention, low level natural landscape treatments and trail corridors, but other uses may be explored as allowed. Improvements should complement nearby or adjacent landscape Character Zones by blending similar materials and density as permitted.

### South Mountain Perimeter Zone

The Gila Foothills property shares a significant common boundary with South Mountain Park. Within 50 feet of this shared boundary, future development within this zone shall seek to transition to the Park by evaluating opportunities for open edges, natural open spaces, hardscape materials and colors that appropriately complement those native to the Sonoran Desert and an overall emphasis on emphasizing complementary desert themes and character.

### Open Space / Buffer Zone

This zone is no less than 50 feet in depth and typically does not include walls or other vertical hardscape elements within the setback area. These passive landscaped areas will provide visual screening to roadways and may include retention basins, drainage conveyance and natural surface pathways.

### Shade Pocket Eligible Zones

Shade pockets are an alternative to the Street Tree and Sidewalks Shading landscape standards. In this Zone, Shade Pockets aim to provide shade while also providing visibility into the adjoining EV-focused auto campus.



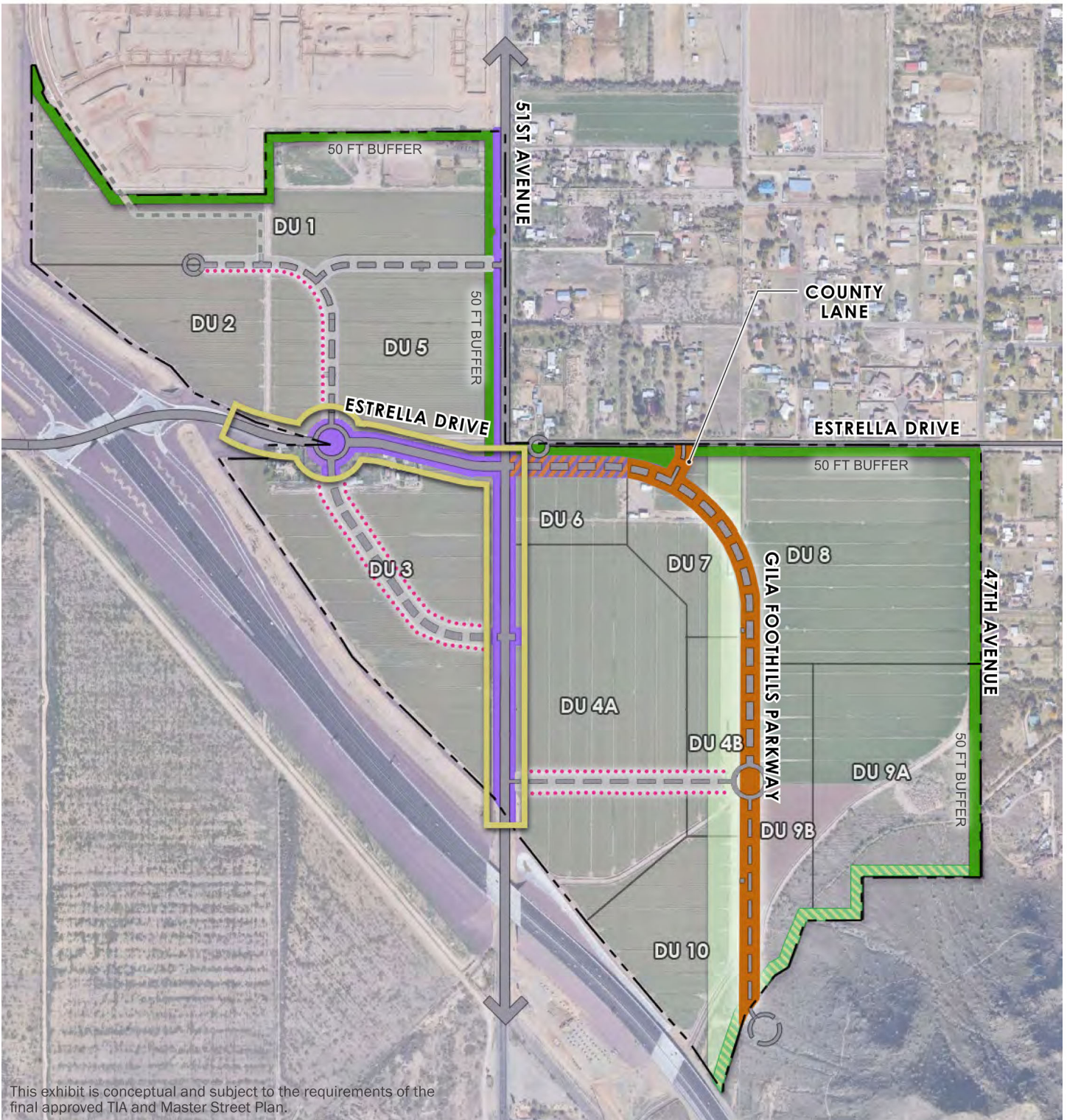


EXHIBIT DG3 - Landscape Character Plan

N.T.S.

LEGEND

- |  |                          |  |                                    |  |                                   |
|--|--------------------------|--|------------------------------------|--|-----------------------------------|
|  | Gateway Zone             |  | WAPA Easement Zone                 |  | Shade Pocket Eligible Zone        |
|  | Formal Streetscape Zone  |  | South Mountain Perimeter Zone      |  | Transition from Formal to Natural |
|  | Natural Streetscape Zone |  | Open Space / Landscape Buffer Zone |  |                                   |





# THE FOOTHILLS CHARACTER AREA DESIGN GUIDELINES



## FOOTHILLS CHARACTER AREA

### 2.1 Site and Building Design

#### a. Site Planning / Building Placement

- The site planning and building placement goals for Gila Foothills strive to create memorable environments. The design for these environments shall focus on the unique streetscapes with well-placed buildings and interconnected open spaces.
- Special landscape features shall be used to create paths of shade and points of appreciation to encourage pedestrian travel.
- In The Foothills Character Area it is strongly encouraged to place buildings to limit their visual presence to adjoining residential areas and external roadways. The placement of buildings shall further work to contribute to a comfortable high-quality street atmosphere that feels open and approachable to users.



#### b. Surface Parking

- Where parking is adjacent to a street, provide screening using a combination of an enhanced low screen wall and landscaping, and/or berming.
- Service entrances will be located to not disturb the reasonable enjoyment of on-site or off-site uses and shall be screened from view by landscaping, screening wall, or adjacent buildings.





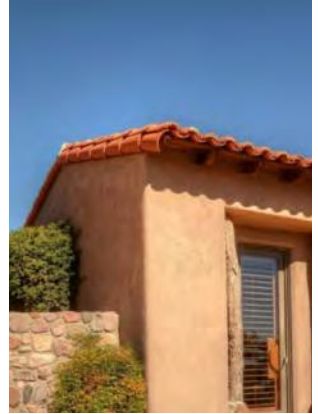
### c. Architectural Style and Character

- The Foothills Character Area Design Guidelines call for modern Sonoran architectural aesthetic that incorporates and refines the building materials, landscaping, and architectural details of the surrounding community.
- The Design Guidelines for The Foothills Character Area seek to integrate features from the surrounding Laveen residential character and support the proposed transition from the residential properties to The Hub Character Area and freeway.
- Modern Sonoran inspired flat and gable rooflines, masonry, stone, and exposed columns to support deep overhangs should be used to provide shade and contribute to quality outdoor spaces that reflect the agricultural history of this part of Laveen.
- Every building shall consider its relationship to the adjacent developments with regards to building placement, orientation, height, massing, and design vernacular. Larger buildings will be designed with projected and recessed facades to break up monotonous building masses and smaller, single-story buildings should be simple in design and incorporate strong rooflines and fenestration to articulate the building façade. Design elements should be incorporated to create a heightened sense of detail on the building at the pedestrian level while incorporating shaded outdoor areas in their design to link with open space, hardscape, and landscape features.
- Articulated building facades are encouraged.



*The design imagery shows stucco, sloped tile roof, recessed openings, and building and site layout/orientation to preserve building adjacency/proximity too open space between and connectivity to surrounding buildings*

- All hillside parcels will recognize that development of land in hilly or mountainous areas involves special considerations and unique situations which result from the slope of the land. Design elements should respond to the unique terrain of the site by blending with the natural shape and texture of the land while minimizing disturbances to the natural environment. These special considerations and unique situations include but are not limited to increased hazards to development from rock falls, storm water runoff, geologic hazards, increased limitations on vehicular travel, and increased difficulties in providing public services. In addition, steeply sloped lands introduce design limitations to roadways, cuts and fills, and building sites. In general, the more steeply the land slopes, the greater potential hazard and development limitation.
- To accomplish the modern Sonoran architectural theme envisioned for The Foothills Character Area, a minimum of three of the following elements shall be used:
  - Pitched roofs and/or flat roofs consistent with the territorial architectural style;
  - Articulated parapet lines and roof heights;
  - Exposed wood, structural wood decorative detailing;
  - Exposed Rafter, beams, rustic columns;
  - Rusted or patina finish metal accents;
  - Tubular metal (weathered/patina finish) or wood frame trellis;
  - Deep overhangs and/or building attached shade elements;



*Pitched, flat, and/or articulated rooflines*



*Exposed timber used for columns, beams, and rafters. Curved roof tiles. Warm natural earth tones. Shaded or inset windows.*



#### d. Miscellaneous Exterior Building Materials and Design Imagery

Design elements and detailing is required to be continuous around the structure with the same level of detail applied to each side. Articulation of wall planes, a variety of forms and heights, and other architectural design elements shall be used to simultaneously create visual interest provide and avoid long, monotonous facades.

Following is a list of miscellaneous building materials, design elements and features to obtain a modern Sonoran design aesthetic within The Foothills Character Area:

##### Building Skin and Accent Materials:

- Architectural concrete, either cast-in-place or precast, natural or integral-color finish.
- Adobe, slump block masonry or natural brick, rammed earth.
- Cut natural stone or faux stone.
- Traditional and Synthetic stucco (EIFS), integrally colored, smooth or textured finish.
- Metal panels, rusted or patina finish to give an aged look where glare and reflection are not objectionable.
- Wood. Engineered or natural (as appropriate for the desert climate).
- Clear, tinted, or minimally reflective glazing.

##### Building Roofing:

- Concrete or clay tile with earth-tone color range.
- Natural/coreten or patina-finish metal, seamed.
- Tubular metal (weathered/patina finish) or wood frame trellis.



Stucco and natural stone in warm earth tones



In some instances, greater detail may be given to building components such as the design of beams, and facade elements such as shutters or molding may be used.



**Window and Door Framing:**

- Anodized or painted aluminum
- Painted or stained wood
- Integrally colored PVC
- Painted steel

**Materials prohibited for exterior architectural finishes:**

- Exposed non-anodized aluminum.
- Mineral-fiber panels or facing materials.
- Mirrored finishes
- Vinyl façade materials

**Awnings**

- Awnings, natural or synthetic fiber, material designed to resist sun exposure, non-reflective, non-translucent, colors to be uniform, integrated with the color palette of the site or building.

**Screening of Mechanical Equipment:**

- Roof mounted mechanical equipment on flat roof structures shall be screened by a parapet or architectural screening element. The parapet is to be as high as the highest component of mechanical equipment on the structure.
- Hillside developments shall take special measures to screen mechanical units from buildings uphill. Preferably hillside buildings would not have roof mounted mechanical equipment.
- The parapet shall be designed and constructed with forms, materials and colors which are compatible with the structure below.
- Roof mounted mechanical equipment on pitched roofs must be contained within the attic space and not visible from ground level.

- All ground-mounted mechanical equipment shall be screened with site walls exceeding the equipment's height by at least 2 feet with adjacent ornamental planting.
- All refuse collection, storage, compaction and processing containers and facilities shall be screened from all vantage points with a site wall and opaque (solid) gates exceeding the height of the facility by 2 feet with ornamental planting.







## 2.2 CIRCULATION

### a. Primary Vehicular Entries and Circulation Gila Foothills Parkway

Gila Foothills Parkway represents the continuation of the Estrella Drive east of the realigned intersection with 51st Avenue, then curves to the south to parallel the WAPA powerline corridor. This roadway will be classified as a Collector street, servicing nearly all of the traffic originating within The Foothills Character Area. The conceptual road section shows a landscaped center median transitioning from a four-lane facility for the northern segment, narrowing to a 2-lane facility for the southern segment as it approaches the trailhead area. A more detailed description of the two Gila Foothills Parkway segments are provided later in this section.

### b. Perimeter Roads with Vehicular Access Restrictions

#### 47th Avenue

47th Avenue is an existing rural-character public roadway that extends along the entirety of the Project's eastern perimeter. The roadway is used exclusively for existing resident access. No public or primary points of vehicular access will be provided from 47th Avenue to serve the proposed Gila Foothills community. To comply with regulatory standards for emergency access, at the time of site plan review the City of Phoenix may determine that an emergency point of ingress/egress is needed from 47th Avenue. If required by the City of Phoenix, an access limited to emergency purposes only may be provided.

#### Estrella Drive

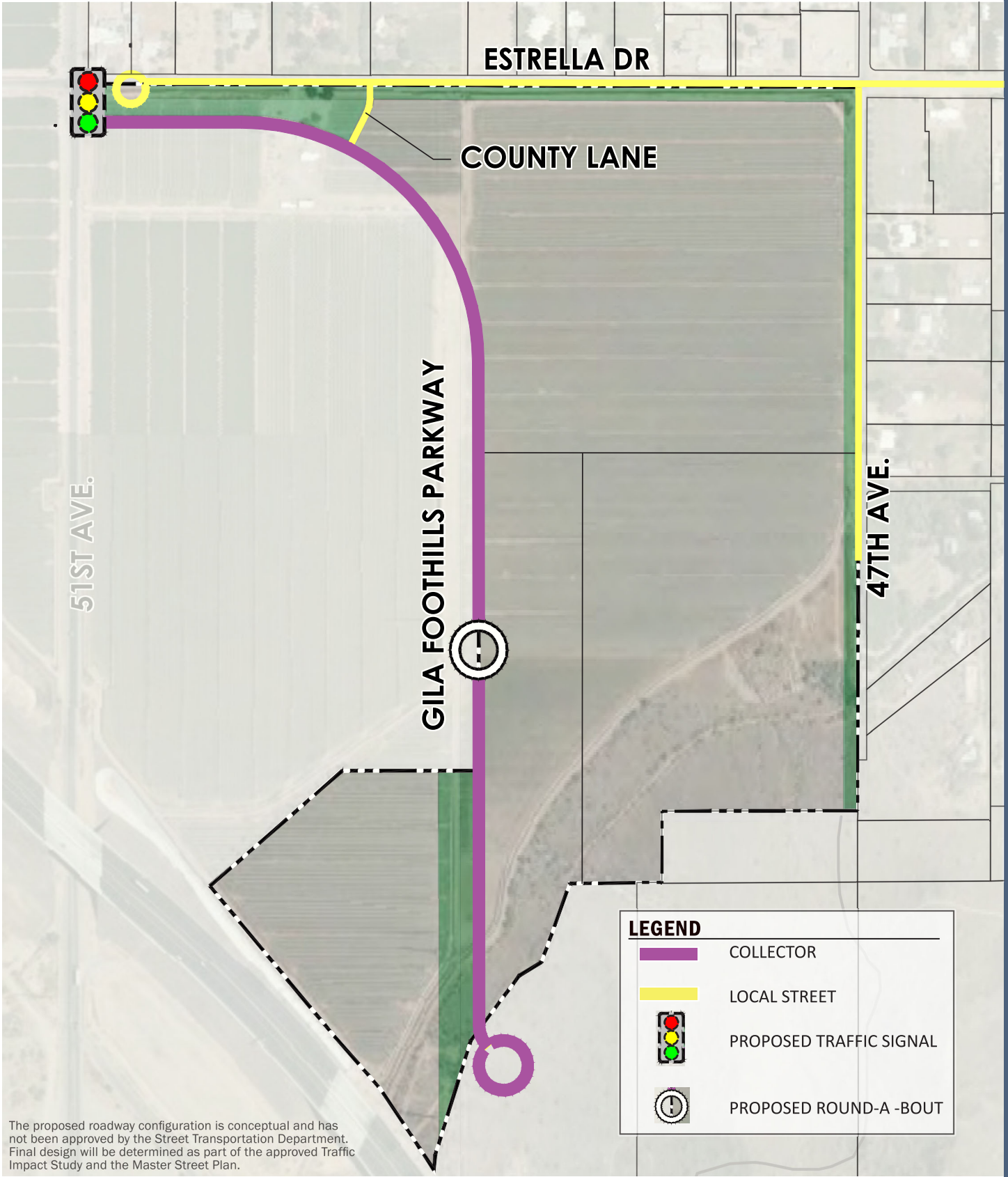
One of the primary goals of the Gila Foothills planned circulation network is to ensure that traffic impacts to adjacent neighborhoods are diminished to the greatest extent possible. The proposed Circulation Plan seeks to protect existing properties that front onto Estrella Drive east of 51st Avenue by eliminating through traffic movements along this corridor. The current 51st Avenue and Estrella Drive intersection will be shifted south, effectively disconnecting the east-west traffic pattern. East of the intersection, Estrella Drive will transition into the new Gila Foothills Parkway alignment that will curve southward, ultimately terminating at the proposed public trailhead. A small north-south connector lane, built in conjunction with the Gila Foothills Parkway improvements will provide a local-only linkage between Gila Foothills Parkway and the existing Estrella Drive (the section east of 51st Avenue) to maintain existing resident access to both 51st Avenue and the freeway.

#### County Lane

County Lane is a short roadway connecting Gila Foothills Parkway to Estrella Drive. This roadway will provide maintain access to the arterials and Freeway for surrounding residents and is not meant for commercial traffic.







The proposed roadway configuration is conceptual and has not been approved by the Street Transportation Department. Final design will be determined as part of the approved Traffic Impact Study and the Master Street Plan.

**LEGEND**

- COLLECTOR
- LOCAL STREET
- PROPOSED TRAFFIC SIGNAL
- PROPOSED ROUND-A -BOUT

EXHIBIT F1 - Foothills Character Area Circulation Plan

N.T.S.

### c. Pedestrian Circulation and Trail Connectivity

Pedestrian corridors are major walkways that link users to open spaces, employment areas, commercial establishments, and residential neighborhoods. They are characterized by shade, quality landscaping, and opportunities for pedestrian comfort, seating and relaxation.

Comfortable and safe pedestrian and bicycle routes will be provided to connect users from the surrounding area to establishments within The Foothills Character Area as well as to the proposed public trailhead at South Mountain Park. Adequate width, surface treatment, lighting, shade, and visibility will play an important role in ensuring the safety and comfort of users.

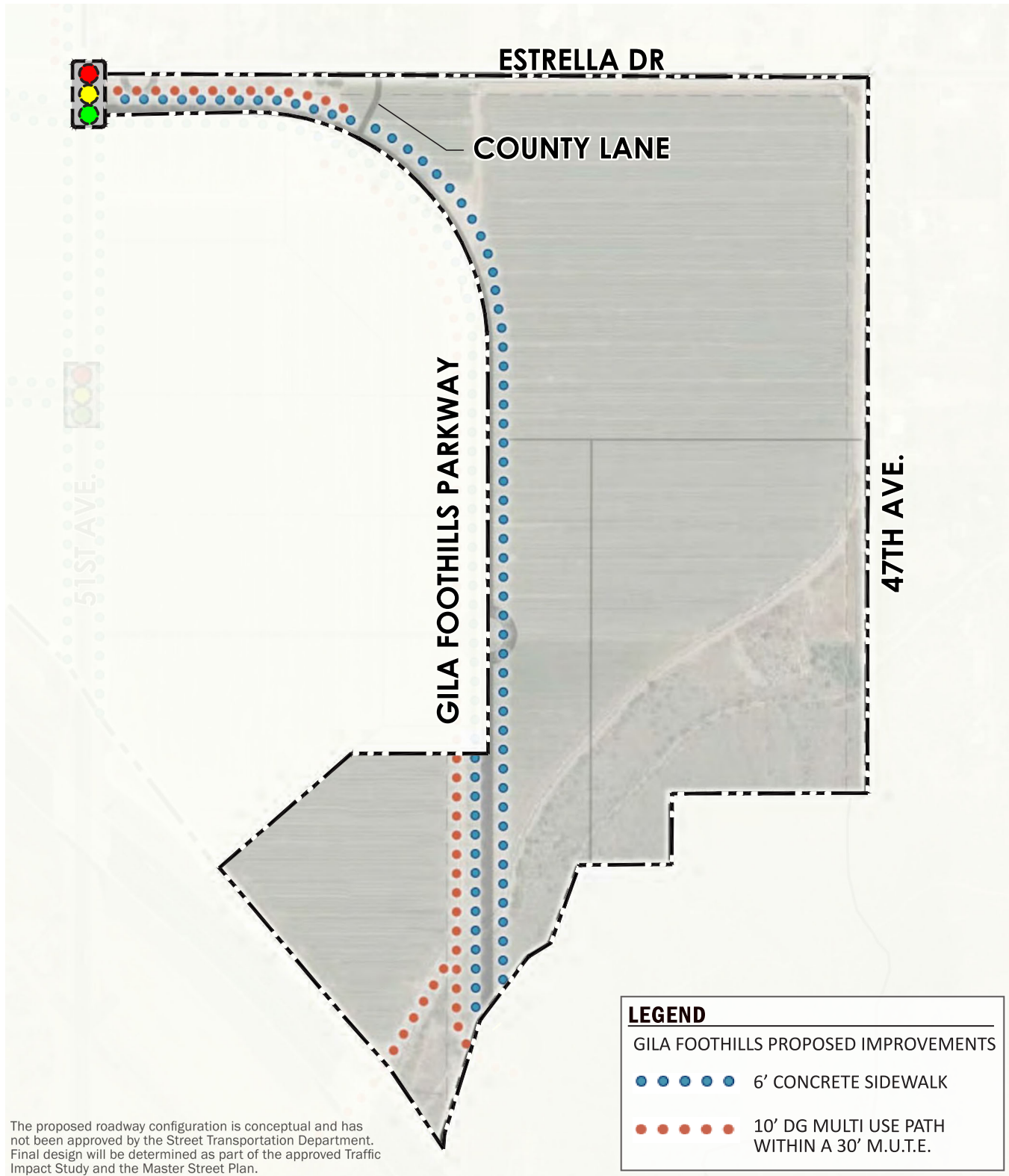
A variety of trail types and sidewalks are proposed across Gila Foothills to provide connectivity that is appropriate for the adjoining street intensity, building heights, and land uses.

#### Trail Connectivity to South Mountain Park

As part of the circulation plan, a new natural surface multi-use trail will extend along Gila Foothills Parkway from 51st Avenue to a new public trailhead that will be located at South Mountain Park. This trail will be a minimum of 8-feet wide and will meander along the west side of Gila Foothills Parkway. Along this multi-use path informal improvements for seating may be provided such as large boulders or low timber rails, as well as lean structures for bicycles or bicycle repair stands. Shading and landscaping along this trail will be as permitted within the overhead power easement. This may be limited to shrubs and ground cover.







The proposed roadway configuration is conceptual and has not been approved by the Street Transportation Department. Final design will be determined as part of the approved Traffic Impact Study and the Master Street Plan.

EXHIBIT F2 - Foothills Character Area Conceptual Trail Plan

N.T.S.

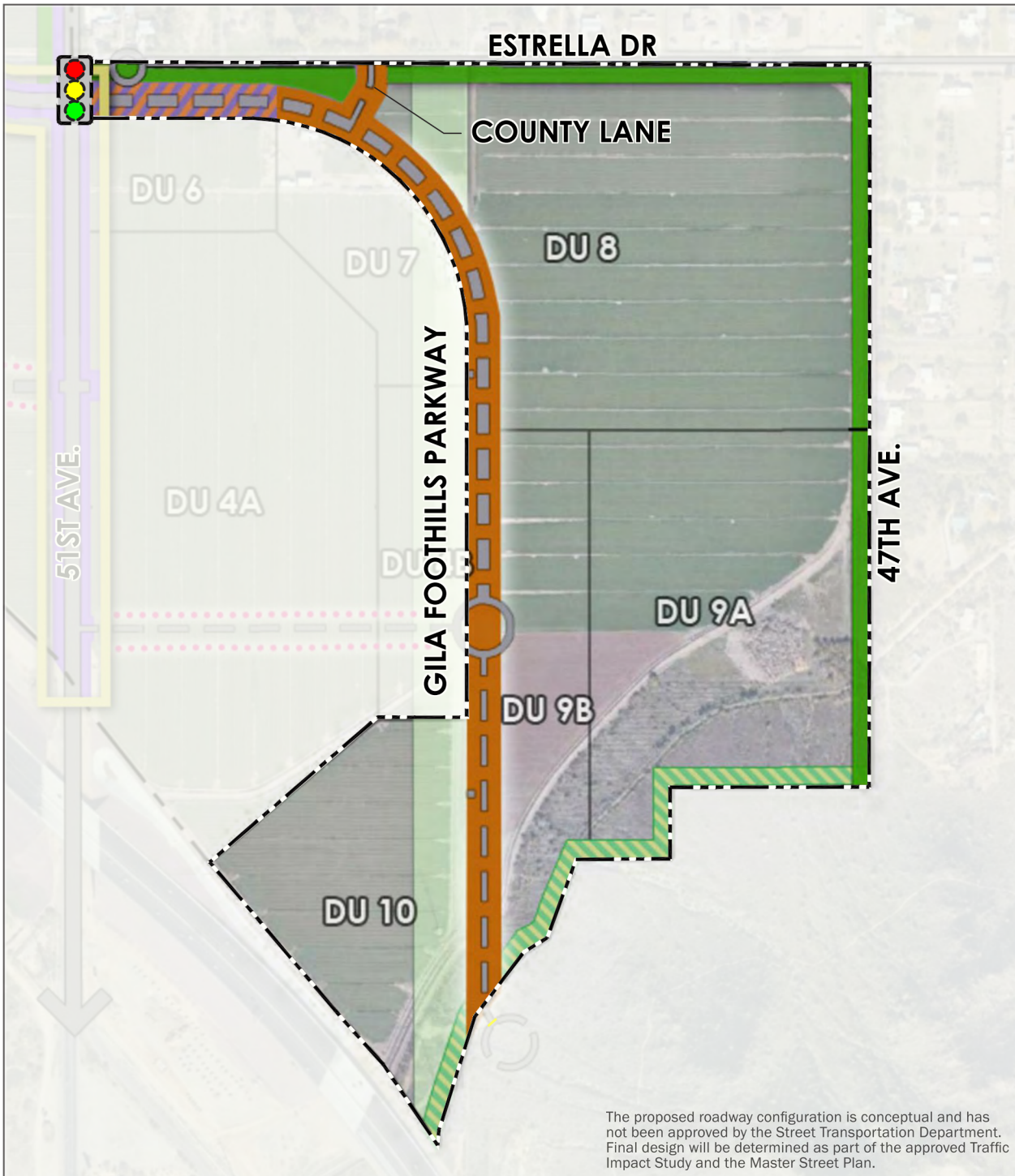


#### **d. Street Design and Landscape Character**

Within the Foothills Character Area, Gila Foothills Parkway will provide a connection from 51st Avenue to South Mountain Park and to adjoining Development Units. As shown in the Landscape Character Plan and the following conceptual street sections and plans, the landscaping and street design will transition from a formal appearance and street improvements for higher traffic closer to 51st Avenue to a natural appearance and tapered street improvements towards South Mountain Park.



*Landscape will transition from formal to informal along Gila Foothills Parkway and throughout the Foothills Character Area*



**LEGEND**

- |                          |                                    |                                   |
|--------------------------|------------------------------------|-----------------------------------|
| Gateway Zone             | WAPA Easement Zone                 | Shade Pocket Eligible Zone        |
| Formal Streetscape Zone  | South Mountain Perimeter Zone      | Transition from Formal to Natural |
| Natural Streetscape Zone | Open Space / Landscape Buffer Zone |                                   |

EXHIBIT F3- Foothills Character Area Landscape Character Plan

N.T.S.



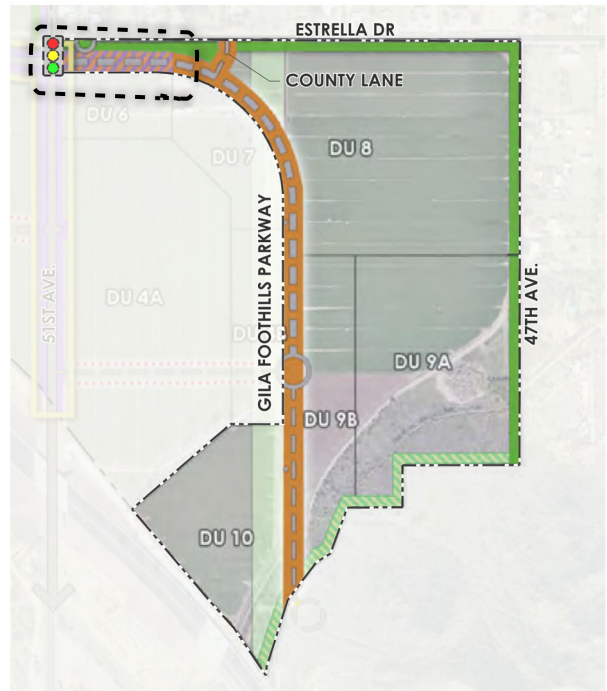
## Gila Foothills Parkway, Northern Segment

Exhibit F4 shows a conceptual street section of the northern segment of Gila Foothills Parkway as it will appear when viewed looking west from the existing overhead easement for Western Area Power Administration (“WAPA”).

As shown, two vehicle lanes and a bike lane in each direction will be separated by a landscaped median. Along the north side of the roadway, a detached 6-foot wide sidewalk and an 8-foot wide multi-use natural surface trail will be provided. On the south side of the roadway, a detached 6-foot wide sidewalk will be provided.

As regulated by the Landscape Standards included in Part 1 of this document, the center median of this street segment will alternate between Date Palms and Ironwood trees. The intentionally reduced use of Date Palms will visually relate to the prominent use of Date Palms on Estrella Drive west of 51st Avenue but also convey to drivers that Gila Foothills Parkway is not an extension of the Gateway Zone and does not provide access to the EV-focused auto campus or other major employment or commercial uses.

Outside of the roadway, landscaping will follow the City of Phoenix Standards for quantity, spacing, size, and shading. Regimented planting patterns and specific tree species are not applied to this street segment. These less formal requirements support the transition towards a natural street appearance further down Gila Foothills Parkway.



Location of the Gila Foothills Parkway Northern Segment conceptual street section and plan. Here, the Formal Streetscape Zone is transitioning to the Natural Streetscape Zone.

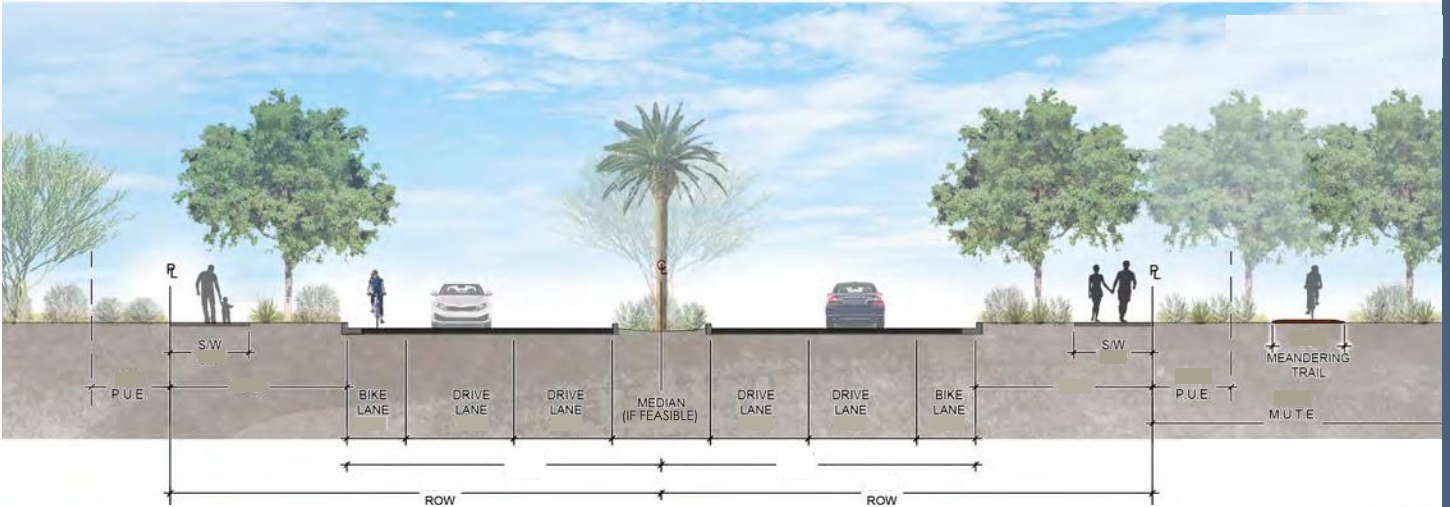


Date Palms and Ironwoods will alternate in this northern segment of Gila Foothills Parkway



The streetscape will be formal in appearance, but less formal than the west side of 51st Avenue. Fewer palms and more shade trees will be used within this northern segment of Gila Foothills Parkway.

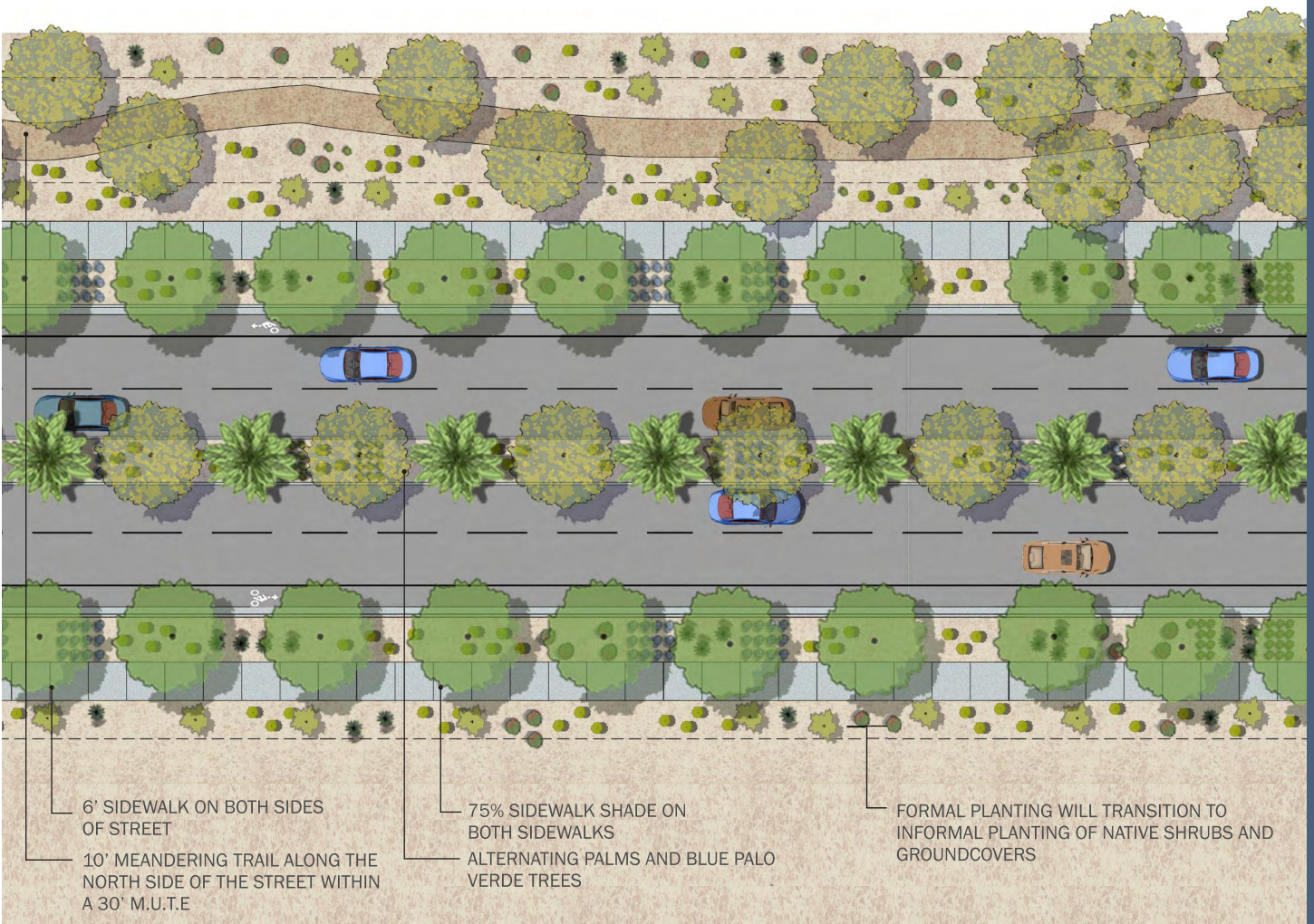




**EXHIBIT F4 - Conceptual Street Section - Gila Foothills Parkway, Northern Segment (View Looking West)**


This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.

N.T.S.



**EXHIBIT F5 - Conceptual Street Plan - Gila Foothills Parkway Northern Segment**

This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.

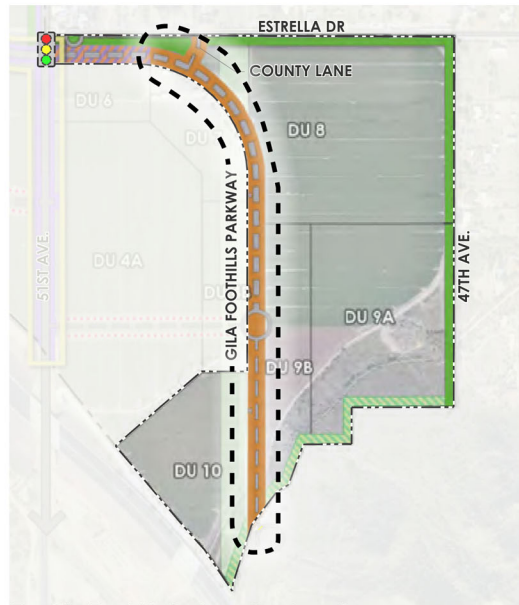
N.T.S. 



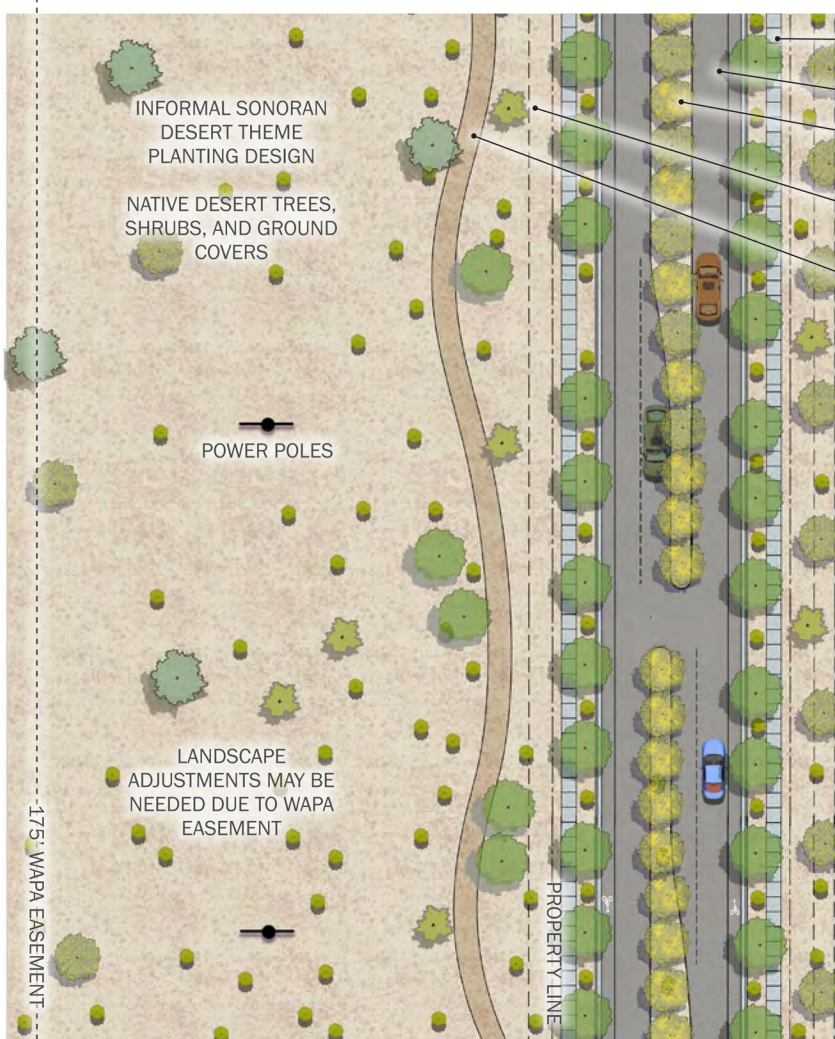
### Gila Foothills Parkway, Southern Segment

As Gila Foothills curves and extends to the south toward South Mountain Park, it takes on a natural appearance and follows along an existing 175-foot wide WAPA easement. This easement may limit opportunities for landscaping, streetscape and vertical improvements within proximity of the existing poles.

Exhibits F6, F7, F7.1 show the streetscape design following the assumption that Gila Foothills Parkway will need to be located outside of the easement. Later in the plan review process, if the WAPA easement allows for the road or a portion of the road to be located within the easement, the road may shift to the west.



The Gila Foothills Parkway Southern Segment conceptual street design is located between County Lane and South Mountain Park.



- 6' SIDEWALK & 75% SIDEWALK SHADING ON BOTH SIDES OF STREET
- 1 DRIVE LANE & 1 BIKE LANE IN EACH DIRECTION
- A CENTER LANDSCAPED MEDIAN WITH BREAKS FOR TURNING MOVEMENTS
- EAST BOUNDARY OF 175' WAPA EASEMENT & WEST BOUNDARY OF 8' P.U.E.
- 10' NATURAL SURFACE TRAIL MAY MEANDER AROUND ELECTRICAL POLES. TRAIL TO BE LOCATED WITHIN A M.U.T.E AS REQUIRED BY THE CITY OF PHOENIX.
- BEYOND THE TRAIL, PRIVATE PROPERTY OF DEVELOPMENT UNITS 4B, 7, & 10 MAY BE DEVELOPED AS PERMITTED BY THIS PUD, M.U.T.E, AND WAPA EASEMENT RESTRICTIONS

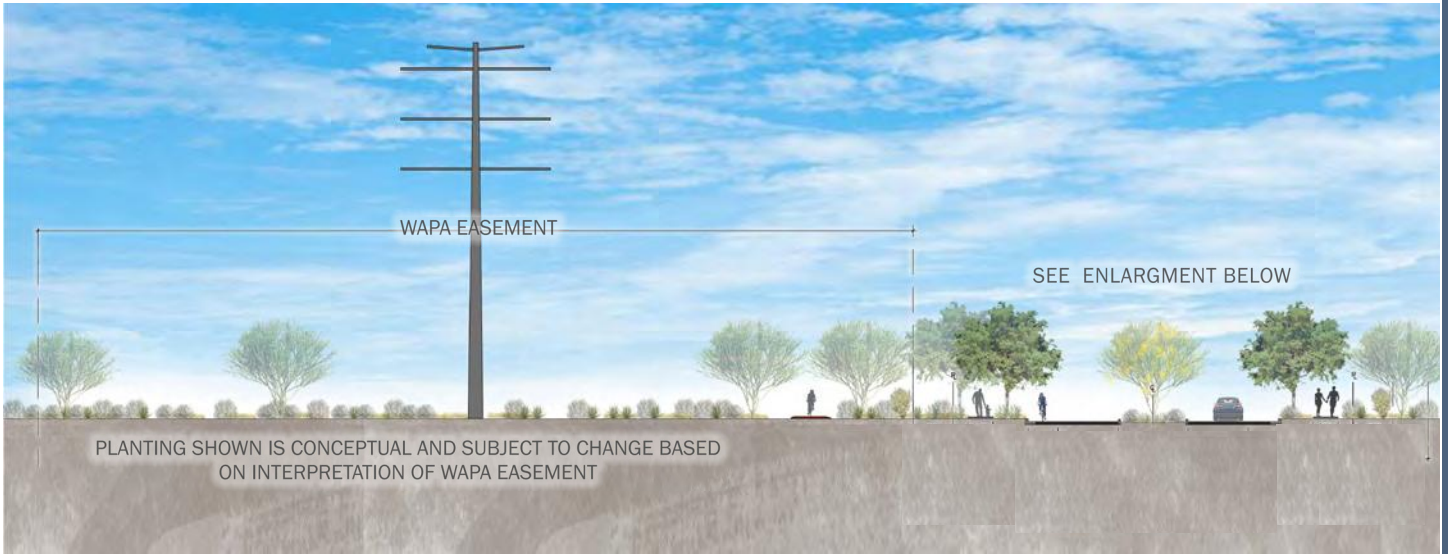
LANDSCAPING:  
 PER THE LANDSCAPE STANDARDS FOUND IN PART 1, MEDIAN TREES BEGIN BY ALTERNATING BETWEEN IRONWOODS AND PALO VERDES AND TRANSITION TO A ROW OF PALO BRES, BLUE PALO VERDES, AND DESERT MUSEUM PALO VERDES AS IT APPROACHES SOUTH MOUNTAIN PARK.  
 OUTSIDE OF THE ROADWAY, AS ALLOWED WITHIN THE WAPA EASEMENT ZONE, A VARIETY OF TREES FROM THE FOOTHILLS CHARACTER AREA PLANT PALETTE WILL BE PLANTED IN A NATURAL AND UNREGIMENTED WAY.

**EXHIBIT F6 - Conceptual Street Section - Gila Foothills Parkway Southern Segment (View Looking North)**

N.T.S.

This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.

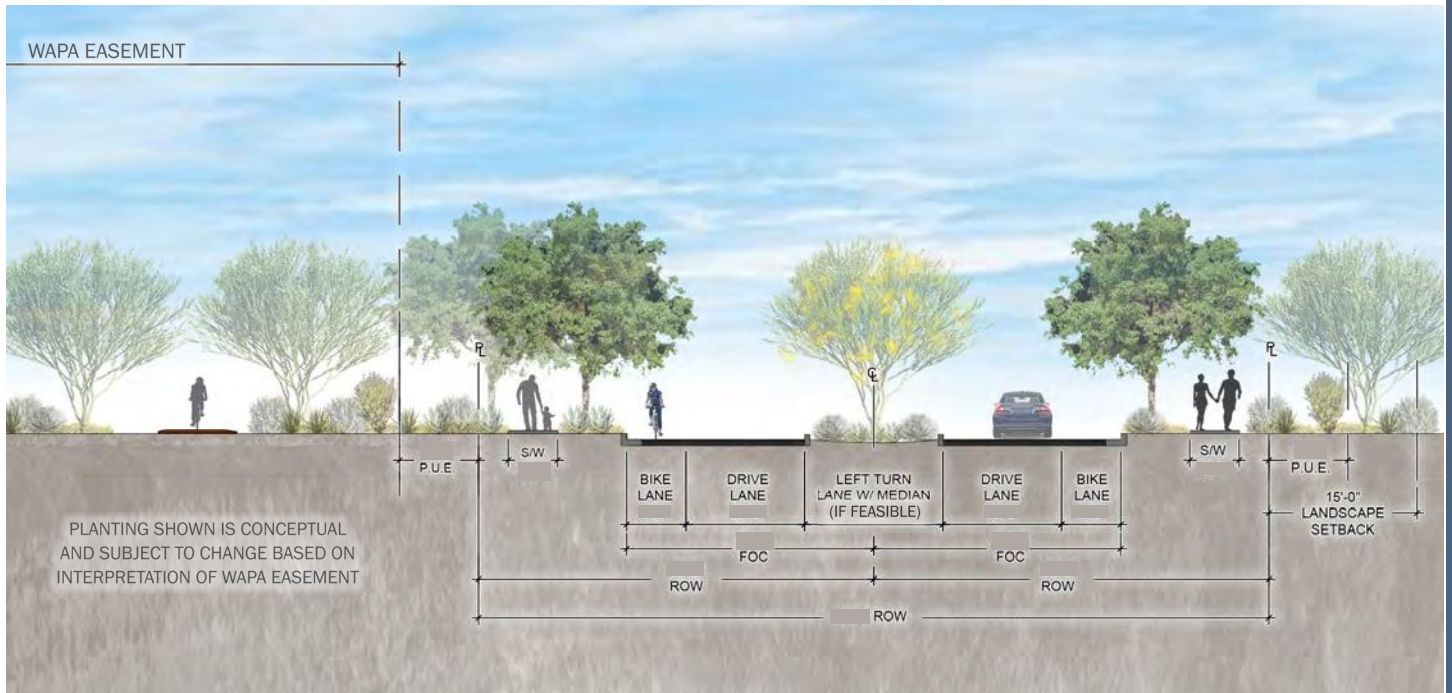




**EXHIBIT F7 - Conceptual Street Section - Gila Foothills Parkway Southern Segment (View Looking North)**

N.T.S.

This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.



**EXHIBIT F7.1 - Enlarged Conceptual Street Section - Gila Foothills Parkway Southern Segment (View Looking North)**

This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.



## 2.3. LANDSCAPING

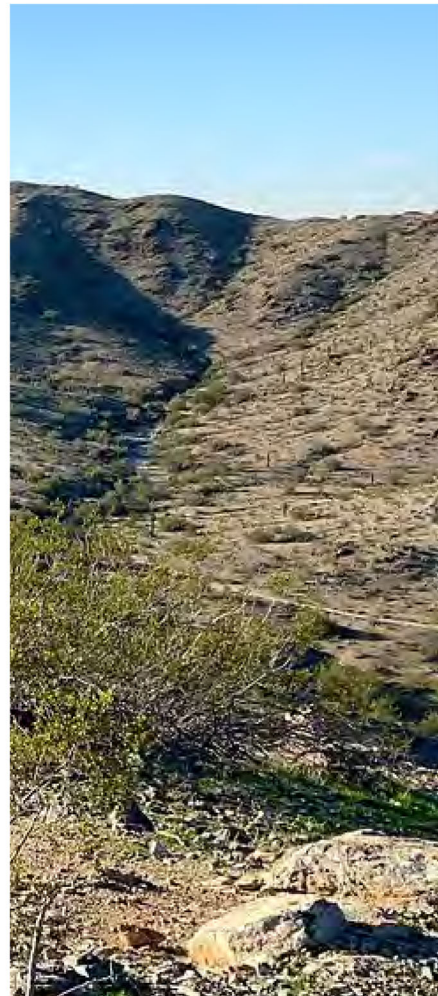
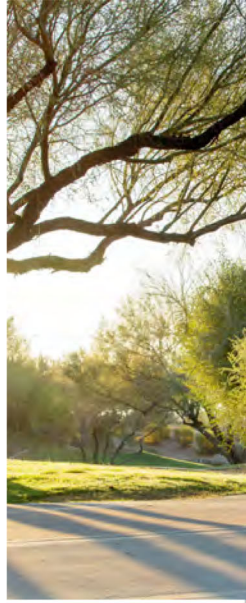
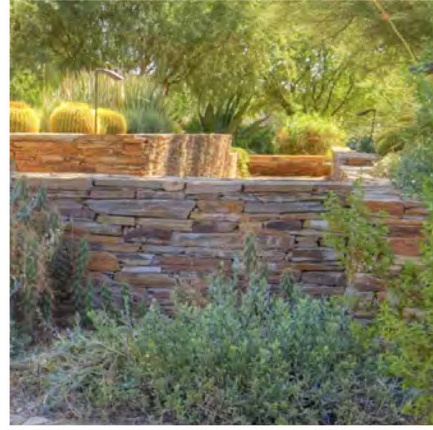
- Natural
- Tree-lined streets
- Mass planting
- Desert groves
- Informal planting
- Organic forms

### a. Landscaping Design Guidelines

Landscape design guidelines for The Foothills Character Area will ensure the variety of open spaces such as the landscape buffers, landscape setbacks, open spaces, and spaces encumbered by easements will be appropriately landscaped to provide a high-quality appearance that is comfortable to residents and other users such as customers of employment or commercial uses, surrounding neighborhoods, and people visiting South Mountain Park.

While these landscape guidelines apply to The Foothills Character Area, it is the intent that landscaping will support an overall unified character that extends and appropriately transitions throughout the Site. To help meet this goal, Exhibit DG3 Landscape Character Plan identifies landscape zones across all three Character Areas. These zones coordinate with the landscape guidelines below and the Gila Foothills Plant Palette (Appendix Attachment 1) to identify the best plants and landscape application for areas within each Character Area to help guide the landscape theming across the Site.

As shown on Exhibit DG3 Landscape Character Plan, the appearance of landscaping should transition from being more formal in appearance closer to 51st Avenue to a natural appearance closer to South Mountain Park, Estrella Drive, and 47th Avenue.







NATURAL LANDSCAPE





## **b. Site Walls, Screens, and Fences**

Thematic walls will be used within The Foothills Character Area. These walls will be comprised partially of opaque materials such as stone, brick, stucco, etc. as well as more transparent materials such as split rail style fencing.

Vertical and horizontal changes of materials, colors, and textures will be provided to ensure a high-quality appearance that reflects the surrounding Laveen character.

Thematic walls or fences will be used primarily to contribute to the character of the area and to identify property boundaries or provide privacy. Walls will be implemented with much consideration to not result in secluded areas or edges hidden from view from the public realm.

Opaque wall materials will primarily be used for a low base of a wall or accented columns. Character rich materials that are more transparent should be incorporated in all wall and fence designs.

Beyond contributing to the character of the area, low walls, fences, and seating walls may be used to guide users to desired areas and identify a transition from public to private spaces.

Any walls visible from the public right-of-way will have breaks to avoid a long blank face. Walls will be limited to a maximum length of 80-feet along a continuous plane, at which point the wall will be articulated with a setback, change of materials, use of artwork, or other design feature.

The maximum height along the Freeway will be 10-feet. The maximum height for fences or walls within or bounding a required rear or side yard is 6-feet in height at the property line. Columns and column caps may exceed the maximum wall height up to 4-inches.



*Conceptual wall imagery*



### c. Landscape Lighting

Subtle landscape lighting may be used to enhance the natural appearance of landscaping.

No colored lights or filters shall be used.

Conceal all transformers, switches and other electrical service elements.

### d. Miscellaneous Site Structures, Art, and Furnishings

Art monumentation, and furnishings should follow the aesthetic guidance of the Landscape Character Exhibit and transition from a formal appearance to a natural appearance. These improvements will be considered at the time of site plan and landscaping review. These features will coordinate with the surrounding building architecture, landscaping, and open spaces to enhance the character of the area.



*An example of using natural materials to create an edge or low wall*



*Conceptual landscape lighting imagery*



### EXHIBIT F8 - Foothills Character Area Open Space

This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.

N.T.S. 

## e. Open Spaces

Within The Foothills Character Area a public open space will be provided between the newly proposed road alignment and the cul-de-sac of Estrella Drive. This open space is meant to provide a meaningful separation between the Gila Foothills Site and the residential lots on Estrella Drive. Improvements within this open space will support a comfortable, enhanced, and shaded environment along the multi-use path and sidewalk that will follow along the new roadway alignment to the south. Landscaping, grading, low accent walls, or other improvements will be considered along Estrella Drive.

A variety of both public and private open spaces will be provided across The Foothills Character Area. These areas will be amenitized or landscaped to provide passive and active use by residents and members of the public.

Improved open spaces will be appropriate near the center of The Foothills Character Area. Along the perimeter, a more natural landscape design will appropriately transition from the existing residential neighborhoods to the east and relate to the natural setting of the South Mountain Park.



## f. Plant Palette

The use of native, well adapted, low water use and ADWR approved plants will enhance the overall theme and character of the development. The following Plant List provides a landscape palette for The Foothills Character Area.

Latin Name	Common Name
<b>TREES</b>	
Acacia aneura	Mulga
Acacia farnesiana	Desert Sweet Acacia
Acacia salicina	Willow Acacia
Acacia stenophylla	Shoestring Acacia
Caesalpinia cacalaco 'smoothie'	Thornless Cascalote
Chilopsis linearis	Desert Willow
Lysiloma microphylla	Desert Fern
Olneya tesota	Ironwood
Parkinsonia floridum	Blue Palo Verde
Parkinsonia x 'Desert Museum'	Desert Museum Palo Verde
Parkinsonia microphyllum	Foothill Palo Verde
Parkinsonia praecox	Palo Brea
Prosopis chilensis	Chilean Mesquite
Prosopis glandulosa	Texas Mesquite
Prosopis pubescens	Screwbean Mesquite
Prosopis x phoenix	Thornless Hybrid Mesquite
Prosopis velutina	Arizona Velvet Mesquite
Ungnadia speciosa	Mexican Buckeye
Vachellia farnesiana	Sweet acacia
<b>PALMS</b>	
Washingtonia filifera	California Fan Palm
Washingtonia robusta	Mexican Fan Palm
Phoenix dactylifera	Date Palm
<b>SHRUBS</b>	
Aloysia lycioides (gratissima)	Bee Bush
Ambrosia deltoidea	Triangleleaf bursage
Anisacanthus thurberi	Desert Honeysuckle
Aristida purpurea	Purple Three-awn
Asclepias linaria	Pine-leaf Milkweed
Atriplex l. breweri	Old Man Salt Bush
Bougainvillea spp.	Bougainvillea
Bouteloua curtipendula	Sideoats Grama
Caesalpinia gilliesii	Desert Bird of Paradise
Caesalpinia mexicana	Mexican Bird of Paradise
Calliandra californica	Baja Fairy Duster
Calliandra eriophylla	Pink Fairy Duster
Cassia artemisoides	Feathery Cassia
Cassia phyllodenia	Silver Leaf Cassia
Cassia wislizenii	Shrubby Cassia
Celtis pallida	Desert Hackberry
Cordia boissieri	Anacahuita
Cordia parvifolia	Little Leaf Cordia
Dalea pulchra	Pea Bush
Dodonaea viscosa	Hop Bush
Encelia farinosa	Brittlebush
Ephedra trifurca	Mormon Tea
Ericameria laricifolia	Turpentine Bush
Eremophila hygrophana 'Blue Bells'	Blue Bells Emu Bush
Eremophila maculata 'Valentine'	Valentine Bush
Eremophila racemosa	Easter Egg Emu Bush
Euphorbia antisiphylitica	Candellilla
Fouquieria splendens	Ocotillo
Justica californica	Chuparosa
Hyptis emoryi	Desert Lavender
Justica candicans	Red Justicia
Lantana camera	Bush Lantana
Larrea tridentata	Creosote Bush
Leucophyllum frutescens	'Compacta'
Leucophyllum laevigatum	Chihuahuan Sage
Leucophyllum zygophyllum	Blue Ranger
Muhlenbergia spp.	Muhly Grass
Nasella tenuis	Mexican Feather Grass
Rhus ovata	Sugar Bush
Rhus trilobata	Squaw Bush
Ruellia brittoniana	Mexican Petunia
Ruellia peninsularis	Baja Ruellia
Simmondsia Chinensis	Jojoba
Sophora secundiflora	Texas Mountain Laurel
Sphaeralcea ambigua	Globe Mallow
Tecoma stans v. angustata	Arizona Yellow Bells
Vauquelinia californica	Arizona Rosewood
<b>GROUNDCOVERS</b>	
Acacia redolens	Trailing acacia
Baccharis 'Centennial'	Desert Broom Hybrid
Baileya multiradiata	Desert Marigold
Chrysactinia mexicana	Damianita
Convolvulus cneorum	Bush Morning Glory
Dalea greggii	Trailing Indigo Bush
Dyssodia pentachaeta	Daisy
Eremophila glabra 'Mingenew Gold'	Outback Sunrise Emu Bush
Lantana montevidensis	Trailing Lantana
Oenothera berlandieri	Mexican Evening Primrose
Oenothera caespitosa	White Evening Primrose
Rosmarinus officinalis 'Prostratus'	Trailing Rosemary
Ruellia brittoniana 'Katie'	Katie Ruellia
Sphagneticola trilobata	Yellow Dot
Verbena gooddingii	Goodding's Verbena
Verbena rigida	Sandpaper Verbena
Zauschneria californica	California Fuchsia
Zinnia grandiflora	Prairie Zinna
<b>VINES</b>	
Antigonon leptopus	Vine
Macfadyena unguis-cati	Cat's Claw Vine
Mascagnia lilacaena	Lilac Orchid Vine
Merremia aurea	Yellow Morning Glory Vine
<b>ACCENTS</b>	
Agave desmettiana	Smooth Agave
Agave geminiflora	Twin-flowered Agave
Agave parryi	Perry's Agave
Agave vilmoriniana	Ocotopus Agave
Agave weberi	Weber's Agave
Aloe barbadensis	Medicinal Aloe
Aloe x 'Blue Elf'	Blue Elf Aloe
Asclepias subulata	Desert Milkweed
Carnegiea gigantea	Saguaro
Cereus hildmannianus	Hildmann's Cereus
Dasyliirion wheeleri	Desert Spoon / Sotol
Echinocactus grusonii	Golden Barrel
Echinocereus engelmannii	Hedgehog Cactus
Ferocactus acanthodes	Compass Barrel
Fouquieria splendens	Ocotillo
Hesperaloe funifera	Giant Hesperaloe
Hesperaloe parviflora	Red Yucca / Hesperaloe
Muhlenbergia rigens	Deer Grass
Opuntia acanthocarpa	Buckhorn Cholla
Opuntia basilaris	Beavertail Prickly Pear
Opuntia ficus-indica	Indian Fig
Opuntia phaeacantha	Engelmann's Prickly Pear
Opuntia violacea v. 'Santa Rita'	Purple Prickly Pear
Pachycereus marginatus	Mexican Fencepost
Pedilanthus macrocarpus	Lady Slipper
Penstemon pseudospectabilis	Canyon Penstemon
Penstemon superbus	Superb Penstemon
Stenocereus marginatus	Mexican Organ Pipe
Yucca baccata	Banana Yucca
Yucca elata	Soaptree Yucca
Yucca rigida	Blue Yucca





*Acacia aneura*  
MULGA



*Washingtonia robusta*  
MEXICAN FAN PALM



*Prosopis x Phoenix*  
THORNLESS MESQUITE



*Parkinsonia x 'Desert Museum'*  
Palo Verde



*Bouteloua gracilis 'Blond Ambition'*  
BLUE GRAMA GRASS



*Muhlenbergia lindheimeri*  
MUHLY GRASS



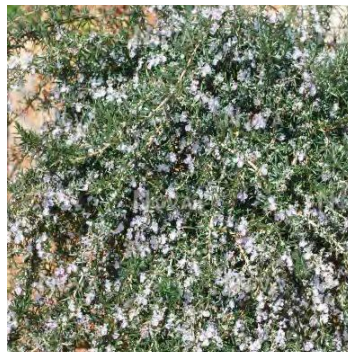
*Eremophila hygrophana* Blue Bells  
BLUE BELLS EMU BUSH



*Agave desmetiana*  
SMOOTH AGAVE



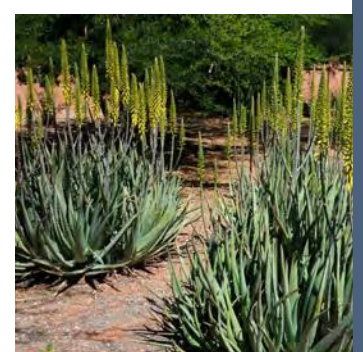
*Opuntia santa-rita*  
PURPLE PRICKLY PEAR



*Rosmarinus officinalis 'Huntington Carpet'*  
TRAILING ROSEMARY



*Lantana 'Dallas Red'*  
RED LANTANA



*Aloe barbadensis*  
MEDICINAL ALOE



*Dodonea viscosa*  
HOPBUSH



*Pachycereus marginatus*  
MEXICAN FENCEPOST



*Tecoma stans*  
YELLOW BELLS



*Dasylirion wheeleri*  
DESERT SPOON



## g. General Landscaping Requirements

Landscaping improvements shall occur on all ground that is not paved or otherwise covered and unsuitable for plant growth.

Berming may be used for screening.

Where boulders are set in landscape areas, they shall be set into berms and imbedded into the earth so as to replicate a natural rock outcrop or boulder formation, with a minimum of one-third of boulder to be imbedded.

The maximum surface gradient in landscape areas shall be 4:1 (four feet horizontal to 1 foot vertical).

The surface of landscape areas may have smooth transitions between changes in surface gradient, with rounded slope changes. Alternatively, landscaping may take on a more formalized appearance with vertical improvements to act as the perimeter, such as low retaining or seating walls, or low ornamental walls.

Cracked rock (rip-rap) may be used in drainage swales and as an accent ground cover in areas adjacent to buildings.

Caliper size of trees listed are minimum requirements. Larger tree sizes may be provided.

Courtyards, patios, pedestrian plazas, outdoor dining areas and exterior sales and display areas may use shrubs and groundcovers in addition to those on the approved list.

The use of turf and lush/green, high water plants are encouraged in high pedestrian use areas and gathering areas.

All planting must be in healthy growing condition at the time of installation.

### **i. Site Lighting**

In The Foothills Character Area, lighting will be designed to create an ambient and comfortable environment for pedestrians. Sources of light will be hidden such that visual glare is avoided and ambient light is provided. Bollard type lighting can be used along pedestrian walks and gathering areas. Pole mounted pedestrian lighting can be used but should be limited in height to create a comfortable human-scaled environment. Tall parking lot light poles will be limited and carefully designed to prohibit spillover into residential areas. All light sources shall be shielded to prevent light shining up past a horizontal plane. Landscape lighting will be carefully considered to create a memorable outdoor environment.

### **j. Perimeter Treatments**

Perimeter treatments shall be used to transition between the character areas and surrounding developments. The edge transitions shall also enhance the relationship and compatibility between the character areas and land uses.

With the exception of County Lane which connects Gila Foothills Parkway to Estrella Drive, the entirety of the perimeter of The Foothills Character area will be occupied by a 50-foot landscaped buffer and open space.

Landscape buffers will meet all applicable landscape standards of The City of Phoenix Zoning Code. Where the site is adjacent to South Mountain Park, development will be limited by all applicable Hillside Ordinance criteria. Overall, the proposed treatment of the perimeter will provide an extensive buffer to surrounding residential properties, and sensitively transition to South Mountain Park.



### 47th Avenue Perimeter Treatment

A 50-foot landscape buffer will extend from the back of the right-of-way line of 47th Avenue into The Foothills Character Area. Said landscape buffer will be planted with ornamental and shade trees, shrubs, and ground covers to screen view of development within The Foothills Character Area. Walkways and retention may also be located in this buffer.

No parking, signage, or privacy walls will be permitted within the landscape buffer. Privacy walls up to 6-feet in height may be located beyond the buffer to ensure development is adequately screened from view from 47th Street.

No private or public vehicular access to the Site from 47th Avenue will be provided. If required by the City of Phoenix, a gated emergency-only point of access may be provided on 47th Avenue.

### Estrella Drive Perimeter Treatment

A 50-foot landscape buffer will extend from the back of the right-of-way line of Estrella Drive into The Foothills Character Area. Said landscape buffer will be planted with ornamental and shade trees, shrubs, and ground covers to screen view of development within The Foothills Character Area.

No privacy walls will be permitted within the landscape buffer. Privacy walls up to 6-feet in height may be located beyond the buffer to ensure development is adequately screened from view from Estrella Drive.

Beyond the one proposed connector street, no private or public vehicular access to the Site from Estrella Drive will be provided. If required by the City of Phoenix, a gated emergency-only point of access may be provided on Estrella Drive.



**EXHIBIT F9 - Conceptual 50-Foot Landscape Buffer**

This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.

N.T.S. 

## South Mountain Perimeter Treatment

The following standards apply to the first 50-feet of Development Units 9A, 9B, and 10, as measured from the property line shared with South Mountain Park.

Public access will be provided to a City developed and maintained public trailhead in South Mountain Park.

Desert vegetation typically found in South Mountain will be used in the Perimeter Treatment Area to support survivability, minimize water use, and maintain a sense of place. Invasive plant species will be minimized.

Any signage located within the South Mountain Perimeter Treatment Area will be dimensional and externally illuminated or halo illuminated.

Box gabions, concrete channels, or riprap will not be used unless designed to look natural in the desert setting through color, texture, landscaping or other means.

Walls and fences will be designed to blend into the desert environment by addressing the color, materials, and alignment of the structure. Walls will vary by a minimum of four feet every four hundred lineal feet to visually reflect a meandering or staggered setback. All perimeter fences or walls adjacent to South Mountain Park will be view fencing or low seat walls.

Undisturbed, improved, or unimproved open spaces (public or private), no less than fifty by one hundred feet in dimension, shall adjoin sixty percent of the property line shared with South Mountain Park.

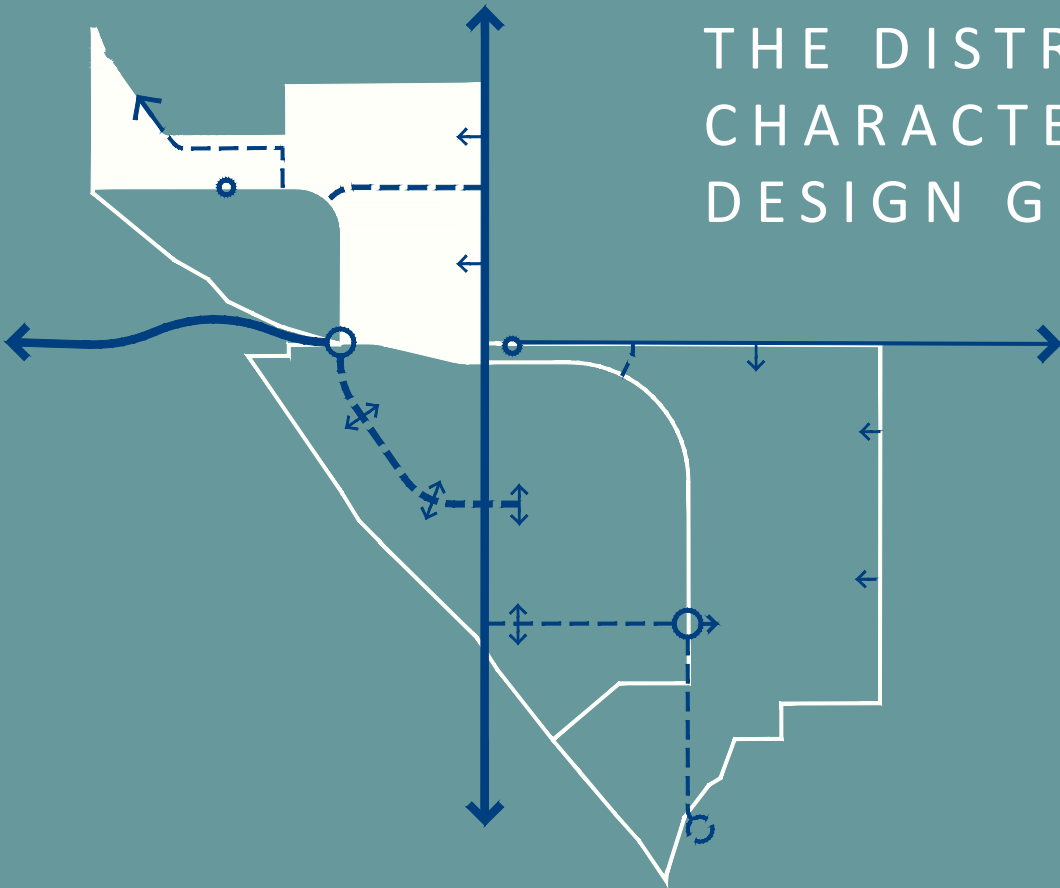
Drive aisles, parking areas, and streets will follow existing contours to the greatest extent possible to minimize cut and fill. They will also avoid riparian stands of vegetation and natural rock outcroppings to a reasonable extent.

Buildings located within the Perimeter Treatment Area will be limited to 30-feet (two stories) in height from the finished floor. One foot of additional height will be allowed for every two feet additional setback from South Mountain Park.





# THE DISTRICT CHARACTER AREA DESIGN GUIDELINES





## THE DISTRICT CHARACTER AREA

### 3.1. Site and Building Design

#### a. Site Planning / Building Placement

- Guidelines for The District Character Area encourage a dynamic variety of diverse building styles that are governed by a modern rural architectural style and connected by a coordinated and comfortable circulation system that engages pedestrians, bicyclists, and vehicles.
- The Design Guidelines for The District Character Area seek to integrate features from the surrounding Laveen residential character and support the proposed transition from the residential properties to The Hub Character Area and Freeway.
- Guidelines for this Character Area aim to distance building height and commercial traffic from the surrounding residential developments and extend the existing trail system south from the Tierra Montana neighborhood to South Mountain Park.
- Buildings and outdoor patios are located close to internal streets to create a walkable pedestrian and bicycle friendly environment.

#### b. Surface Parking

- In The District Character Area, pedestrians and bicyclists should have a direct access route to building entrances without navigating extensively through parking lots.
- Where parking is adjacent to a street, screening will be provided by a combination of an enhanced low screen wall and landscaping.





- Service entrances will be located to not disturb the reasonable enjoyment of on-site or off-site uses and shall be screened from view by landscaping, screening wall, or adjacent buildings.

### c. Architectural Style and Character

- As a transition from the single-family developments to the north and east of the Gila Foothills Site the modern rural architectural style in The District Character Area should consider the distinctive qualities of the surrounding context and, as appropriate, incorporate a refined version of those qualities in its design. Site layout, building massing, and building design should be sensitive to the evolving context of the area and reflect the transition from the historical Laveen residential character to the heightened intensity of The Hub Character Area and Freeway.
- Every building shall consider its relationship to the adjacent developments with regards to building placement, orientation, height, massing, and design vernacular. Larger buildings will be designed with projected and recessed facades to break up monotonous building masses. The design of smaller, single-story buildings should be strengthened by a simplified design of strong rooflines and fenestration to articulate the building façade.





- Design elements should be incorporated to create a heightened sense of detail on the building at the pedestrian level while incorporating shaded outdoor areas in their design to link with open space, hardscape, and landscape features.
- Modern rural and ranch inspired flat and gable rooflines, masonry, stone, and exposed columns to support deep overhangs should be used to provide shade and contribute to quality outdoor spaces that reflect the agricultural history of this part of Laveen.
- Articulated building facades are encouraged.

To accomplish the modern rural architectural theme envisioned for The District Character Area, a minimum of three of the following elements and/or the additional design characteristics as shown in Figure X: Architectural Design Character in the District Character Area are required:

- Pitched roofs.
- Rectangular gable roof form.
- Deep eaves.
- Exposed wood, structural wood decorative detailing.
- Operable shutters or windows that open to an outdoor dining or seating area.
- Rafter, beams, rustic style.
- Galvanized and corrugated metal accents.
- Metal/Composite wood-looking materials for pergolas.
- Board and batten, horizontal or vertical siding or shake.
- Brick.



#### d. Miscellaneous Exterior Building Materials and Design Imagery

Articulation of wall planes, a variety of forms and heights, and other architectural design elements shall be used to simultaneously create visual interest provide and avoid long, monotonous facades.

Below is a list of miscellaneous building materials, design elements and features to obtain a modern rural design aesthetic within The District Character Area:

##### Building Skin and Accent Materials:

- Architectural concrete, either cast-in-place or precast, natural or integral-color finish.
- CMU masonry or brick.
- Cut natural stone or faux stone.
- Traditional and Synthetic stucco (EIFS), integrally colored, smooth or textured finish.
- Metal panels, rusted or patina finish to give an aged look where glare and reflection are not objectionable.
- Wood. Engineered or natural (as appropriate for the desert climate).
- Cedar shake or Board and batten siding.
- Clear, tinted, or minimally reflective glazing.

##### Building Roofing:

- Concrete or clay tile with earth-tone color range.
- Cedar shake.
- Natural/coreten or patina-finish metal, seamed.
- Tubular metal (weathered/patina finish) or wood frame trellis.





**Window and Door Framing:**

- Anodized or painted aluminum.
- Painted or stained wood.
- Integrally colored PVC.
- Painted steel.

**Materials prohibited for exterior architectural finishes:**

- Exposed non-anodized aluminum.
- Mineral-fiber panels or facing materials.
- Mirrored finishes.
- Vinyl façade materials

**Awnings**

- Awnings, natural or synthetic fiber, material designed to resist sun exposure, non-reflective, non-translucent, colors to be uniform, integrated with the color palette of the site or building.

**Screening of Mechanical Equipment:**

- Roof mounted mechanical equipment on flat roof structures shall be screened by a parapet or architectural screening element. The parapet is to be as high as the highest component of mechanical equipment on the structure.
- The parapet shall be designed and constructed with forms, materials and colors which are compatible with the structure below.
- Roof mounted mechanical equipment on pitched roofs must be contained within the attic space and not visible from ground level.

- All ground-mounted mechanical equipment shall be screened with site walls exceeding the equipment's height by at least 2 feet with adjacent ornamental planting.
- All refuse collection, storage, compaction and processing containers and facilities shall be screened from all vantage points with a site wall and opaque (solid) gates exceeding the height of the facility by two feet with ornamental planting.

## 2 Circulation

### a. Primary Vehicular Entries and Circulation

The District Character Area is primarily accessed from 51st Avenue and Estrella Drive.

### b. Vehicular Access Restrictions

The extension of 55th Avenue indirectly connects to Estrella Drive and 51st Avenue. This indirect street layout has been intentionally designed to limit traffic traveling north on 55th Avenue towards the existing Tierra Montana neighborhood.



**EXHIBIT D1 - The District Character Area Circulation Plan**

This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.

N.T.S.



### c. Pedestrian and Trail Connectivity

- Pedestrian corridors are major walkways that link users to open spaces, employment areas, commercial establishments, and residential neighborhoods. They are characterized by shade, quality landscaping, and opportunities for pedestrian comfort, seating, and relaxation.
- Comfortable and safe pedestrian and bicycle routes will be provided to connect users from the surrounding areas, including the Tierra Montana neighborhood to establishments within The District Character Area as well as to the Foothills Character Area, and South Mountain. Adequate width, surface treatment, lighting, shade, and visibility will play an important role in ensuring the safety and comfort of users.
- A variety of trail types and sidewalks are proposed across Gila Foothills to provide connectivity that is appropriate for the adjoining street intensity, building heights, and land uses.
- Pedestrian access to businesses from parking areas and other transportation modes will be shaded and designed in recognition of the extreme summer climate and a need for seating and points of interest and design features to make walking a pleasant experience.



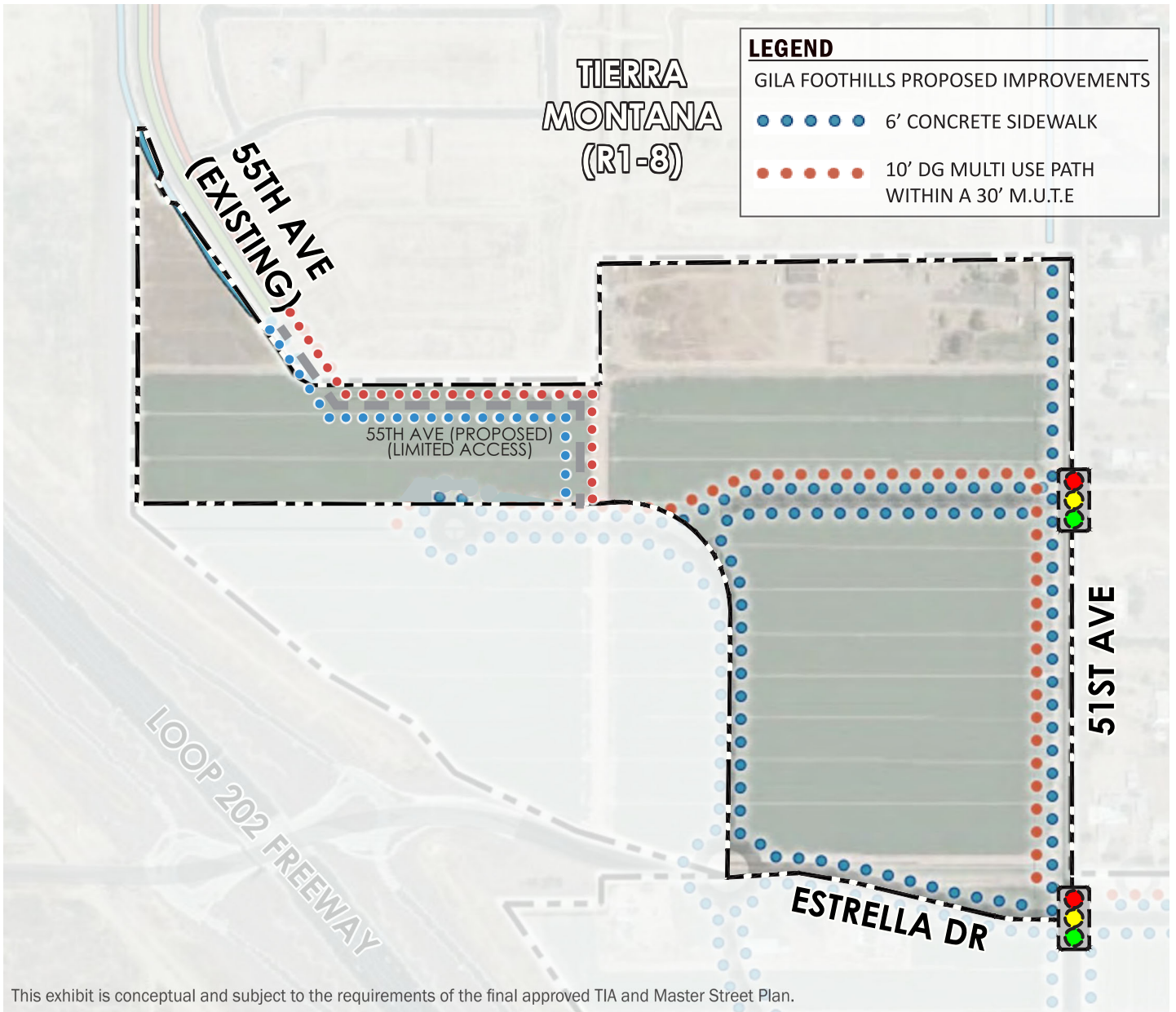


EXHIBIT D2 - The District Character Area Conceptual Trail Plan

N.T.S. ↻



## d. Street Design and Landscape Character

Internal streets span across The District Character Area to connect to 51st Avenue and Estrella Drive.

As shown in the Landscape Character Plan, these internal streets are not associated with any Landscape Character Zone. These streets are relatively short and will serve local commercial and residential traffic primarily. These internal streets will exceed the applicable City of Phoenix landscape standards including providing 75% sidewalk shade.

Estrella Drive is shown within the Gateway Character Zone along the south boundry of The District Character Area. This prominent roadway will provide a sense of arrival to the Site and is discussed more in the Hub Character Area Design Guidelines.

51st Avenue is classified as a Formal Steetscape zone as regimented linear landscaping along the sidewalk and natural surface path will ensure pedestrians have sufficient shade. This formal appearance will give way to a slightly more natural apperance within the 50-foot landscape buffer to the west.

The design of 51st Avenue is discussed in more detail on the following pages.



This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.

**LEGEND**

- Gateway Zone
- Formal Streetscape Zone
- Open Space / Landscape Buffer Zone

EXHIBIT D3 - The District Character Area Landscape Character Plan

N.T.S.



### 51st Avenue (North of Estrella)

The conceptual street section and plan on the following page show improvements for 51st Avenue north of Estrella Drive as viewed looking north.

As shown, the street design includes a natural surface trail and a 6-foot wide detached sidewalk on the west side of the roadway and two vehicular lanes will travel in each direction. In the center, a landscaped median and turning lane are planned.



Location of 51st Avenue (North of Estrella) Conceptual Street Section. This street segment is identified as a Formal Streetscape Zone.

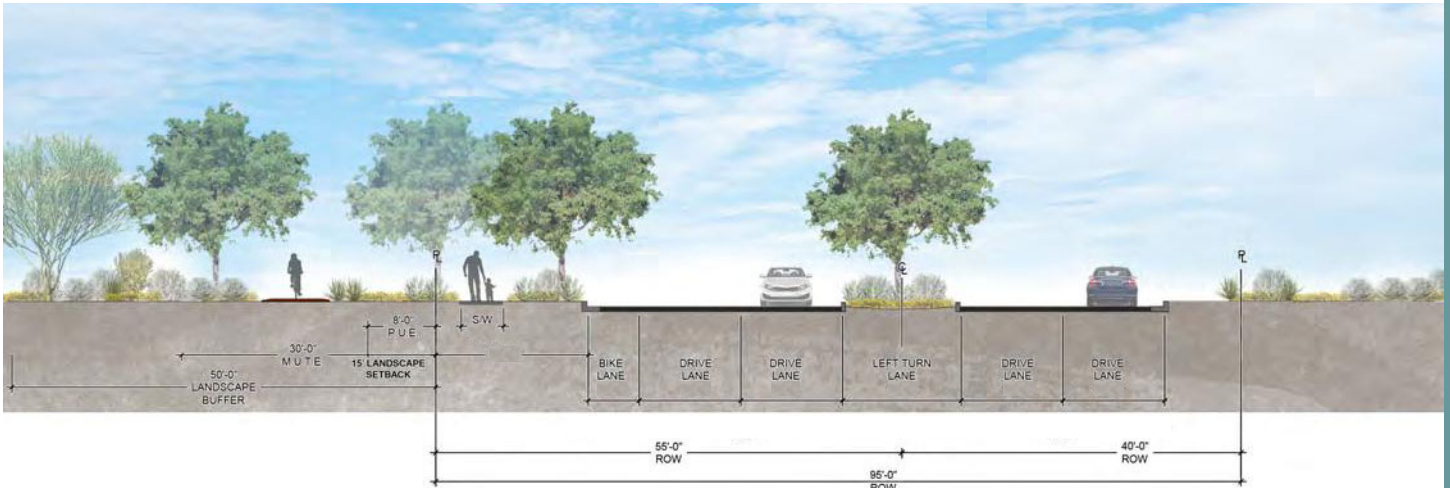


A large landscape buffer will separate buildings from 51st Avenue.



Buildings may be located closer to internal streets within the District Character Area to create an active and comfortable pedestrian environment

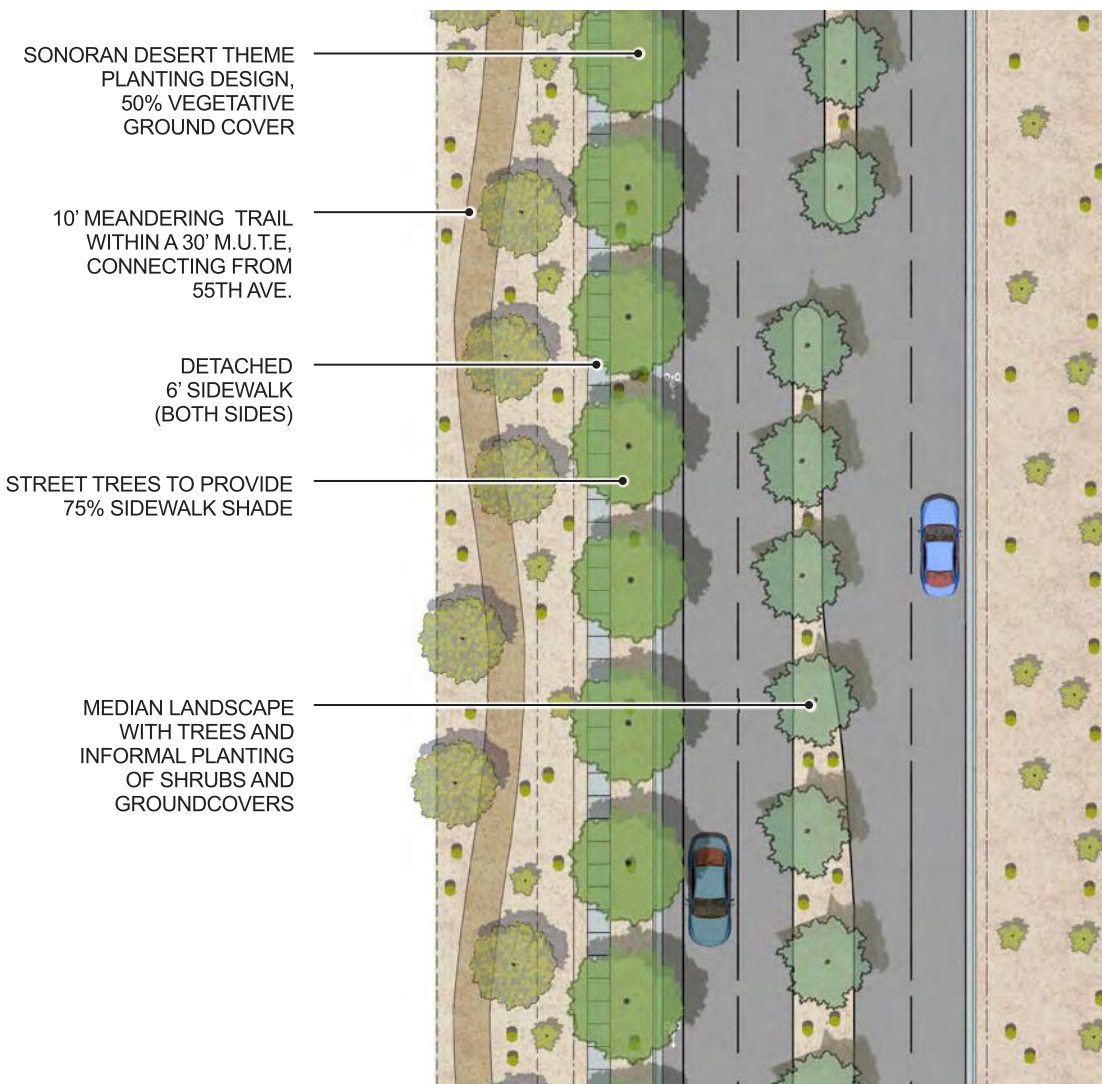




**EXHIBIT D4- Conceptual Street Section - 51st Avenue, North of Estrella (View Looking North)**

This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.

N.T.S.



**EXHIBIT D5 - Conceptual Street Plan - 51st Avenue, North of Estrella**

This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.

N.T.S.



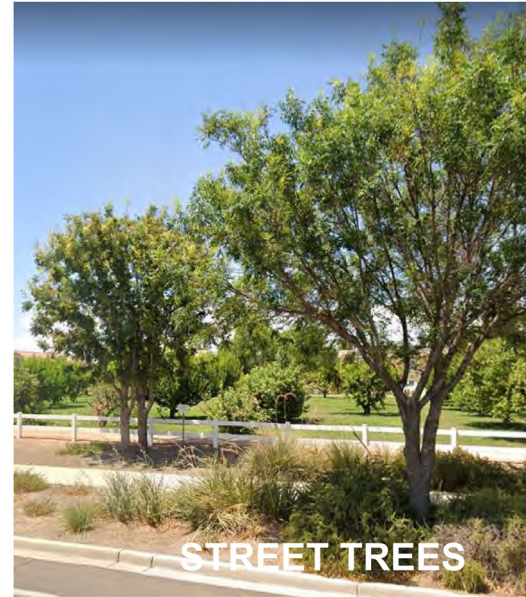
### 3. LANDSCAPING

- Linear Rows
- Repetition of Elements
- Grid Pattern
- Geometric Design
- Grove of Shade Trees
- Tree-Lined Streets
- Mass Planting
- Formal Planting
- Modern

#### Landscape Design Guidelines

Landscape design guidelines for the District Character Area will ensure the 50-foot landscape buffers, landscape setbacks along street frontages, and open spaces will be appropriately landscaped to provide a high-quality appearance that is comfortable to residents and other users such as neighbors of the Tierra Montana neighborhood or customers of the commercial uses.

While these landscape guidelines apply to the District Character Area, it is the intent that landscaping will support an overall unified character that extends and appropriately transitions throughout The Gila Foothills Site. To help meet this goal, Exhibit DG3 Landscape Character Plan identifies zones across all three Character Areas. These zones coordinate with the landscape guidelines and the Gila Foothills Plant Palette (Appendix Attachment 1) to identify the best plants and landscape application for areas within each Character Area to help guide the landscape theming across the Site.







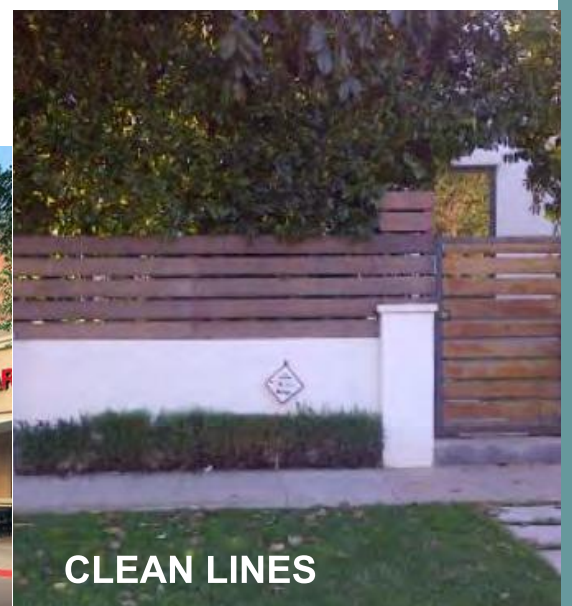
**LINEAR PATTERN**



**SIDEWALK PLANTERS**



**STREET LANDSCAPING**



**CLEAN LINES**



### a. Site Walls, Screens, and Fences

- Vertical and horizontal changes of materials, colors, and textures will be provided to ensure a high-quality appearance that reflects the surrounding Laveen character.
- Thematic walls or fences will be used primarily to contribute to the character of the area and to identify property boundaries or provide privacy. Walls will be implemented with much consideration to not result in secluded areas or edges hidden from view from the public realm.
- Opaque wall materials will primarily be used for a low base of a wall or accented columns. Character-rich materials that are more transparent should be incorporated in all wall and fence designs.
- Beyond contributing to the character of the area, low walls, fences, and seating walls may be used to guide users to desired areas and identify a transition from public to private spaces.
- Any walls visible from the public right-of-way will have breaks to avoid a long blank face. Walls will be limited to a maximum length of 80-feet along a continuous plane, at which point the wall will be articulated with a setback, change of materials, use of artwork, or other design feature.
- The maximum height for fences or walls within or bounding a required rear or side yard is 6-feet in height at the property line. Columns and column caps may exceed the maximum wall height up to 4-inches.
- Parking area screen walls will be a minimum of 36-inches in height and a maximum of 40-inches in height.



## b. Landscape Lighting

- No colored lights or filters shall be used.
- Conceal all transformers, switches and other electrical service elements.

## c. Miscellaneous Site Structures, Art, and Furnishings

Art monumentation, and furnishings should follow the aesthetic guidance of the Landscape Character Exhibit and transition from a formal appearance to a natural appearance. These improvements will be considered at the time of site plan and landscaping review. These features will coordinate with the surrounding building architecture, landscaping, and open spaces to enhance the character of the area.

## d. Open Spaces

- A variety of both public and private open spaces will be provided in The District Character Area. These areas will be amenitized or landscaped to provide passive or active use by residents and members of the public.
- Open spaces are proposed to provide meaningful landscape buffers to the existing residential lots of the adjacent Tierra Montana Neighborhood and large residential lots east of 51st Avenue.
- Open spaces will visually and physically connect uses by providing walkways, gathering, and activity areas.
- Open spaces in The District Character area will be placed between buildings and public sidewalks to support a lively and walkable environment.
- Open spaces may be compact but carefully designed to provide enjoyable outdoor dining and seating areas. Instead of large naturally planted areas, smaller usable spaces may be densely planted to create impactful separations from closely spaced or adjoining land uses.
- Commercial open spaces should be shaded or misted, and illuminated as necessary to be usable during all hours of operations.
- Open spaces here should be visible from the public right-of-way where it is reasonably possible, and should neighbor multi-use paths or sidewalks.
- Outdoor, people-oriented spaces shall provide visual and pedestrian connections to adjacent interior building space, common open space, plazas, and/or adjacent public sidewalks.
- Pedestrian oriented spaces shall incorporate shading elements. Patios and courtyard shall be incorporated as part of private development to complement and enhance the design of the buildings and invite visitors to relax and enjoy activities.
- Common open space shall be designed to create a sense of place and should be used as opportunities to allow for gathering areas and may include such things but not limited to outdoor entertainment, farmers market space, benches, gathering spaces, fountains, shade elements, and play areas.



## e. Plant Palette

The use of native, well adapted, low water use and ADWR approved plants will enhance the overall theme and character of the development. The following Plant List provides a landscape palette for the District Character Area.

Latin Name	Common Name
<b>TREES</b>	
<i>Acacia aneura</i>	Mulga
<i>Acacia salicina</i>	Willow Acacia
<i>Caesalpinia cacalaco 'smoothie'</i>	Thornless Cascalote
<i>Chilopsis linearis</i>	Desert Willow
<i>Ebenopsis ebano</i>	Texas Ebony
<i>Fraxinus velutina "Fan tex"</i>	Fan Tex Ash
<i>Olea europea 'Swan Hill'</i>	Olive Tree (Fruitless)
<i>Parkinsonia x 'Desert Museum'</i>	Desert Museum Palo Verde
<i>Pistachia chinensis</i>	Chinese pistache
<i>Prosopis chilensis</i>	Chilean Mesquite
<i>Prosopis x phoenix</i>	Thornless Hybrid Mesquite
<i>Ulmus parvifolia</i>	Evergreen Elm
<i>Vitex agnus-castus</i>	Chaste tree
<b>PALMS</b>	
<i>Bismarckia nobilis</i>	Bismark Palm
<i>Phoenix dactylifera</i>	Date Palm
<i>Washingtonia filifera</i>	California Fan Palm
<i>Washingtonia robusta</i>	Mexican Fan Palm
<b>SHRUBS</b>	
<i>Aristida purpurea</i>	Purple Three-awn
<i>Atriplex l. breweri</i>	Old Man Salt Bush
<i>Bougainvillea spp.</i>	Bougainvillea
<i>Bouteloua curtipendula</i>	Sideoats Grama
<i>Caesalpinia gilliesii</i>	Desert Bird of Paradise
<i>Caesalpinia mexicana</i>	Mexican Bird of Paradise
<i>Calliandra californica</i>	Baja Fairy Duster
<i>Calliandra eriophylla</i>	Pink Fairy Duster
<i>Cassia artemisoides</i>	Feathery Cassia
<i>Cassia phyllodenia</i>	Silver Leaf Cassia
<i>Cassia wislizenii</i>	Shrubby Cassia
<i>Cordia boissieri</i>	Anacahuita
<i>Cordia parvifolia</i>	Little Leaf Cordia
<i>Dalea pulchra</i>	Pea Bush
<i>Dodonaea viscosa</i>	Hop Bush
<i>Encelia farinosa</i>	Brittlebush
<i>Ephedra trifurca</i>	Mormon Tea
<i>Eremophila hygrophana 'Blue Bells'</i>	Blue Bells Emu Bush
<i>Eremophila maculata 'Valentine'</i>	Valentine Bush
<i>Eremophila racemosa</i>	Easter Egg Emu Bush
<i>Eragrostis fasciculatum</i>	California Buckwheat
<i>Euphorbia antisyphilitica</i>	Candellila
<i>Fouquieria splendens</i>	Ocotillo
<i>Justica californica</i>	Chuparosa
<i>Hyptis emoryi</i>	Desert Lavender
<i>Justica candicans</i>	Red Justica
<i>Justica spicigera</i>	Mexican Honeysuckle
<i>Lantana camera</i>	Bush Lantana
<i>Larrea tridentata</i>	Creosote Bush
<i>Leucophyllum candidum</i>	Cenizo
<i>Leucophyllum frutescens</i>	Texas Sage
<i>Leucophyllum langmanniae</i>	Rio Bravo Sage
<i>Leucophyllum zygophyllum</i>	Blue Ranger
<i>Muhlenbergia spp.</i>	Muhly Grass
<i>Nasella tenuis</i>	Mexican Feather Grass
<i>Ruellia brittoniana</i>	Mexican Petunia
<i>Ruellia peninsularis</i>	Baja Ruellia
<i>Simmondsia Chinensis</i>	Jojoba
<i>Sophora secundiflora</i>	Texas Mountain Laurel
<i>Sphaeralcea ambigua</i>	Globe Mallow
<i>Tecoma stans v. angustata</i>	Arizona Yellow Bells
<i>Vauquelinia californica</i>	Arizona Rosewood

Latin Name	Common Name
<b>GROUNDCOVERS</b>	
<i>Acacia redolens</i>	Trailing acacia
<i>Baccharis 'Centennial'</i>	Desert Broom Hybrid
<i>Baileya multiradiata</i>	Desert Marigold
<i>Chrysactinia mexicana</i>	Damianita
<i>Convolvulus cneorum</i>	Bush Morning Glory
<i>Dalea greggii</i>	Trailing Indigo Bush
<i>Dyssodia pentachaeta</i>	Daisy
<i>Eremophila glabra 'Mingenew Gold'</i>	Outback Sunrise Emu Bush
<i>Lantana montevidensis</i>	Trailing Lantana
<i>Oenothera berlandieri</i>	Mexican Evening Primrose
<i>Oenothera caespitosa</i>	White Evening Primrose
<i>Rosmarinus officinalis 'Prostratus'</i>	Trailing Rosemary
<i>Ruellia brittoniana 'Katie'</i>	Katie Ruellia
<i>Sphagneticola trilobata</i>	Yellow Dot
<i>Verbena rigida</i>	Sandpaper Verbena
<b>VINES</b>	
<i>Antigonon leptopus</i>	Vine
<i>Macfadyena unguis-cati</i>	Cat's Claw Vine
<i>Mascagnia lilacaena</i>	Lilac Orchid Vine
<i>Merremia aurea</i>	Yellow Morning Glory Vine
<b>ACCENTS</b>	
<i>Agave desmettiana</i>	Smooth Agave
<i>Agave geminiflora</i>	Twin-flowered Agave
<i>Agave parryi</i>	Perry's Agave
<i>Agave vilmoriniana</i>	Octopus Agave
<i>Agave weberi</i>	Weber's Agave
<i>Aloe barbadensis</i>	Medicinal Aloe
<i>Aloe x 'Blue Elf'</i>	Blue Elf Aloe
<i>Asclepias subulata</i>	Desert Milkweed
<i>Carnegiea gigantea</i>	Saguaro
<i>Cereus hildmannianus</i>	Hildmann's Cereus
<i>Dasyliroton wheeleri</i>	Desert Spoon / Sotol
<i>Echinocactus grusonii</i>	Golden Barrel
<i>Echinocereus engelmannii</i>	Hedgehog Cactus
<i>Ferocactus acanthodes</i>	Compass Barrel
<i>Fouquieria splendens</i>	Ocotillo
<i>Hesperaloe funifera</i>	Giant Hesperaloe
<i>Hesperaloe parviflora</i>	Red Yucca / Hesperaloe
<i>Lophocereus schottii</i>	Cactus
<i>Muhlenbergia rigens</i>	Deer Grass
<i>Opuntia acanthocarpa</i>	Buckhorn Cholla
<i>Opuntia basilaris</i>	Beavertail Prickly Pear
<i>Opuntia ficus-indica</i>	Indian Fig
<i>Opuntia phaeacantha</i>	Engelmann's Prickly Pear
<i>Opuntia violacea v. 'Santa Rita'</i>	Purple Prickly Pear
<i>Pachycereus marginatus</i>	Mexican Fencepost
<i>Pedilanthus macrocarpus</i>	Lady Slipper
<i>Penstemon pseudospectabilis</i>	Canyon Penstemon
<i>Penstemon superbus</i>	Superb Penstemon
<i>Stenocereus marginatus</i>	Mexican Organ Pipe
<i>Yucca baccata</i>	Banana Yucca
<i>Yucca elata</i>	Soaptree Yucca
<i>Yucca rigida</i>	Blue Yucca





*Caesalpinia cacalaco*  
CASCALOTE



*Washingtonia robusta*  
MEXICAN FAN PALM



*Ulmus parvifolia*  
EVERGREEN ELM



*Vitex agnus-castus*  
CHASTE TREE



*Pachycereus marginatus*  
MEXICAN FENCEPOST



*Agave desmetiana*  
SMOOTH AGAVE



*Opuntia santa-rita*  
PURPLE PRICKLY PEAR



*Dalea greggii*  
TRAIL INDIGO BUSH



*Aristida purpurea*  
PURPLE THREE-AWN GRASS



*Nasella tenuis*  
MEXICAN FEATHER GRASS



*Acacia redolens* 'Desert Carpet'  
TRAILING ACACIA



*Lantana x 'New Gold'*  
NEW GOLD LANTANA



*Bougainvillea* spp.  
BOUGAINVILLEA



*Dasyliirion wheeleri*  
DESERT SPOON



*Aloe x Blue Elf*  
BLUE ELF ALOE



*Tecoma stans*  
ARIZONA YELLOW BELLS



## f. General Landscape Requirements

- Landscaping improvements shall occur on all ground that is not paved or otherwise covered and unsuitable for plant growth.
- Berming may be used for screening objectionable views. Berms should primarily be used within landscape buffers. Where used along internal collector or local street frontages, berms should be limited in scale and not conflict with direct bicycle and pedestrian routes.
- Where boulders are set in landscape areas, they shall be set into berms and imbedded into the earth so as to replicate a natural rock outcrop or boulder formation, with a minimum of one-third of the boulder to be imbedded.
- The maximum surface gradient in landscape areas shall be 4:1 (four feet horizontal to 1 foot vertical).
- The surface of landscape areas may have smooth transitions between changes in surface gradient, with rounded slope changes. Alternatively, landscaping may take on a more formalized appearance with vertical improvements to act as the perimeter, such as low retaining or seating walls, or low ornamental walls.
- Cracked rock (rip-rap) may be used in drainage swales and as an accent ground cover in areas adjacent to buildings.
- Caliper size of trees listed are minimum requirements. Larger tree sizes may be provided.
- Courtyards, patios, pedestrian plazas, outdoor dining areas and exterior sales and display areas may use shrubs and groundcovers in addition to those on the approved list
- The use of turf and lush/green, high water plants are encouraged in high pedestrian use areas and gathering areas
- All planting must be in healthy growing condition at the time of installation.

## H. Site Lighting

In The District Character Area, lighting will be designed to not detract from the night sky. Site lighting will support a community-friendly and vibrant environment for pedestrians.

Sources of light will be hidden such that visual glare is avoided and ambient light is provided. Bollard type lighting can be used along pedestrian walks and gathering areas. Pole mounted pedestrian lighting can be used but should be limited in height to create a comfortable human-scaled environment. Tall parking lot light poles will be limited and carefully designed to avoid spillover into adjoining residential Sites. All light sources shall be shielded to prevent light shing up past a horizontal plane. Landscape lighting will be carefully considered to create a memorable outdoor environment.



## I. Landscape Buffer

Landscape Buffers will be used to transition to surrounding land uses. Careful thought was given to transitioning to the Tierra Montana neighborhood to the north, and towards 51st Avenue and the county properties to the east.

### Landscape Buffer Adjacent to Tierra Montana

A 50-foot landscape buffer will extend into the site from the property line shared with the Tierra Montana neighborhood.

In conjunction with the existing neighborhood privacy theme wall the 50-foot buffer will provide ample space to successfully transition from the existing one and two-story homes to any proposed development.

This landscape buffer will be planted with ornamental and shade trees, shrubs, and ground covers. Retention may also be located in this buffer.

To ensure an adequate transition and buffer is provided parking and signage may not be located in this buffer.

Along 55th Avenue, a multi-use path may be located within the buffer.

### 51st Avenue Landscape Buffer

A 50-foot landscape buffer will extend into the site from the property line adjacent to 51st Avenue.

This buffer may be improved with retention, sidewalks, and multi-use trails. This buffer will serve to screen the visibility of buildings over 30-feet in height located within The District Character Area as well as The Hub Character Area.

This buffer will be planted with ornamental and shade trees, shrubs, and ground covers. Retention and walkways may also be located in this buffer.



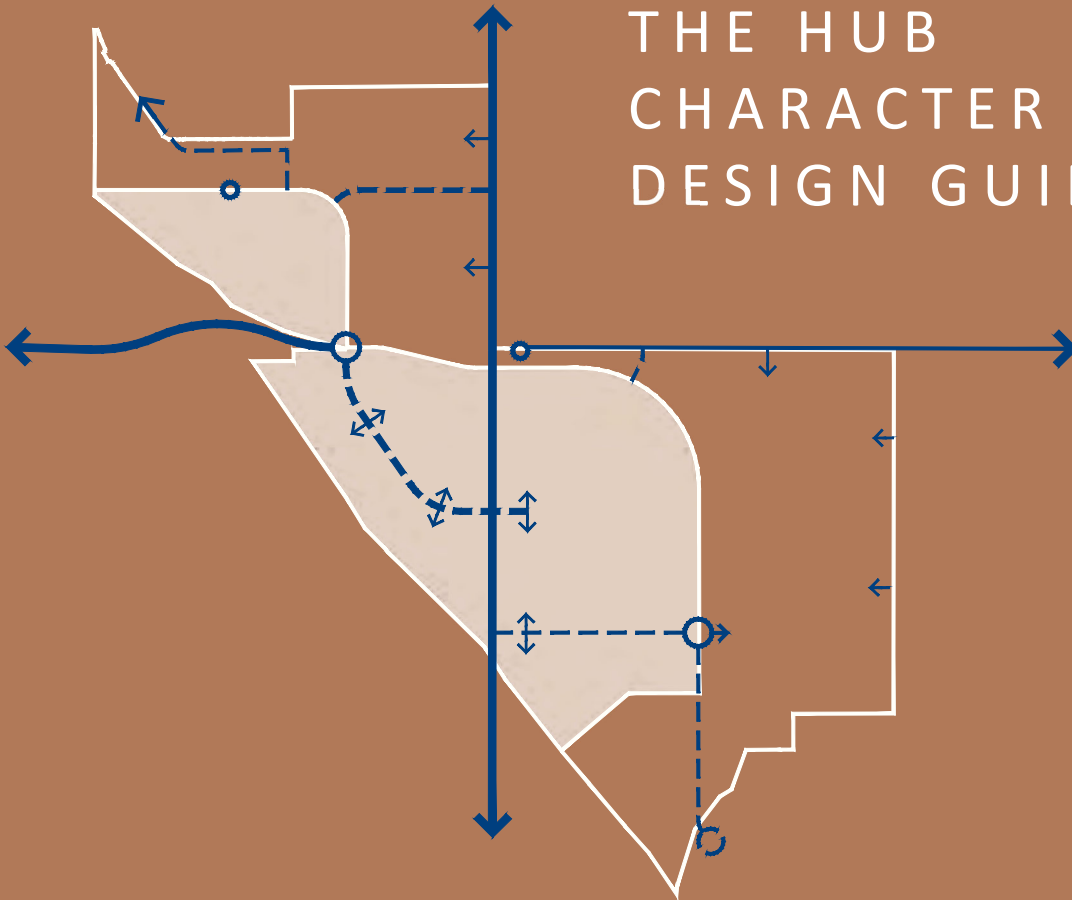
EXHIBIT D6 - Conceptual 50-Foot Landscape Buffer

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# THE HUB CHARACTER AREA DESIGN GUIDELINES





## 1. SITE AND BUILDING DESIGN

### A. Site Planning / Building Placement

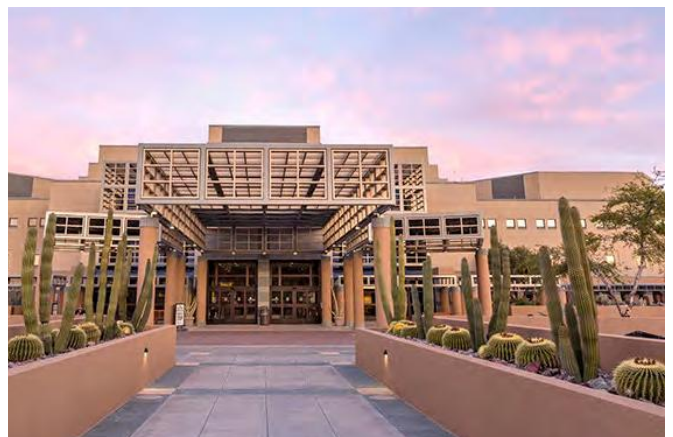
The Hub Character Area is intended for employment use, specifically an EV-focused auto campus and/or medical campus use with additional provisions for other commercial uses as dictated by the approved zoning.

Building placement should be well coordinated with vehicular access and associated parking areas. Buildings should relate to the street in a relatively consistent manner as much as is practical to support a cohesive campus experience.

Design elements shall be incorporated to create a heightened sense of detail at site entrances and building entrances.

Building placement and site layouts shall promote visibility and efficient access to employment uses while providing a reasonable balance of landscaped areas.

The Hub Character Area Design Guidelines establish a sense of arrival along Estrella Drive streetscape and promote the commercial employment land uses along the Freeway.



## B. Architectural Style and Character

Design Guidelines for The Hub Character Area focus on providing direction for a clean and modern, potentially branded architectural style and parameters to ensure landscaped and open areas are provided between larger buildings. Product visibility is a fundamental goal for site design with provisions made for low walls, landscaping, and berms to create a comfortable site to streetscape transition.

Buildings shall be designed with clean lines, high-quality materials, and simple color palettes; for non-vehicle sale uses, branding colors should only serve as accents and should not dominate the building design. For vehicle sale uses, branding and identity components may be integrated in the building design in a high-quality manner.

Taller buildings should be primarily oriented towards the Freeway.

Larger buildings with longer facades should be designed with projected and recessed, horizontal and vertical elements to break up building masses.







Conceptual architectural imagery for employment land uses other than EV-focused auto campus uses



### C. Miscellaneous Exterior Building Materials and Design Imagery

The Hub Character Area shall reflect a pattern of higher intensity developments to accentuate the intended architectural design cues from traditional urban, employment environments, (i.e. compact vertical form, higher FAR's, etc.) Appropriate building scale, height, and massing, along with high quality detailing, articulation, and materials that will create a distinguishable identity and identifiable design and will become a positive addition to the public realm.

Below is a list of miscellaneous building materials, design elements and features that are permitted for within The Hub Character Area:

#### Building Skin:

- Architectural concrete, either cast-in-place or precast, natural or integral-color finish.
- Adobe, CMU masonry or brick, rammed earth.
- Cut natural stone or faux stone.
- Traditional and Synthetic stucco (EIFS), integrally colored, smooth or textured finish.
- Metal panels or siding.
- Wood. Engineered or natural (as appropriate for the desert climate).
- Tinted and/or reflective glazing.

#### Building Roofing:

- Concrete or clay tile, flat or arched, earth-tone color range.
- Architectural asphalt shingle.
- Natural or patina-finish copper, seamed.
- Batten-seamed metal roofing systems.
- Tubular metal or wood frame trellis.





**Accent materials:**

- Cut natural stone or faux stone.
- Formed concrete, natural or integral-color finish.
- Adobe or rammed earth.
- Natural, brushed or burnished metals, where glare and reflection are not objectionable.
- Stainless steel ornament, where glare or reflectivity are not objectionable.

**Window and Door Framing:**

- Anodized or painted aluminum
- Painted or stained wood
- Integrally colored PVC
- Painted steel

**Materials prohibited for exterior architectural finishes:**

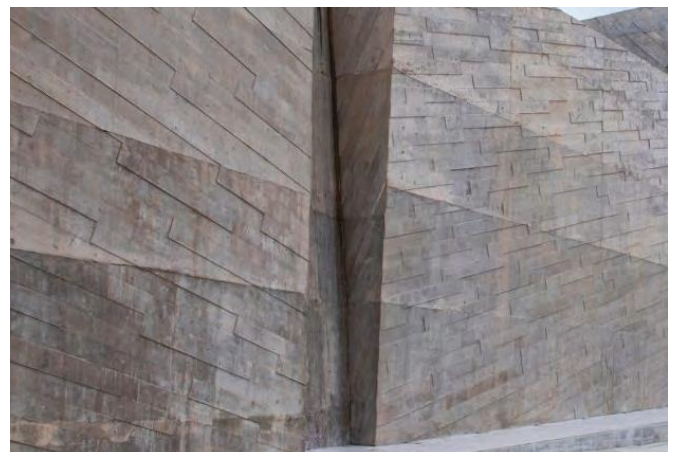
- Exposed non-anodized aluminum.
  - Asbestos or mineral-fiber panels or facing materials.
  - Mirrored finishes
  - Vinyl façade materials

**Awnings**

- Awnings, natural or synthetic fiber, material designed to resist sun exposure, non-reflective, non-translucent, colors to be uniform, integrated with the color palette of the site or building.

**Screening of Mechanical Equipment:**

- Roof mounted mechanical equipment on flat roof structures shall be screened by a parapet or architectural screening. The parapet is to be as high as the highest component of mechanical equipment on the structure. Equipment which is specified within the construction documents may not be changed at the construction site if the equipment exceeds the specified height of the parapet.



- The parapet shall be designed and constructed with forms, materials and colors which are compatible with the structure below.
- Roof mounted mechanical equipment on pitched roofs must be contained within the attic space and not visible from ground level.
- All ground-mounted mechanical equipment shall be screened with site walls exceeding the equipment's height by at least two feet, and with ornamental planting.
- All refuse collection, storage, compaction and processing containers and facilities shall be screened from all vantage points with a site wall and opaque (solid) gates exceeding the height of the facility by two feet, and with ornamental planting.

## D. Surface Parking

### Surface Parking for Employment Uses

Where parking is adjacent to a street, a 36-inch high wall and/or landscaping should be used to screen the parking area and enhance the comfort of the sidewalk environment.

Service areas should be located to not disturb the reasonable enjoyment of on-site or off-site uses and should be screened from view by landscaping, screening walls or buildings

Any area used for parking, display, storage or circulation shall be dust free.

### Surface Parking within an EV-Focused Auto Campus

Where a parcel is developed for vehicle sales, 100% of the parcel frontage (beyond areas used for required landscaping, access, etc.) may be occupied with the vehicle display areas.

Display areas are not required to be screened with a 36-inch screen wall or landscaping. Display areas shall require a minimum of a 10-inch high decorative wall or landscaping between the display area and the right-of-way line.

Vehicle display racks are permitted within display areas where they do not obstruct on-site circulation. Vehicle display racks are moveable podiums often crafted of metal and may be up to 10-feet in height.

Vehicle display pedestals are permitted within display areas where they do not obstruct on-site circulation. Vehicle display pedestals are fixed in-place raised platforms with a facade comprised of stone, stucco, concrete, or other similar high-quality material.

Vehicle display pedestals may encroach into the setback to be no closer than 5-feet from the public right-of-way line.



## 2. CIRCULATION

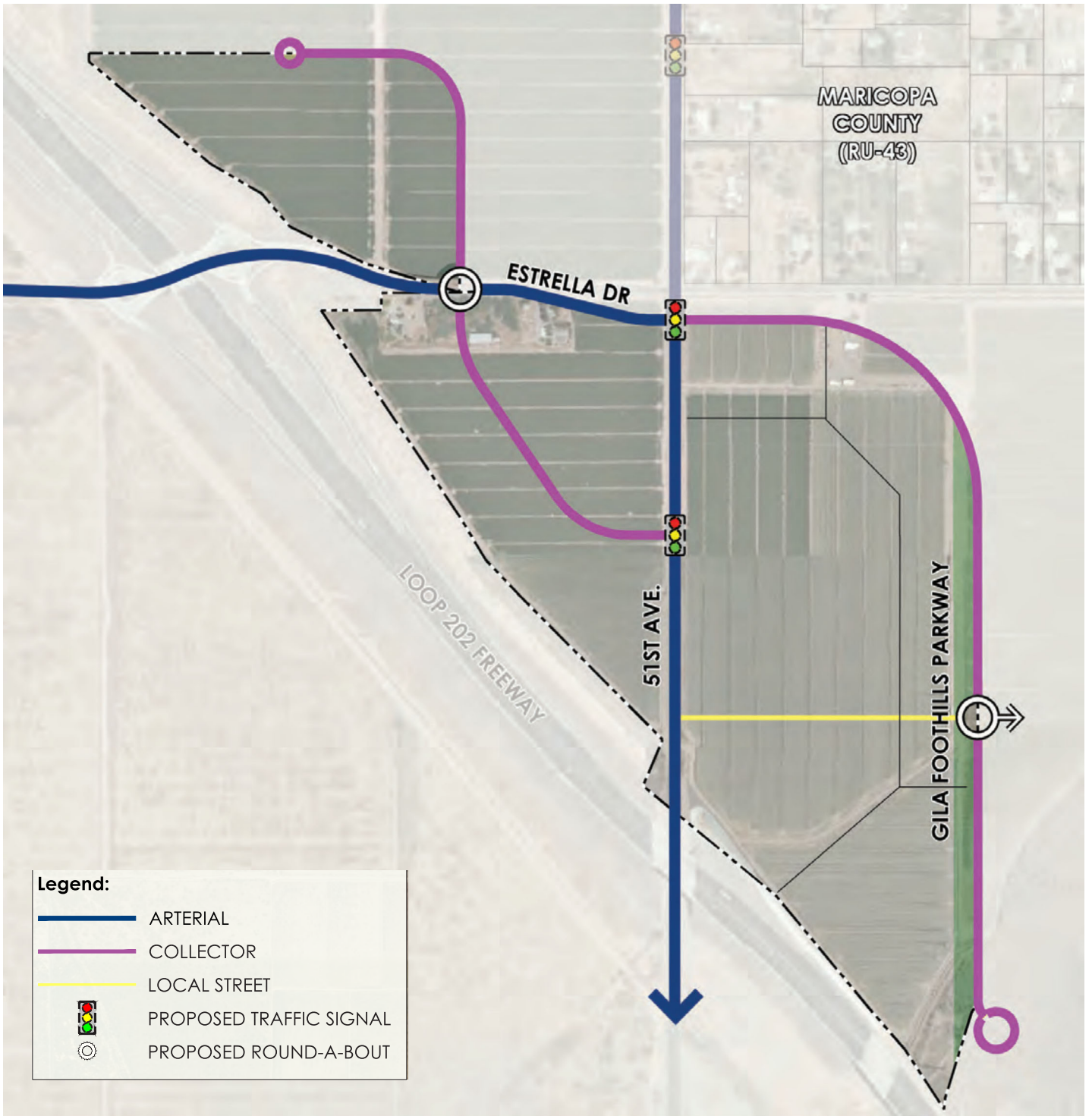
### A. Primary Vehicular Entries and Circulation

As shown in Exhibit H1, The Hub Character Area Circulation Plan, the Hub Character Area spans across Estrella Drive and 51st Avenue. The roundabout on Estrella Drive will serve as a gateway and will help signal to drivers that they are arriving to a distinct high-quality EV-focused auto campus in Laveen. Enhanced landscaping, wayfinding, monumentation, signage and hardscape improvements are provided within the roundabout and along Estrella Drive extending to the traffic signal at 51st Avenue to create a sense of arrival to a high-quality EV-focused auto campus and commercial area with Laveen.

51st Avenue is the primary north-south vehicular corridor through The Hub Character Area that will provide access to developments that are part of an EV-focused auto campus. This roadway will need to safely and easily accommodate commercial traffic within The Hub Character Area. Design accommodations to facilitate efficient access and clear visibility to adjacent vehicle sales land uses are discussed later in this section.

To the east of 51st Avenue along Gila Foothills Parkway, only secondary access to EV-focused auto campus developments and the Hub Character Area is planned. The appearance of the east side of the traffic signal at 51st Avenue and Estrella Drive/Gila Foothills Parkway will indicate the boundary of the EV-focused auto campus and should encourage the majority of commercial drivers to turn south on 51st Avenue to continue in the EV-focused auto campus instead of continuing east.

The most southern traffic signal on 51st Avenue will visually and functionally connect the The Hub Character Area and EV-focused auto campus where it extends to the east of 51st Avenue.



**EXHIBIT H1 - The Hub Character Area Circulation Plan**

This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.

N.T.S.



## B. Pedestrian and Bicycle Circulation

As shown in Exhibit H2 The Hub Character Area Conceptual Trail Plan, six-foot-wide concrete sidewalks traverse the majority of the Hub Character Area. Pedestrian amenities along the sidewalk are limited in comparison to the the Foothills Character Area and District Character Area because the EV-focused auto campus envisioned within The Hub Character Area will primarily attract drivers more than pedestrians.

On the eastern boundary of The Hub Character Area where Development Units 7 and 4b front on Gila Foothills Parkways, an 8-foot-wide multi-use trail will curve along a 6-foot wide sidewalk. Access to EV-focused auto campus land uses is secondary or limited from Gila Foothills Parkway.

It is expected that most vehicles will access Development Units 4A and 4B from the local road proposed to extend from 51st Avenue. This will relieve Gila Foothills Parkway to take on a more pedestrian and bicycle friendly atmosphere. While Gila Foothills Parkway borders both The Hub Character Area and The Foothills Character Area it is primarily described earlier in this document in the Foothills Character Area Design Guidelines.



*A detached sidewalk is the primary pedestrian improvement within the EV-focused auto campus within The Hub Character Area.*

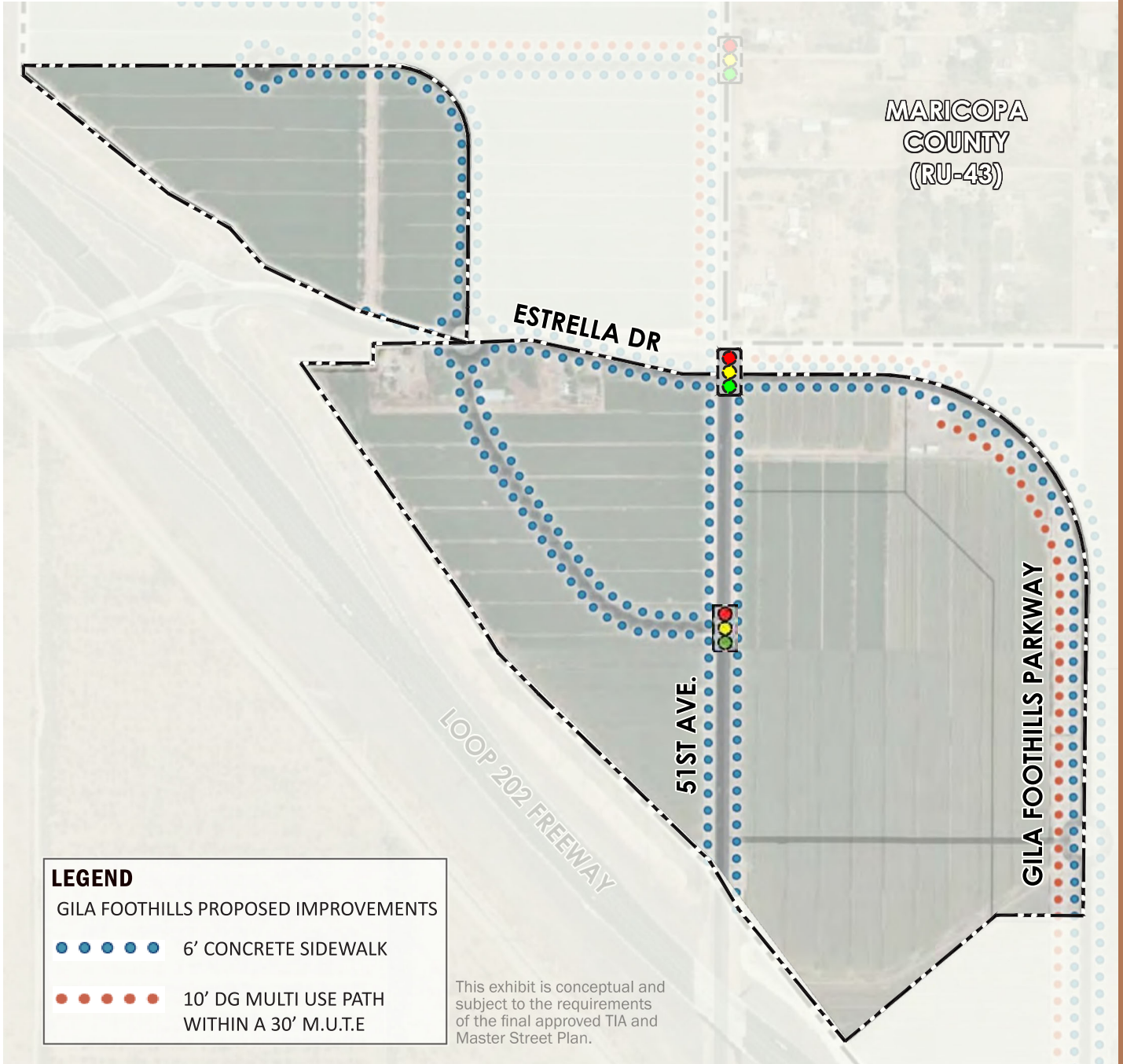


EXHIBIT H2 - The Hub Character Area Conceptual Trail Plan

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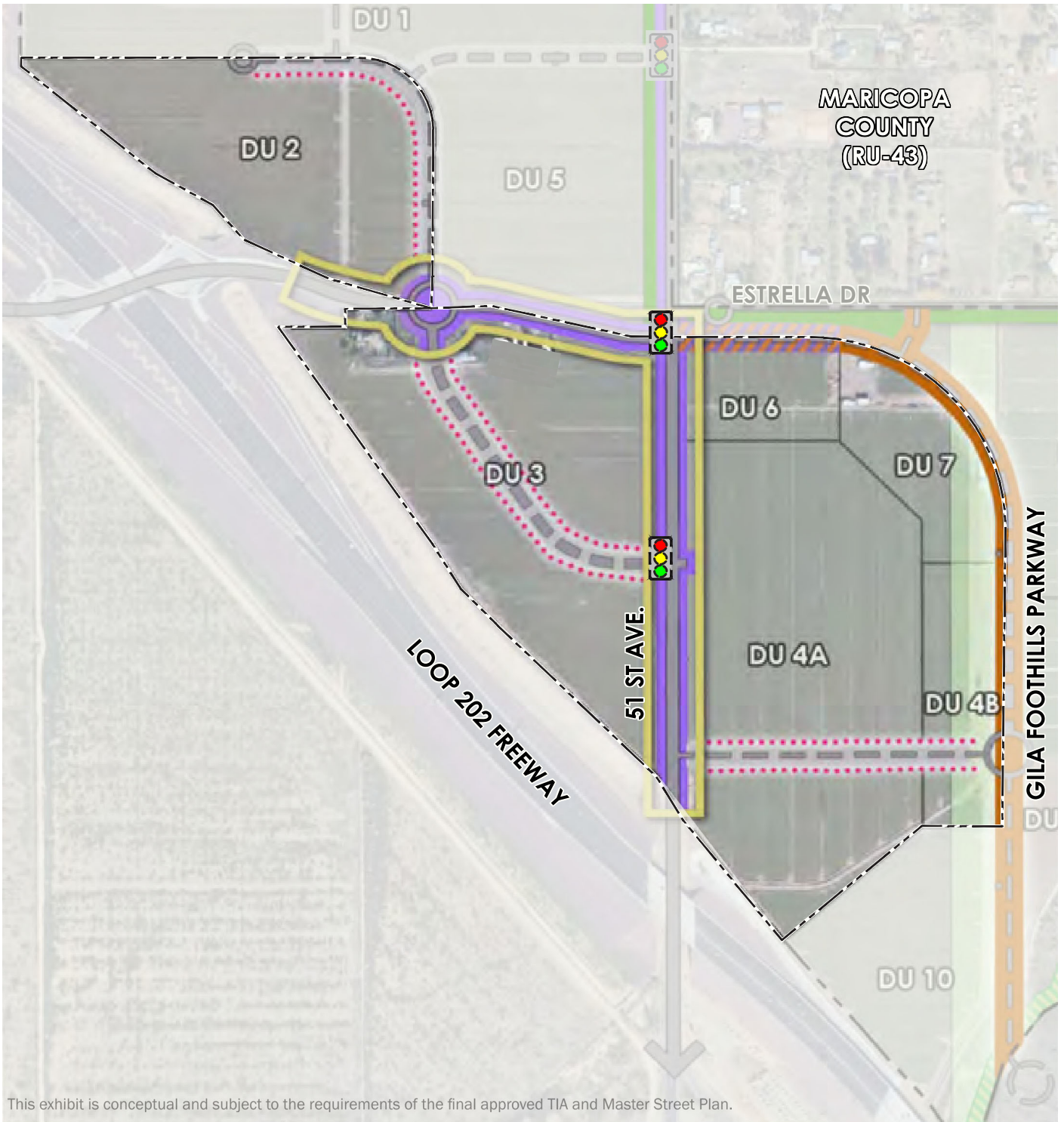
## C. Street Design and Landscape Character

Within the Hub Character Area, a variety of street designs are proposed, each with a different function. To guide the appearance of each of these street designs, The Hub Character Area is organized into many Landscape Character Zones.


Landscape Character Zones provide design guidelines on how to plant within the right-of-way as well as within the landscape setbacks of adjoining properties. These guidelines work with the Street Sections, Plant Palette, Development Standards and Landscape Standards to ultimately ensure the appearances of each street frontage.

The Gateway Zone, Formal Streetscape Zone, and Shade Pocket Eligible Zone are all located within the Hub Character Area. As shown in the Landscape Character Plan on the next page, Estrella Drive and 51st Avenue are located in the Gateway Zone and Formal Street Scape Zone. The internal streets of the Hub Character Area are located in the Shade Pocket Eligible Zone.







Lastly, where The Hub Character Area fronts on Gila Foothills Parkway, the Transition Zone, Natural Streetscape Zone, and WAPA Easement Zone provide guidance on the landscape appearance. Since Gila Foothills Parkway primarily provides access to The Foothills Character Area and only provides secondary access to The Hub Character Area, it is discussed in The Foothills Character Area design guidelines section.



**EXHIBIT H3 - The Hub Character Area Landscape Character Plan**

N.T.S. 

**LEGEND**

- |   |   |   |
|---|---|---|
|  Gateway Zone             |  Shade Pocket Eligible Zone        |  WAPA Easement Zone |
|  Formal Streetscape Zone  |  Transition from Formal to Natural |   |
|  Natural Streetscape Zone |   |   |

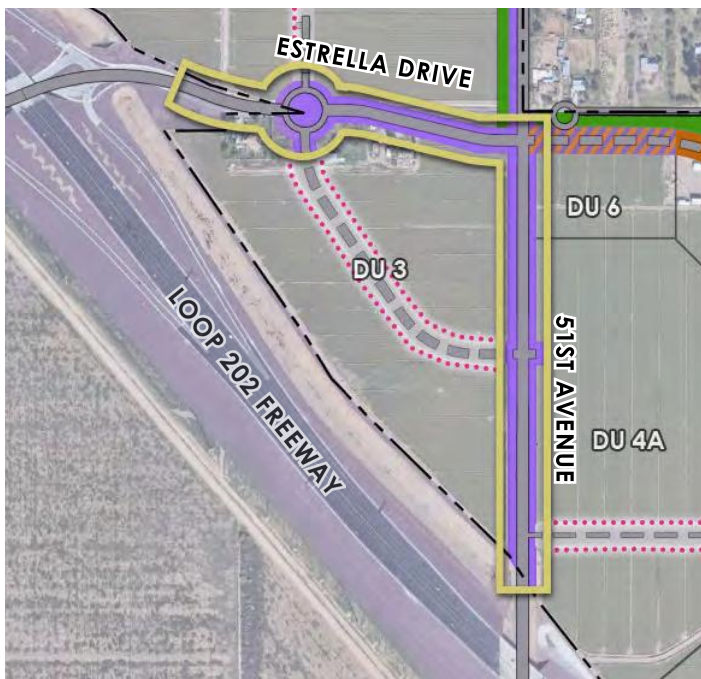


## The Gateway Zone

As shown in yellow on the Landscape Character Plan, the Gila Foothills PUD Site includes a primary project gateway within The Hub Character Area. This Gateway Zone extends from the Freeway along Estrella Drive and 51st Avenue. An enhanced roundabout is located within this zone on Estrella Drive.

With a majority of the vehicular trips accessing the Site from the Freeway, this Gateway Zone will be designed to create an enhanced sense of arrival and will clearly guide commercial and employment traffic to stay within the Site and avoid traveling on local streets.

Thematic elements associated with the Gateway Zone may also be replicated throughout the Hub Character Area at street intersections or property entries to extend a cohesive appearance.



The Gateway Zone and Formal Streetscape Zone call for enhanced and coordinated landscaping within the ROW and landscape setbacks of these portions of Estrella Drive and 51st Avenue.

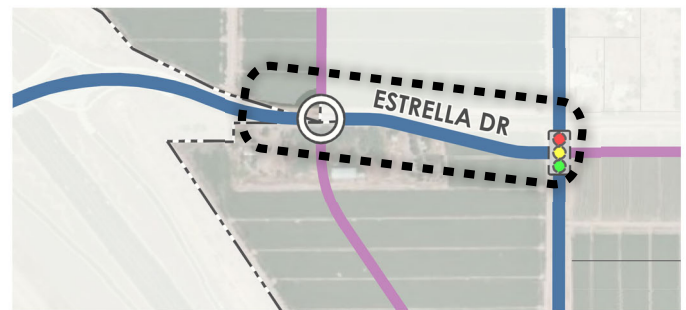
## Estrella Drive

Estrella Drive between the Freeway and 51st Avenue will be enhanced following the guidance of the Gateway Zone to provide a strong sense of arrival to the Gila Foothills Site.

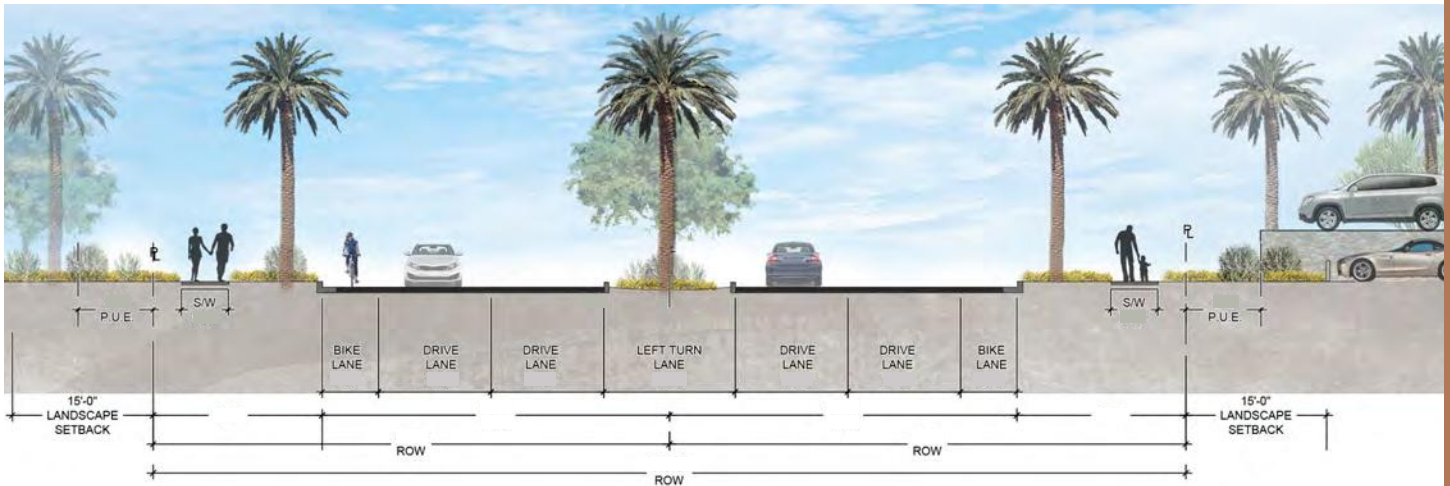
On the following page, the conceptual street section and plan for Estrella Drive shows two vehicle lanes and a bike lane traveling in each direction which will be separated by a central raised median. A detached 6-foot wide sidewalk will also be provided on each side of the roadway.

Following the guidance of the Gateway Zone and Formal Streetscape Zone, as well as the Landscape Standards from Part 1 of this document, thematic plantings are shown within the median and along the sidewalk. Date Palms alternate with Ironwood Trees in the center median and align with palm trees along the sidewalks.

Beyond the ROW, the landscape setback of each Development Unit will follow the respective Character Area Plant Palette but will maintain a coordinated appearance as guided for by the Formal Streetscape Zone.



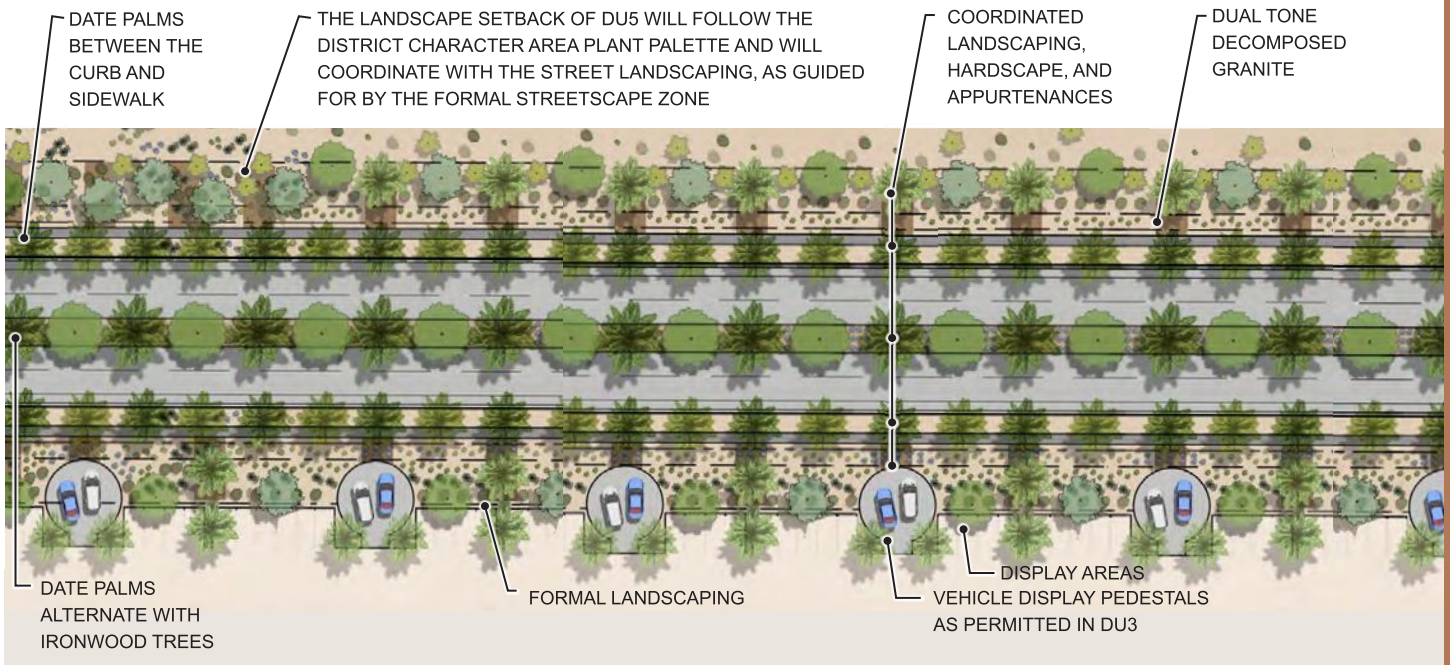
The conceptual street section and plan on the following page are for this portion of Estrella Drive



**EXHIBIT H4 - Conceptual Street Section - Estrella Drive, Gateway Zone (View Looking East)**

N.T.S.

This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.



**EXHIBIT H5 - Conceptual Street Plan - Estrella Drive, Gateway Zone**

N.T.S.

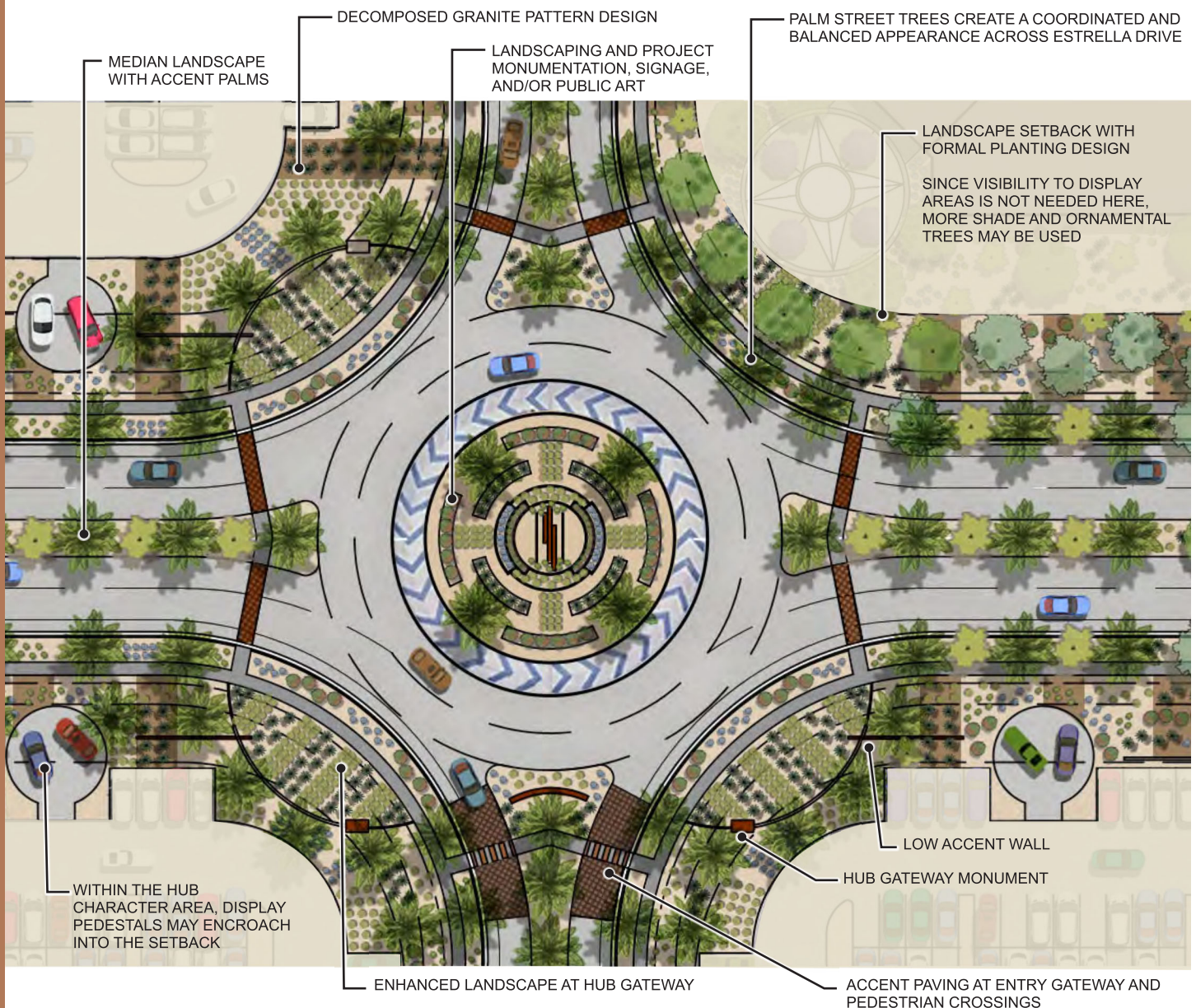


This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.



### Estrella Drive Roundabout

The same enhanced and thematic plantings along Estrella Drive will continue into the roundabout design. As shown in the Conceptual Roundabout Plan below and in the conceptual roundabout imagery on the following page, additional enhancements such as monumentation, signage & wayfinding, regimented thematic plantings, and/or public art are allowed within the roundabout. Roundabout features that are located within the public right-of-way will be constructed to geometric standards approved by the City of Phoenix Streets Transportation Department. These features serve as a traffic calming elements, but are also intended as gateway features for the Project. Monumentation and accessory signage or public art installations shall be permitted with the central round-about and all associated maintenance of such features will be the responsibility of the property ownership association.



**EXHIBIT H6 - Conceptual Roundabout Plan - Estrella Drive, Gateway Zone**

This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.

N.T.S.





Examples of signage, public art, landscaping, and/or monumentation to be located within roundabout



Palms support an apparent sense of arrival.



Palms and ornamental or shade trees in a center median visually fill the streetscape with greenery.

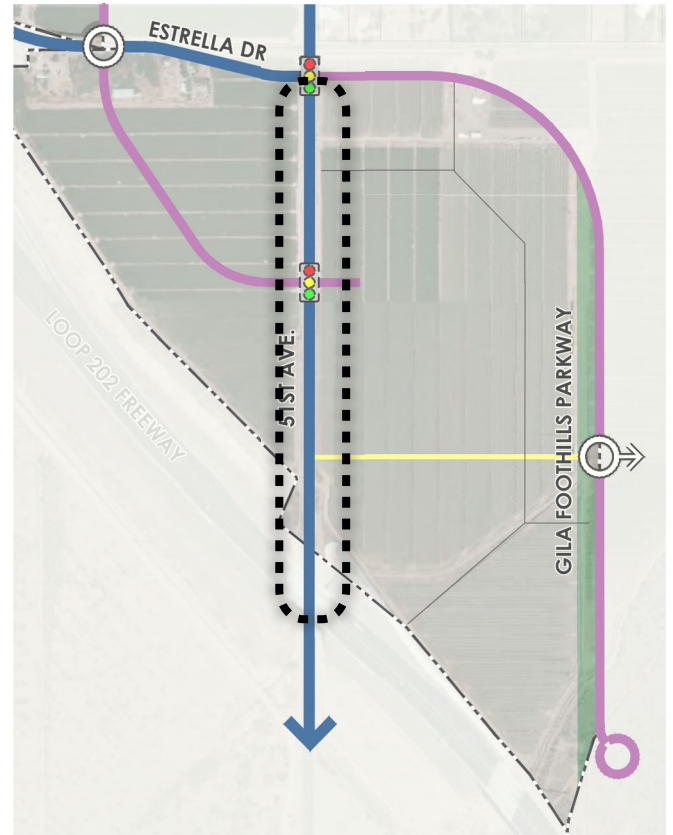


### 51st Avenue, South of Estrella Drive

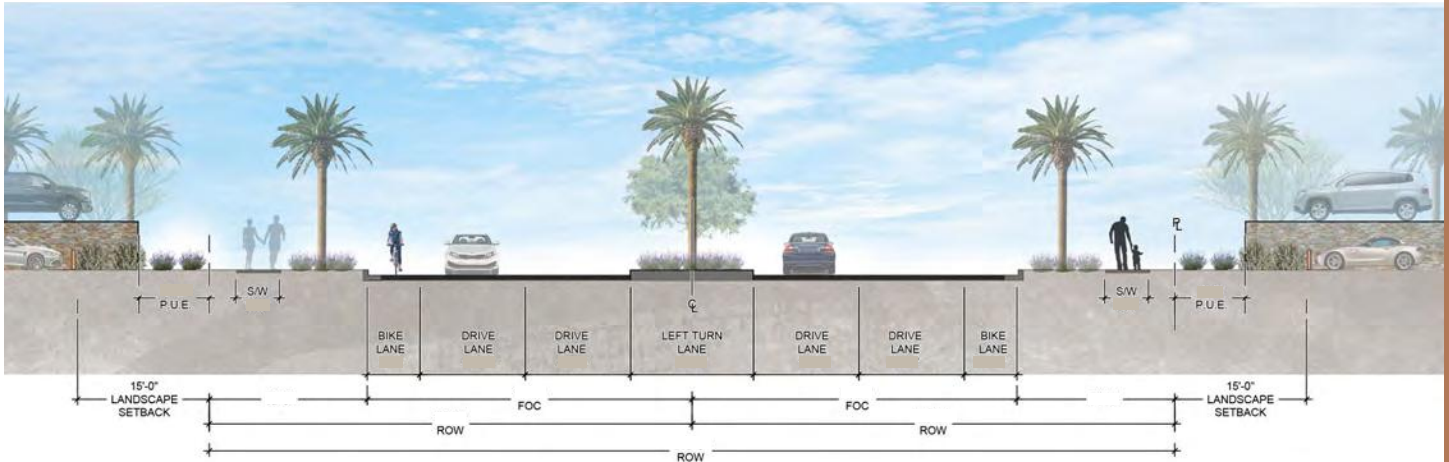
South of Estrella Drive, 51st Avenue will continue the sense of arrival and will primarily serve to provide access for commercial traffic to employment land uses and/or an EV-focused auto campus within the Hub Character Area.

On 51st Avenue, two lanes of traffic and a bike lane will travel in each direction. A center turn lane will provide enhanced access to commercial driveways and to the proposed local roads that will extend to the east and west to access sites within the Hub Character Area.


The Gateway Zone and Formal Streetscape Zone treatment of Estrella Drive will continue on this portion of 51st Avenue. The same enhanced and coordinated street and thematic landscape design will continue in the median, along the sidewalks, and within the landscape setbacks.



The Conceptual Street Section and Plan to the right show conceptual improvements allowed within the portion of 51st Avenue identified above.




**EXHIBIT H7 - Conceptual Street Section - 51st Avenue, Gateway Zone**

N.T.S. 

This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.



**EXHIBIT H8 - Conceptual Street Plan - 51st Avenue, Gateway Zone**

N.T.S. 

This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.

### Streetscape Elements:

- Formal planting design
- Enhanced entryways with pavers or stamped asphalt to accent in a complementing and contrasting tone
- Palm trees shown within the landscape setback with coordinated groundcover treatment or alternating tones of decomposed granite
- Screen wall at parking areas
- Vehicle display pedestals permitted to encroach within the landscape setback
- Monumentation may be at primary site entry
- Decorative paving at entry area and pedestrian crossings
- Planting to reflect character of building and building tenant
- Drought tolerant and low water use planting
- Low maintenance landscape



## Shade Pocket Eligible Zone

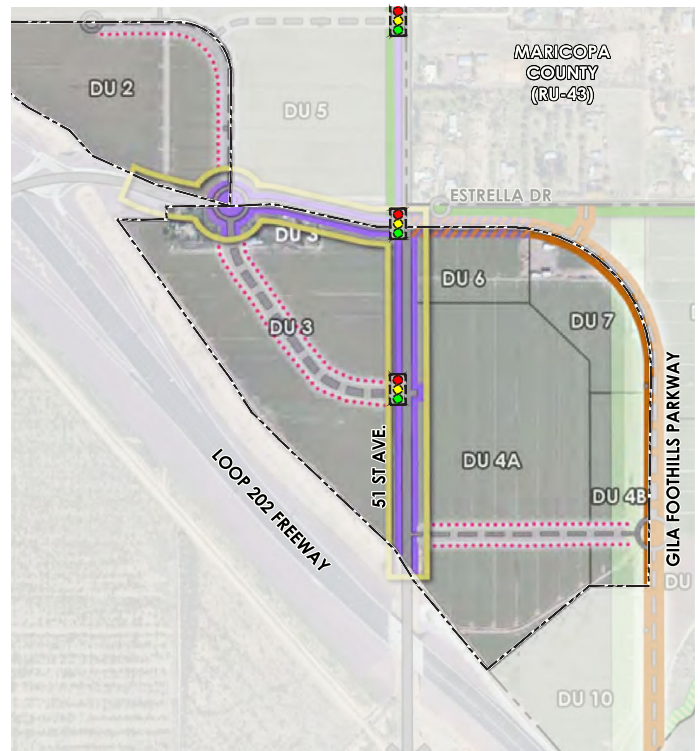
The internal streets of the Hub Character area are small collector streets that provide access to employment land uses and are located in the Shade Pocket Eligible Zone. These collector streets are secondary in comparison to the Estrella Drive and 51st Avenue arterials and thus are not recognized as part of the Formal Streetscape Zone.

These streets may provide one to two lanes of traffic and a bike lane in each direction. A detached 6-foot wide sidewalk will also be on each side of the street.

Where a median is feasible, a lightly themed landscaped median is proposed on the internal streets to visually connect to the Gateway Zone improvements and the surrounding campus of employment or EV-focused auto uses.

Per the Landscape Standards found in Part 1 of this document, palms and Chinese Pistache trees will alternate within the median. A variety of palms are permitted and the tree spacing is five-feet greater than the tree spacing on Estrella Drive and 51st Avenue. This will create a similar but secondary appearance on the internal streets and provide a spine of design continuity.

As further explained in the following design guidelines, outside of the street curb line, the appearance of the internal streets and adjoining landscape setbacks will vary based on the land use. The greatest variation is the ability of sites developed for vehicle sales to implement Shade Pockets.



The internal streets of the Hub Character area connect to arterials to provide vehicular access to employment land uses. Along these streets, in lieu of meeting the sidewalk shading standard with shade trees, Shade Pockets may be used.

 Shade Pocket Eligible Zone



*Phoenix dactylifera*  
DATE PALM



*Pistacia chinensis*  
CHINESE PISTACHE

Where a median is feasible, Date Palms and Chinese Pistache trees will alternate to create a coordinated appearance within The Hub Character Area.

### Shade Pocket Eligible Zone - Right-of-Way Landscape Guidelines for Employment Uses

Where land uses other than vehicle sales are developed within the Hub Character Area, the following street tree and landscaping standards apply:

- Street trees, including those located between the curb and sidewalk, should consist of those shown on the Plant Palette as permitted within The Hub Character Area.
- Street trees shall be limited to those specified in Table 1309.1 of the City of Phoenix Zoning Code.
- Street trees shall be spaced at twenty-five (25) feet on center average with five (5) shrubs per tree.
- A mix of tree sizes shall be used within The Hub Character Area.
- Sidewalks located within the public right-of-way will be shaded a minimum of 50 percent.

### Shade Pocket Eligible Zone - Right-of-Way Landscape Guidelines within an EV-Focused Auto Campus

Where an EV-focused auto campus is developed within the Hub Character Area, the following street tree and landscaping standards apply:

- Where called for within The Hub Character Area street trees should consist of those shown on the Plant Palette as permitted within the Hub Character Area, including a variety of palms.
- Sidewalks must be shaded a minimum of 25 percent. Shade produced by palm trees may be counted towards this percentage.
- As shown on the Landscape Character Plan, the internal streets are located within the Shade Pocket Eligible Zone. In this zone sidewalks located within the public right-of-way may be shaded by a Shade Pocket as an alternative to meeting the minimum sidewalk shading criteria. Further details on Shade Pockets is provided in the following section.



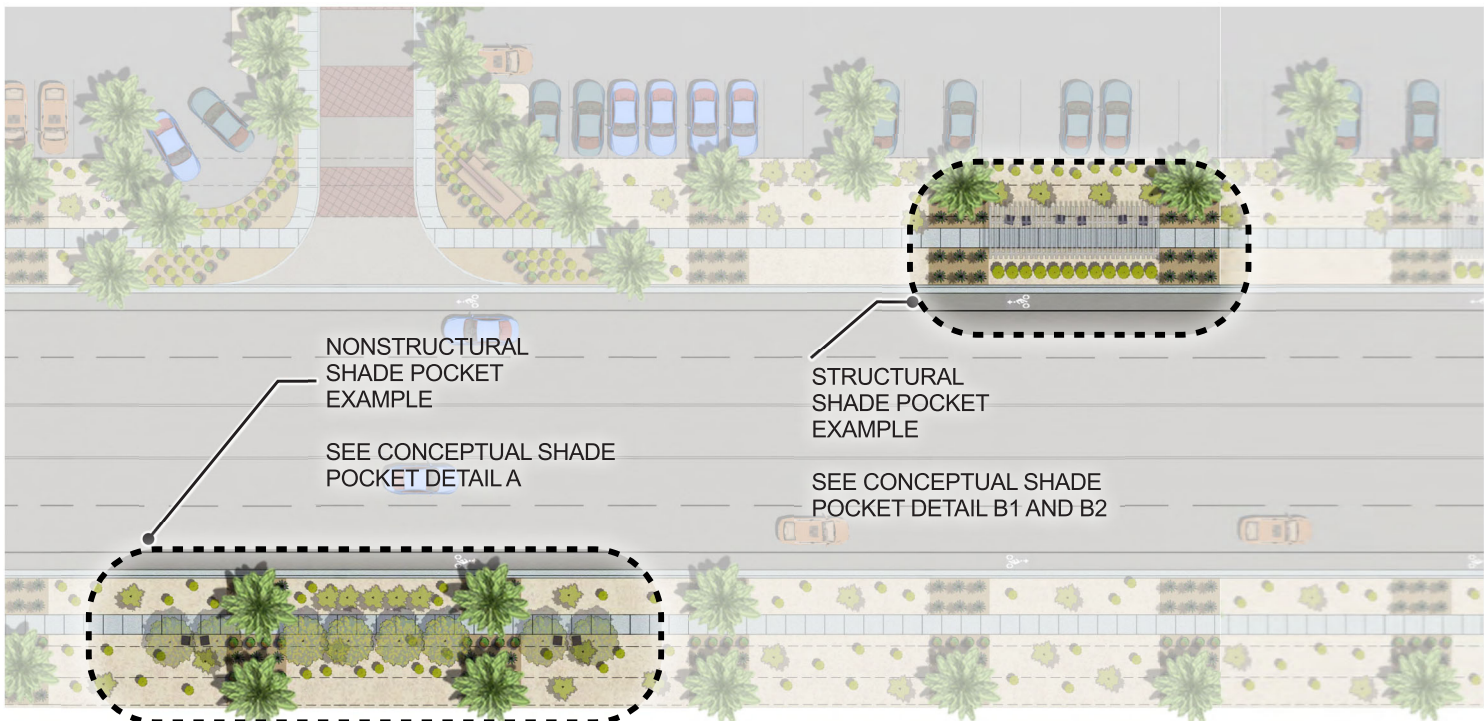
## Shade Pockets

Shade Pockets are an alternative to the sidewalk shading requirements of the City of Phoenix that carefully balance the need for sidewalk shade as well as the need for visibility into display areas.

Shade Pockets are located only within an EV-focused auto campus where visibility into on-site display areas from the roadway is important. Shade Pockets are areas where shade is provided by means of a structural improvement or cluster of trees. Developers may choose to provide a shade structure in order to provide greater visibility to the site than what could be achieved if a grouping of trees was used.


Shade Pockets are only allowed within the Hub Character Area and shall be subject to the following standards:

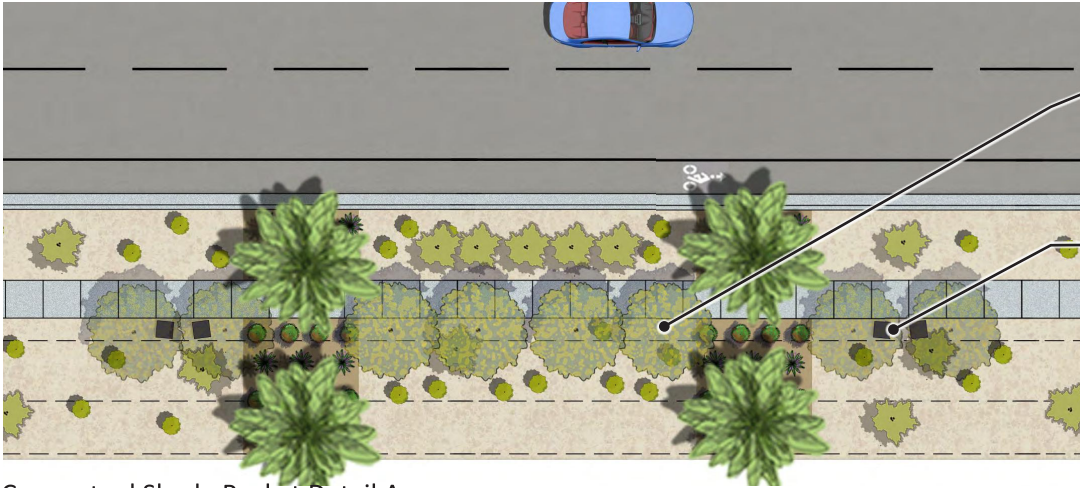
- Each shade pocket must provide 300 square feet of shade over and adjacent to the public sidewalk. Pedestrians must be able to reasonably access the minimum required shaded area.
- One Shade Pocket is required per site plan. Where a site plan has frontage on two streets located within the Hub Character Area, one Shade Pocket per street frontage is required.
- Shade Pockets must coordinate with the surrounding streetscape and site improvements to not appear out of place or create an unbalance street appearance.
- A bench or similar seating element must be provided
- Developers maintain the option to follow the typical street landscaping and shading requirements so long as the appearance of the overall streetscape remains balanced and well-coordinated.



### EXHIBIT H9 - Shade Pocket Concepts

This exhibit is conceptual and subject to the requirements of the final approved TIA and Master Street Plan.

N.T.S. 



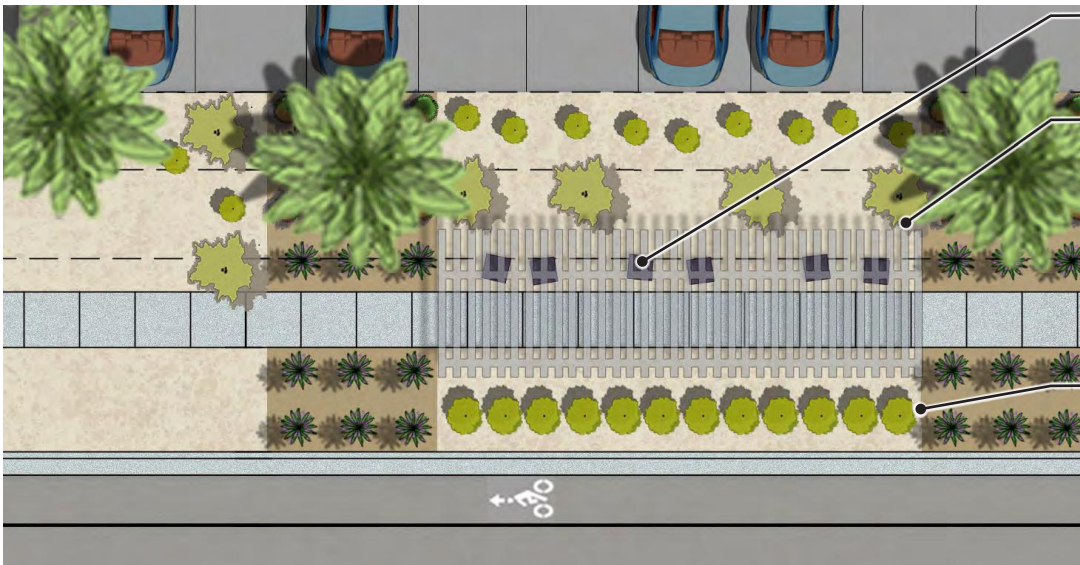
Conceptual Shade Pocket Detail A

DEVELOPERS MAY USE TREES TO CREATE A SHADE POCKET.

IN THIS EXAMPLE, TREES PROVIDE 75% SHADE COVERAGE OF 400 SQ. FT. OF SIDEWALK RESULTING IN A 300 SQ. FT. SHADE POCKET

SEATING IS PROVIDED ADJACENT TO THE SIDEWALK

AS SHOWN IN EXHIBIT H9, THIS NON-STRUCTURAL SHADE POCKET IS COORDINATED WITH THE SURROUNDING LANDSCAPING AND DRIVEWAY ACROSS THE STREET TO RESULT IN A BALANCED OVERALL STREETScape DESIGN



Conceptual Shade Pocket Detail B1

SEATING OPTIONS PROVIDED IN SHADED DG AREA ADJACENT TO SIDEWALK

TRELLIS SHADE POCKET CONCEPT

SHADE POCKET IMPROVEMENTS ARE COORDINATED WITH SURROUNDING LANDSCAPING



Conceptual Shade Pocket Detail B2

SIDEWALK WIDENED TO CREATE SPACE FOR SEATING UNDER SHADE STRUCTURE

POSTS POSITIONED TO CANTILEVER OVER SIDEWALK

SOLID SHADE POCKET CANOPY DESIGN

SHADE POCKET IMPROVEMENTS ARE COORDINATED WITH SURROUNDING LANDSCAPING



### Shade Pocket Eligible Zone - Setback Landscape Guidelines for Employment Uses

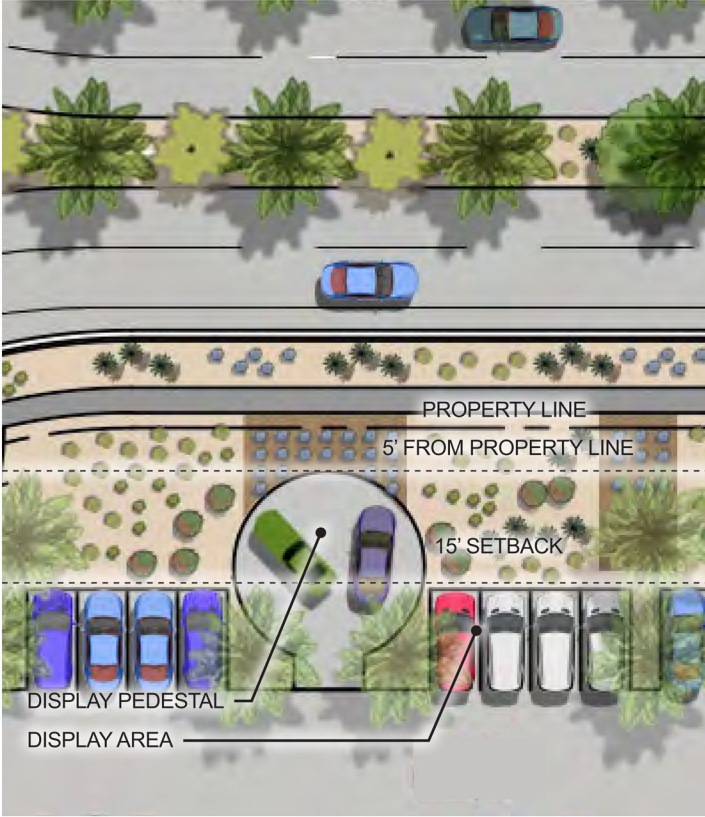
For land uses other than vehicle sales, the following landscape setback guidelines apply:

- An average of a 15-foot landscape setback will extend from any property line adjacent to a public roadway.
- Plants within the landscape will be from The Hub Character Area Plant Palette
- The landscape setback will incorporate an appropriate mix of shade trees and ornamental trees
- Trees shall be spaced at twenty (20) feet on center average with five (5) shrubs per tree
- A mix of tree sizes shall be used within The Hub Character Area; Two (2) inch caliper trees fifty percent (50%); three (3) inch caliper trees fifty percent (50%)
- Landscape setback dimensions will vary based on adjacent land use conditions and site features.

### Shade Pocket Eligible Zone - Setback Landscape Guidelines within an EV-Focused Auto Campus

For Vehicles Sales, within the landscape setback, the following guidelines apply:

- Where display areas are located adjacent to the landscape setback, the landscape setback shall incorporate a mix of plants and trees and that enhances the visibility to the display areas from the roadway. Bismark Palms, Date Palms, Saguaro Cacti, and other similar plants and trees that do not have a substantial canopy size are appropriate in the landscape setback
- Trees shall be spaced at sixty (60) feet on center average with five (5) shrubs per tree. Trees may be grouped to accommodate permitted encroachments within the landscape setback.
- A mix of tree sizes shall be used within the Hub Character Area; Two (2) inch caliper trees fifty percent (50%); three (3) inch caliper trees fifty percent (50%)
- Landscape setback dimensions will vary based on adjacent land use conditions and site features.
- Vehicle display pedestals are permitted within the landscape setback areas for a maximum of 20% of the area. Said vehicle display pedestals are permitted up to a maximum height of three (3) feet above finish grade and must have a high quality and permanent appearance. The face of vehicle display pedestals must be comprised of materials supported by the design guidelines that coordinate with site walls and landscaping. No branding colors should be used in the design of the vehicle display pedestals.



As depicted above, Display Pedestals may come within 5 feet from the property line.



Conceptual imagery of Display Peestals



## LANDSCAPING

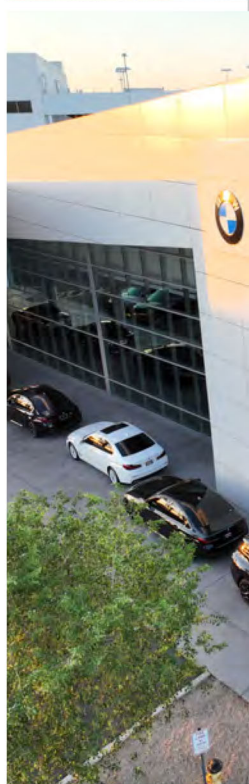
- Linear Rows
- Repetition of Elements
- Formal Design
- Geometric and intentional appearance
- Palm-lined entrances
- Linear and mass planting
- Clean and organized appearance
- Laveen modern

### 3. Landscape Design Guidelines

As outlined in this section, landscape guidelines across the Hub Character Area vary based on land use, functionality, and treatment.

The primary goals of these guidelines are to contribute to a sense of arrival, accent site entry ways, provide pedestrian shading alternatives, promote visibility of vehicle display areas from public roadways, enhance the appearance of employment land uses, and provide appropriate screening.

While these landscape guidelines apply to the Hub Character Area, it is the intent that landscaping will support an overall unified character that extends and appropriately transitions throughout The Gila Foothills Site. To help meet this goal, Exhibit DG3 Landscape Character Plan proposes landscape “zones” across all three Character Areas. These zones coordinate with the following landscape guidelines and the Gila Foothills Plant Palette (Appendix Attachment 1) to identify the best plants and landscape application for areas within each Character Area to help guide the landscape theming across the Site.



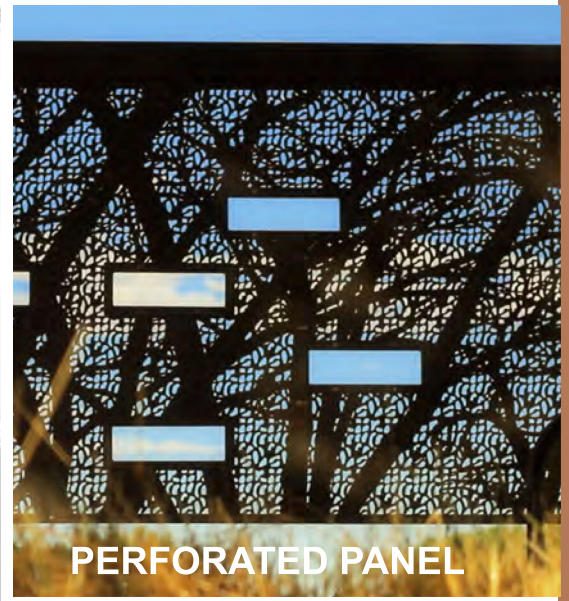




**LINEAR PATTERN**



**DATE PALMS**



**PERFORATED PANEL**



**MODERN**



## A. Plant Palette

The use of native, well adapted, low water use and ADWR approved plants will enhance the overall theme and character of the development. The following Plant List provides a landscape palette for the Hub Character Area.

Latin Name	Common Name
<b>TREES</b>	
<i>Acacia aneura</i>	Mulga
<i>Caesalpinia cacalaco</i> 'smoothie'	Thornless Cascalote
<i>Chilopsis linearis</i>	Desert Willow
<i>Fraxinus velutina</i> "Fan tex"	Fan Tex Ash
<i>Olea europeaea</i> 'Swan Hill'	Olive Tree, Swan Hill (Fruitless)
<i>Parkinsonia x 'Desert Museum'</i>	Desert Museum Palo Verde
<i>Pistachia chinensis</i>	Chinese pistache
<i>Prosopis chilensis</i>	Chilean Mesquite
<i>Quercus virginiana</i>	Live oak
<i>Ulmus parvifolia</i>	Evergreen Elm
<i>Vitex agnus-castus</i>	Chaste tree
<i>Parkinsonia praecox</i>	Palo Brea
<i>Vachellia farnesiana</i>	Sweet acacia

Latin Name	Common Name
<b>PALMS</b>	
<i>Bismarckia nobilis</i>	Bismark Palm
<i>Phoenix dactylifera</i>	Date Palm
<i>Washingtonia filifera</i>	California Fan Palm
<i>Washingtonia robusta</i>	Mexican Fan Palm

Latin Name	Common Name
<b>SHRUBS</b>	
<i>Aristida purpurea</i>	Purple Three-awn
<i>Bougainvillea</i> spp.	Bougainvillea
<i>Bouteloua curtipendula</i>	Sideoats Grama
<i>Caesalpinia gilliesii</i>	Desert Bird of Paradise
<i>Calliandra californica</i>	Baja Fairy Duster
<i>Calliandra eriophylla</i>	Pink Fairy Duster
<i>Cassia artemisoides</i>	Feathery Cassia
<i>Dodonaea viscosa</i>	Hop Bush
<i>Eremophila hygrophana</i> 'Blue Bells'	Blue Bells Emu Bush
<i>Eremophila maculata</i> 'Valentine'	Valentine Bush
<i>Eremophila racemosa</i>	Easter Egg Emu Bush
<i>Fouquieria splendens</i>	Ocotillo
<i>Justica californica</i>	Chuparosa
<i>Justica spicigera</i>	Mexican Honeysuckle
<i>Lantana camera</i>	Bush Lantana
<i>Leucophyllum candidum</i>	Cenizo
<i>Leucophyllum frutescens</i>	Texas Sage
<i>Leucophyllum langmanniae</i>	Rio Bravo Sage
<i>Leucophyllum zygophyllum</i>	Blue Ranger
<i>Muhlenbergia</i> spp.	Muhly Grass
<i>Nasella tenuissa</i>	Mexican Feather Grass
<i>Ruellia brittoniana</i>	Mexican Petunia
<i>Ruellia peninsularis</i>	Baja Ruellia
<i>Salvia coccinea</i>	Cherry Sage
<i>Salvia greggii</i>	Autumn Sage
<i>Salvia leucantha</i>	Mexican Bush Sage
<i>Simmondsia Chinensis</i>	Jojoba
<i>Tecoma stans v. angustata</i>	Arizona Yellow Bells
<i>Vauquelina californica</i>	Arizona Rosewood

Latin Name	Common Name
<b>GROUNDCOVERS</b>	
<i>Acacia redolens</i>	Trailing acacia
<i>Convolvulus cneorum</i>	Bush Morning Glory
<i>Dalea greggii</i>	Trailing Indigo Bush
<i>Eremophila glabra</i> 'Mingenew Gold'	Outback Sunrise Emu Bush
<i>Lantana montevidensis</i>	Trailing Lantana
<i>Rosmarinus officinalis</i> 'Prostratus'	Trailing Rosemary
<i>Ruellia brittoniana</i> 'Katie'	Katie Ruellia
<i>Sphagneticola trilobata</i>	Yellow Dot
<b>VINES</b>	
<i>Antigonon leptopus</i>	Vine
<i>Macfadyena unguis-cati</i>	Cat's Claw Vine
<i>Mascagnia lilacaena</i>	Lilac Orchid Vine
<i>Merremia aurea</i>	Yellow Morning Glory Vine
<b>ACCENTS</b>	
<i>Agave desmettiana</i>	Smooth Agave
<i>Agave geminiflora</i>	Twin-flowered Agave
<i>Agave parryi</i>	Perry's Agave
<i>Agave vilmoriniana</i>	Octopus Agave
<i>Agave weberi</i>	Weber's Agave
<i>Aloe barbadensis</i>	Medicinal Aloe
<i>Aloe x 'Blue Elf'</i>	Blue Elf Aloe
<i>Asclepias subulata</i>	Desert Milkweed
<i>Dasylirion wheeleri</i>	Desert Spoon / Sotol
<i>Fouquieria splendens</i>	Ocotillo
<i>Hesperaloe funifera</i>	Giant Hesperaloe
<i>Hesperaloe parviflora</i>	Red Yucca / Hesperaloe
<i>Muhlenbergia rigens</i>	Deer Grass
<i>Pachycereus marginatus</i>	Mexican Fencepost
<i>Pedilanthus macrocarpus</i>	Lady Slipper





*Acacia aneura*  
MULGA



*Phoenix dactylifera*  
DATE PALM



*Quercus virginiana*  
LIVE OAK



*Ulmus parvifolia*  
Chinese Elm



*Bouteloua gracilis* 'Blond Ambition'  
BLUE GRAMA GRASS



*Eremophila glabra* Winter Blaze  
WINTER BLAZE EMU BUSH



*Eremophila hygrophana* Blue Bells  
BLUE BELLS EMU BUSH



*Hesperaloe parviflora*  
RED YUCCA



*Ruellia brittoniana*  
RUELLIA



*Agave desmetiana*  
SMOOTH AGAVE



*Lantana* 'Dallas Red'  
RED LANTANA



*Lantana x 'New Gold'*  
NEW GOLD LANTANA



*Caesalpinia cacalaco*  
CASCALOTE



*Aloe x Blue Elf*  
BLUE ELF ALOE



*Tecoma stans*  
YELLOW BELLS



*Acacia redolens* 'Desert Carpet'  
TRAILING ACACIA



## B. General Landscape Requirements

The following items shall be incorporated into all spaces of The Hub Character Area that are developed as landscape areas:

- All landscape areas are to be treated with an appropriate top dressing material for dust control and aesthetic treatment. The use of desert cobble is encouraged to be used at all landscape areas adjacent to roadways, project perimeter and open space areas. Cobble is to be salvaged from Site and may be supplemented with similar color and size. Areas within parking lots and adjacent to public spaces near buildings may use decomposed granite top dressing, one-half (½) inch minus, two (2) inch minimum thickness.
- Turf is limited to thirty percent (30%) of the landscape area or ten percent (10%) of the total Site area, whichever is less.
- A minimum of fifty percent (50%) of all landscape areas shall be covered with trees, shrubs or groundcover.
- Berms shall not exceed a ratio of four (4): one (1) side slopes.
- Basins shall not exceed a ratio of three (3): one (1) side slopes.
- All landscape areas are to have an automatic irrigation system. Trees, shrubs and groundcover shall be watered by a drip irrigation system and turf areas shall incorporate high efficiency spray heads.

## C. Open Spaces

A variety of both public and private open spaces will be provided in The Hub Character Area. These areas will be amenitized or landscaped to provide passive or active use by residents and members of the public.

### Open Spaces Associated with Employment, Commercial and Institutional Uses

- Provide connectivity between buildings on the same property, centrally located
- May be internally located within the Site, or may be oriented towards project entries/driveways for enhanced visibility
- Passively amenitized
- Seating
- Shading – through trees or shade structures either attached or detached from adjacent buildings
- Pedestrian scale

### Open Spaces Associated with EV-Focused Auto Campus Uses

As typically indicative of vehicle sales related land uses, functional open spaces will be more limited than alternative uses found within the Hub Character Area. Within the Hub Character Area, where an EV-focused auto campus is developed, open spaces will serve mostly as visible open space as compared to useable open space. Open Spaces will predominantly be located along project frontages and project perimeter as part of the required street landscaping and landscaped setbacks to present an attractive and unified visual appearance of the Character Area as viewed from adjacent public roadways.

## D. Common Area and Retention Basin Landscape

Improved open space and retention basin landscape areas shall be incorporated into the Site development of the project. These common areas provide opportunities for storm water storage while also adding clustered open space which increases the potential for amenities and activity spaces. It is the intent of this PUD to encourage the development of these spaces to include usable areas, tree lined walkways and seating areas, and interesting arrangements of shrub and groundcover plantings in dense plant massing. In providing these elements, the common areas will promote a pleasant pedestrian environment, function as an interconnected open-space system for on-site users as well as adjacent land uses and provide visual connections between spaces and uses within the development. The landscape in these areas shall be designed so that the plant material has opportunities to use the storm water runoff to supplement the irrigation system and provide groundwater recharge. Limited use of turf may be used in the bottom of basins where appropriate, otherwise fractured granite rip rap or river rock should be placed in the low water areas.

The following standards shall apply:

- Common areas will be owned, managed and maintained by a common association formed to address all common areas and facilities within this PUD.
- Landscaping within common area and retention basin landscape areas shall incorporate an appropriate mix of shade trees.
- A mix of tree sizes shall be used in this area; one (1) inch caliper trees forty percent (40%); two (2) inch caliper trees sixty percent (60%) in accordance with the Arizona Nurseryman's Association Standards
- Trees and shrubs shall be included at minimum of one (1) tree and five shrubs per five hundred (500) square feet of landscape area.
- Turf may be used in select locations within common areas and retention basins where it can be justified as a usable space.
- Turf shall only be permitted where designated for passive or active recreational areas and shall be limited to a maximum of 15% of the overall landscape area within each Site plan.



## E Parking Lot Landscaping

As outlined below, it shall be required for all parcels developed within The Hub Character Area to include parking lot landscape treatments for all surface parking lots. Display areas located within parking lots will not be required to meeting parking lot landscaping standards.

Landscape islands shall be dispersed evenly throughout the parking areas to provide sufficient shade cover and be sized accordingly. Pedestrians should have quick access to tree-shaded walkways located throughout the parking areas. These shaded areas increase the user's comfort and provide additional green space that enhances the Site's aesthetic character.

The following Parking Lot Landscaping standards shall apply:

- Parking islands shall be a minimum of one hundred twenty (120) square feet for single row parking and two hundred forty (240) square feet for double row parking.
- Ten percent (10%) of the total surface parking lot area shall be landscaped exclusive of landscaped setbacks.
- Trees used in parking islands shall be a minimum of two (2) inch caliper in accordance with the Arizona Nurseryman's Association Standards
- Trees and shrubs shall be included at one (1) tree and five (5) shrubs (five (5) gal) per parking island row.
- Turf shall not be used in parking islands



Within permitted display areas, the following standards shall apply:

- The previous Parking Lot Landscaping standards do not apply to permitted display areas.
- Parking islands shall be a minimum of one hundred twenty (120) square feet for single row parking and two hundred forty (240) square feet for double row parking.
- Five percent (5%) of the total display area of the parking lot area shall be landscaped exclusive of landscaped setbacks.
- Trees selected for parking islands may be of the same species and selected to provide a landscaped appearance and support visibility to the display area from the roadway. An accent trees and plants such as palms and tall cacti may be considered.
- Trees used in parking islands shall be a minimum of two (2) inch caliper.
- Trees and shrubs shall be included at one (1) tree and five (5) shrubs (five (5) gal) per parking island row.
- Turf shall not be used in parking islands.





## F. Site Walls, Screens, and Fences

Due to the non-residential uses that are envisioned for the Hub Character Area, property walls may be minimized to enhance visibility and Site accessibility.

View fencing or solid walls shall be a maximum of eight feet high. For segments of view fencing, a solid base of no more than two feet above finished grade shall be allowed and the upper six-foot portion will be open to allow visibility, such as with wrought iron.

Where extended segments of view fence or solid walls are provided, design features should be used to break up the appearance of a long straight wall or fence. Such design features may include planters, pilasters or dense landscaping adjacent to the wall or fence.

Embellishments of rock, stone veneer, stone work or faux stone, or appropriate alternative materials shall be incorporated into the design of perimeter walls or fences.

Wall or fence colors shall be muted and blend with, rather than contrast strongly with, themes common to the Laveen area.

Walls with the Hub Character Area may differ from site to site but should appear to be relatively coordinated and related in their appearance.

Reflective materials should not be used.

If the use of retaining walls is required, they shall be a maximum of six-foot high and shall comply with the regulations applicable to retaining walls as described in the Phoenix Zoning Ordinance.



## EV-Focused Auto Campus Walls & Fence

### Guidelines

- The display area of each dealership shall not be required to provide any screening to vehicles on display for sale. The display area shall be defined as any area of the dealership where vehicles are placed on display for view from the public right-of-way and from on-site.
- Vehicle display pedestals shall be permitted within landscape setback areas along arterial and collector frontages up to a maximum of 20% of the lineal street frontage and shall be permitted up to a maximum of five (5) feet above site grade. A screen wall is not required in front of these display pedestals.
- Lots shall be properly enclosed with a wall, fence, hedge, plantings, or combinations thereof having a minimum height of 10 inches above the grade at which cars are to be displayed unless a hedge, planting or combination thereof are approved in lieu of the 10-inch wall, and/or the lot is depressed below the elevation of adjacent lots in which case appropriate landscaping may be approved.
- Dealerships should provide a minimum eight-foot boundary wall for 'back-of-house' areas to screen on-property storage and service areas. Up to a ten-foot boundary wall may be allowed.



## G. Landscape Lighting for All Land Uses

The Preliminary Landscape Plan and Landscape Construction documents for each phase of development within the Hub Character Area shall promote landscape lighting in accordance with the following:

- Select landscape features, specimen trees and directional signage will be highlighted throughout the project with “up-light” and/or floodlight fixtures.
- Lighting will emphasize the informational and dramatic elements within the landscape (monument signs, groups of trees and shrubs and architectural features) rather than uniform, flat lighting of the entire development.
- Special attention will be given to lighting the project entries and building entries.

## H. Landscape Sustainability

Sustainable landscape and irrigation design practices shall be considered. A minimum of three (3) of the six (6) items described below shall be implemented with the development of each parcel:

- Capture gray water discharge from development facilities and re-use for landscape irrigation.
- Utilize “Smart” irrigation controller
- Utilize efficient drip irrigation technology.
- Locate plant material in or near storm water drainage swales or basins to maximize water benefit for landscape areas.
- Use low water use plant material.
- Provide slope stabilizing plant material where appropriate to limit erosion

## J. Site Lighting

Site lighting will support a vibrant employment-campus and EV-focused auto campus environment within the Hub Character Area and not spillover to the District Character Area and Foothills Character Area. Sources of light will be hidden such that visual glare is avoided and a comfortable ambient light is provided. All light sources shall be shielded to prevent light shining up past a horizontal plane. Landscape lighting will be carefully considered to create a memorable outdoor environment

### Non-Vehicle Sales Exterior Site Lighting

- To promote safety and continuity in design of the fixtures as well as the color and intensity of light, this PUD shall comply with lighting standards as defined within the Environmental Performance Standards and the Guidelines for Design Review Section of the Phoenix Zoning Ordinance except as modified below.
- Lighting shall comply with City of Phoenix Ordinance standards, except as modified below.
- Lighting fixtures located within the landscape setback shall be permitted, but shall encourage the use of thematic decorative materials to wrap the pedestal.

### Vehicle Sales Exterior Site Lighting

- Vehicle sales lots will utilize metal halide luminaires, mounted at a height not to exceed twenty-five (25) feet, except that poles installed within thirty (30) feet of the Character Area Boundary shall not exceed eighteen (18) feet in height.
- Light poles on individual dealer properties will be either black or dark grey in color. Building mounted luminaires shall not exceed fourteen (14) feet in height.
- Separate lighting zones shall be established for perimeter and internal areas with automatic controls to reduce lighting levels after close of business.
- Lighting levels shall be consistent with IESNA (Illuminating Engineering Society of North America) recommended practices for auto dealerships as outlined in publication RP-2, table H1.



# **GILA FOOTHILLS PUD - APPENDIX**

# APPENDIX ITEM A1. COMPARATIVE ZONING STANDARDS

1. Minimum Building Setback, Adjacent to:																	
	CITY OF PHOENIX ZONING STANDARDS				PROPOSED PUD DEVELOPMENT STANDARDS (ORGANIZED BY DEVELOPMENT UNIT)												
	C-2	CP-GCP	R-5	RH	1	5	2	3	4A	4B	6	7	8	9A	9B	10	
Estrella Drive	Average of 25' for structures not exceeding two stories or 30'.	30'	20'	20'	NA	25'	25'	25'	NA	NA	NA	NA	50'	NA	NA	NA	
51st Ave		30'	20'	20'	50'	50'	NA	25'	25'	NA	25'	NA	NA	NA	NA	NA	
55th Ave		30'	20'	20'	25'	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	
47th Avenue		30'	20'	20'	NA	NA	NA	NA	NA	NA	NA	NA	NA	50'	50'	NA	NA
Ed Pastor Loop 202 Freeway		20'	10'	10'	NA	NA	15'	15'	15'	NA	NA	NA	NA	NA	NA	NA	15'
Gila Foothills Parkway	Minimum of 20' for 50% of structure	20'	20'	20'	NA	NA	NA	NA	NA	25'	15'	15'	15'	NA	15'	15'	
Property line not adjacent to a public street	0'	20'	10'	10'	5'	5'	5'	5'	5'	5'	5'	5'	5'	5'	5'	5'	
Property line adjacent to interior public or private street	NA	NA	10'	10'	15'	15'	15'	15'	15'	15'	15'	15'	15'	15'	15'	15'	
Property line shared with Tierra Montana Neighborhood	1 story (or 15'): 10' 2 story (or 30'): 15' 3 story (or 42'): 30' 4 story (or 56'): 45'  An additional one foot setback shall be provided for every one foot of height above 30 feet.	20'	10'	10'	50'	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	

2. Minimum Landscape Setback (LSB) Adjacent to:																
	C-2	CP-GCP	R-5	RH	PROPOSED PUD DEVELOPMENT STANDARDS (ORGANIZED BY DEVELOPMENT UNIT)											
					1	5	2	3	4A	4B	6	7	8	9A	9B	10
Estrella Drive	Average 25' LSB for structures not exceeding two stories or 30'; minimum 20' LSB permitted for up to 50% of the frontage.  Average 30' LSB for structures exceeding two stories or 30'; minimum 20' LSB for up to 50% of the frontage.	30'	20'	20'	NA	15' avg. per site plan, min. 5'	15' avg. per site plan, min. 0'				NA		15' avg. per site plan, min. 5'		NA	
Gila Foothills Parkway					NA	NA	NA		15' avg. per site plan, min. 0'				15' avg. per site plan, min. 5'		NA	15' avg. per site plan, min. 5'
51st Avenue					15' avg. per site plan, min. 5'		NA	15' avg. per site plan, min. 0'	NA	15' avg. per site plan, min. 0'	NA	NA				
55th Avenue					15' avg. per site plan, min. 5'	NA	NA				NA					
47th Avenue					NA		NA				15' avg. per site plan, min. 5'		NA			
Ed Pastor Loop 202 Freeway	10'	20'	10'	10'	NA		10' avg. per site plan, 0' min.				NA		10' avg. per site plan, min. 5'			
Property Line not adjacent to public or private street	10'	10'	10'	10'	0'	0' unless adj. to existing residential, then 10'	0' unless adj. to existing residential, then 10'	0'	0' unless adj. to existing residential, then 10'				0' unless adj. to existing residential, then 10'			
Property line adjacent to interior public or private street	NA	NA	NA	NA	15' avg. per site plan, min. 5'		15' avg. per site plan, min. 0'				15' avg. per site plan, min. 5'					
Property line shared with Tierra Montana Neighborhood	NA	NA	NA	NA	15' avg. per site plan, min. 5'	NA	NA				NA					

Footnote: (1) Vehicle Display Pedestals up to 5-feet in height may encroach into the landscape setback so long as they are no closer than 5-feet to the public ROW. Each vehicle display pedestal shall be no larger than 40" in dimension. Vehicle display pads shall not conflict with sight visibility triangles. Where vehicle display pedestals encroach into the landscape setback, they may not occupy more than 20% of the width of the parcel as measured at the landscape setback.

3. Minimum Landscape Buffers Adjacent to:																
	C-2, CP-GCP, R-5, RH				PROPOSED PUD DEVELOPMENT STANDARDS (ORGANIZED BY DEVELOPMENT UNIT)											
	1	5	2	3	4A	4B	6	7	8	9A	9B	10				
Estrella Drive	NA	NA	NA	NA	NA	NA	NA	NA	NA	50'	NA	NA	NA			
51st Ave	50'	50'	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA			
47th Avenue	NA	NA	NA	NA	NA	NA	NA	NA	NA	50'	NA	50'	NA			
Property line shared with Tierra Montana Neighborhood	50'	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA			

Note: Landscape Setbacks may be located within Landscape Buffers. The difference between the landscape setback and the landscape buffer can be counted towards open space.

4. Maximum Building Height:																
C-2	CP-GCP	R-5	RH	PROPOSED PUD DEVELOPMENT STANDARDS (ORGANIZED BY DEVELOPMENT UNIT)												
				1	5	2	3	4A	4B	6	7	8	9A	9B	10	
56'	18' within 30' of perimeter lot line;	48'	Buildings within one hundred feet of any residential district or perimeter street shall not exceed twenty feet starting at one hundred feet from any abutting residential district or perimeter street, or twenty-five feet from property lines which abut nonresidential districts. The height of structures may be increased one foot for each five of horizontal distance. In no event shall any such building exceed a height of four stories not to exceed forty-eight feet.	40'	56'	56' (1)	56'	56'	56'	56'	40'	30'	40'	40'	56'	
4 stories	1' increase per 3' additional setback; Max. 56' to 80' with use permit and site plan.	4 stories														

Footnote: (1) Hospital uses may be up to 150-feet in height for up to 20% of the area of the Development Unit.



5. Building Stepback Adjacent to:																		
	C-2	CP-GCP	R-5	RH	1	5	2	3	4A	4B	6	7	8	9A	9B	10		
Estrella Dr	Average of 25' for structures not exceeding two stories or 30'.  Average of 30' for structures exceeding two stories or 30'. Minimum of 20' for 50% of structure	18' within 30' of perimeter lot line;  1' increase per 3' additional setback; Max. 56' to 80' with use permit and site plan.	NA	Buildings within one hundred feet of any residential district or perimeter street shall not exceed twenty feet. Starting at one hundred feet from any abutting residential district or perimeter street, or twentyfive feet from property lines which abut nonresidential districts, the height of structures may be increased one foot for each five of horizontal distance. In no event shall any such building exceed a height of four stories not to exceed fortyeight feet	NA	NA	NA	NA	NA	NA	NA	NA	(1)	(1)	NA	NA		
Gila Foothills Parkway					NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA
51st Ave					(6)	(2)	NA	NA	NA	NA	NA	NA	(5)	NA	NA	NA	NA	NA
55th Ave					NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA
47th Ave					NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	(1)	(1)	NA
Loop 202 Freeway	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA		
Property line shared with Tierra Montana Neighborhood	1 story (or 15'); 10' 2 story (or 30'); 15' 3 story (or 42'); 30' 4 story (or 56'); 45'  An additional one foot setback shall be provided for every one foot of height above 30 feet.	NA	NA	NA	(4)	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA		
Property line shared with South Mountain Park	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	(3)	(3)	(3)		

Footnotes:

- (1) 20-foot and one-story maximum building height within 150 feet of the property line. (Buildings are not permitted within the 50-foot landscape buffer)
- (2) 30-foot maximum building height within 100 feet of the property line. 48-foot maximum building height within 150' of the property line. (Buildings are not permitted within the 50-foot landscape buffer)
- (3) 30-foot maximum building height within 50' from the property line.

- (4) 20-foot and one-story maximum building height within 100 feet of the property line. 30-foot and two-story maximum building height within 150 feet of the property line. (Buildings are not permitted within the 50-foot landscape buffer)
- (5) 40-foot maximum building height within 50' from the property line.
- (6) 30-foot maximum building height within 100 feet of the property line.

6. Miscellaneous Development Standards

	C-2	CP-GCP	R-5	RH	1	5	2	3	4A	4B	6	7	8	9A	9B	10
Minimum Open Space	NA	NA	5% of gross area	NA	(3)	(3)	NA	NA	NA	(3)	(3)	(3)	(3)	(3)	(3)	(3)
Maximum Residential Density (1)	NA	NA	45.68; 52.20 with bonus	10 du or guestrooms per each 1/2 acre	40 du/ac per site plan (5)	40 du/ac per site plan (2)(5)	(4b)	NA (4a)	NA (4a)	40 du/ac per site plan (5)	40 du/ac per site plan (5)	40 du/ac per site plan (5)	25 du/ac per site plan (5)	40 du/ac per site plan (5)	40 du/ac per site plan (5)	40 du/ac per site plan (5)
Maximum Number of Units	NA	NA	NA	NA	Overall PUD total max. residential units shall not exceed 1,700 unless a hospital is constructed.		Overall PUD total max. residential units shall not exceed 1,700 unless a hospital is constructed.	NA (4a)	Overall PUD total max. residential units shall not exceed 1,700 unless a hospital is constructed.			Overall PUD total max. residential units shall not exceed 1,700 unless a hospital is constructed.				
Parking Standards	Per Ord. Sec. 702	Per Ord. Sec. 702	Per Ord. Sec. 702	Per Ord. Sec. 702	Parking shall be provided per Ordinance Section 702, unless modified below or elsewhere within this PUD.  Each new auto dealership shall additionally meet the following minimum requirements: <ul style="list-style-type: none"> <li>• Offer for sale and service at least one EV or hybrid automobile.</li> <li>• 1 level 3 charger;</li> <li>• 3 level 2 chargers; and</li> <li>• .5 level 2 service chargers per service bay, excluding oil change or collision bays, not to exceed 10 chargers</li> </ul>											
Bicycle Standards	Per Ord. Sec. 1307 H	Per Ord. Sec. 1307 H	Per Ord. Sec. 1307 H	Per Ord. Sec. 1307 H	<ul style="list-style-type: none"> <li>• Bicycle parking and amenities will be provided per Section 1307-H of the City of Phoenix Walkable Urban Code, Chapter 13 of the City Zoning Ordinance.</li> <li>• Bike parking will comply with ADA requirements and not impede on-site pedestrian access.</li> <li>• A clearance of at least four feet in width will be provided for pedestrian access.</li> <li>• Bicycle racks must be a minimum of 30 inches from a wall or other obstruction.</li> <li>• The minimum length of 72 inches will be provided per bicycle parking space.</li> <li>• With the exception of uses located within an EV-focused auto campus, all nonresidential uses over 5,000 square feet floor area shall provide one bicycle space per 25 vehicle parking spaces, with a maximum of 25 required spaces. Dining and drinking establishments less than 5,000 square feet must provide a minimum of four bicycle spaces in the frontage setback and/or right-of-way if no vehicle parking is provided.</li> <li>• Secure bicycle parking will be provided as part of residential projects that include structured parking.</li> </ul>											

Footnotes:

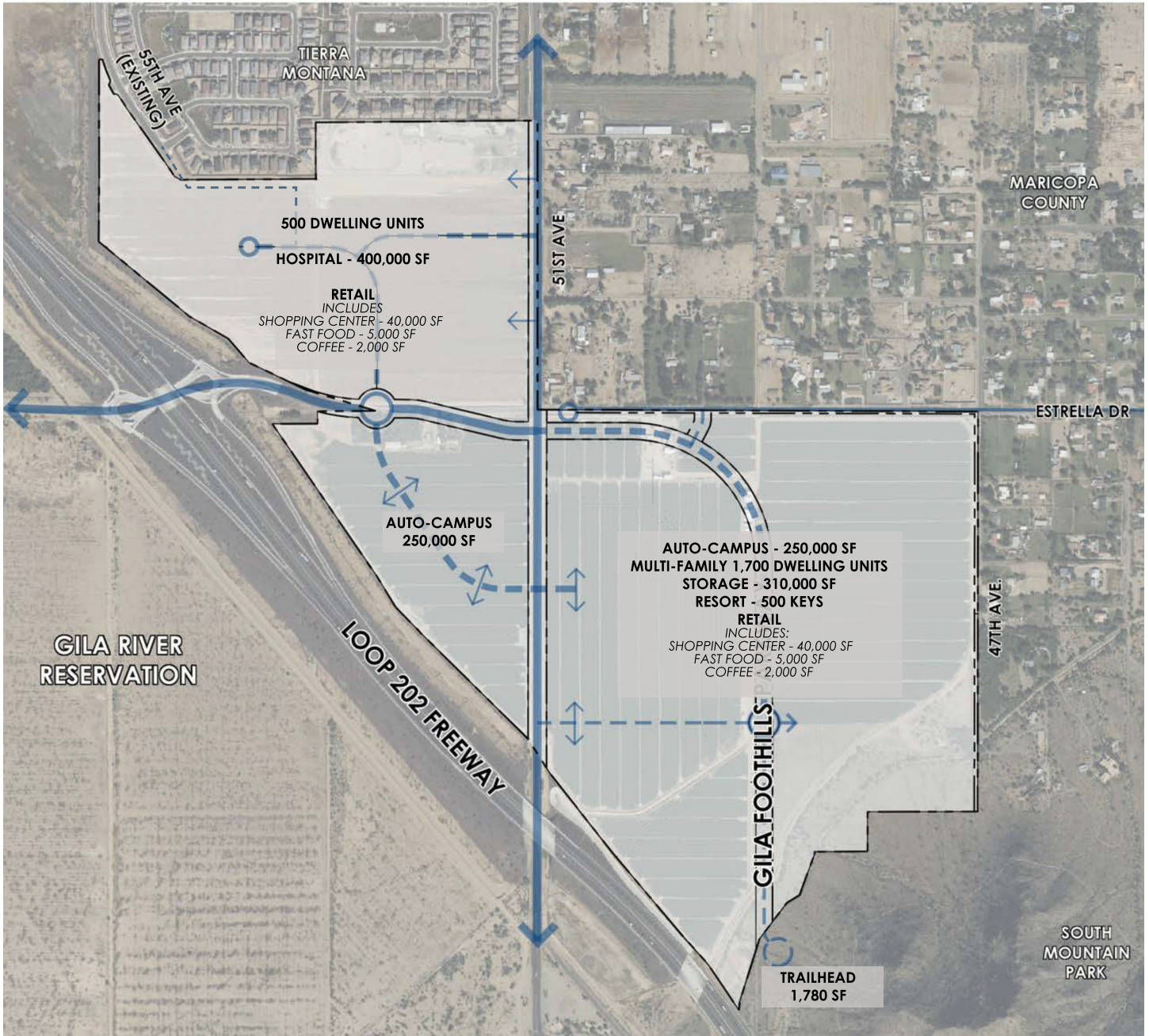
- (1) The entire overall PUD Site may be developed with a maximum of 1,700 residential units. Hotel guest rooms do not count towards the maximum 1,700 dwelling unit for the overall PUD Site. Upon Certificate of Occupancy of a hospital in Development Unit 2 an additional 500 dwelling units above the 1,700 cap are permitted within Development Units 2 and 5.
- (2) Upon Certificate of Occupancy of a Hospital in Development Unit 2, a maximum density of 60 du/ac shall be permitted in this Development Unit.
- (3) As measured per site plan:  
Residential uses greater or equal to 14.0 du/ac: 5% of net area;  
Residential uses less than 14.0 du/ac: 15% of net area;  
Non-Residential uses: 0% of net area.
- (4a) Residential dwelling units are prohibited in this Development Unit
- (4b) Residential dwelling units are prohibited in this Development Unit, unless a hospital is constructed.
- (5) Except as provided for by Footnote 1, no more than 1,700 residential dwelling units are permitted across the Site.

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## APPENDIX ITEM A2. LAND USE ASSUMPTIONS FOR STREETS MASTER PLAN

Development of the Gila Foothills site shall follow these Land Use Assumptions. These Land Use Assumptions may be changed through the Street Master Planning review process so long as changes still otherwise align with the approved PUD and subsequent site plan approvals.



APPENDIX ITEM A2 - LAND USE ASSUMPTIONS FOR STREET MASTER PLAN

N.T.S. ①

The proposed roadway configuration is conceptual and has not been approved by the Street Transportation Department. Final design will be determined as part of the approved Traffic Impact Study and the Master Street Plan. Driveway locations are conceptual and have not been approved by the Street Transportation Department. These locations will be determined through the Master Street Plan.

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## APPENDIX ITEM A3: LEGAL DESCRIPTION

### EXHIBIT "A" LEGAL DESCRIPTION

A PORTION OF SECTIONS 17, 20, AND 21, TOWNSHIP 1 SOUTH, RANGE 2 EAST OF THE GILA AND SALT RIVER MERIDIAN, MARICOPA COUNTY, ARIZONA, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID SECTION 21;

THENCE ALONG THE NORTH LINE OF THE NORTHWEST QUARTER OF SAID SECTION 21, SOUTH 89°49'09" EAST, A DISTANCE OF 2640.71 FEET TO THE NORTH QUARTER CORNER OF SAID SECTION 21;

THENCE ALONG THE EAST LINE OF SAID NORTHWEST QUARTER OF SECTION 21, SOUTH 00°02'24" WEST, A DISTANCE OF 2396.38 FEET;

THENCE LEAVING SAID EAST LINE, SOUTH 89°51'02" WEST, A DISTANCE OF 660.39 FEET;

THENCE SOUTH 00°02'42" WEST, A DISTANCE OF 248.30 FEET TO THE SOUTH LINE OF SAID NORTHWEST QUARTER OF SECTION 21;

THENCE ALONG THE SOUTH LINE OF SAID NORTHWEST QUARTER OF SECTION 21, SOUTH 89°48'59" WEST, A DISTANCE OF 311.23 FEET TO THE NORTHEAST CORNER OF THAT CERTAIN PARCEL OF LAND DESCRIBED AS PARCEL NO. 4 RECORDED IN DOC. 89-0500549, RECORDS OF MARICOPA COUNTY, ARIZONA, SAID POINT ALSO THE NORTHWEST CORNER OF THAT CERTAIN PARCEL OF LAND DEEDED TO THE CITY OF PHOENIX IN DOC. 98-0405987, RECORDS OF MARICOPA COUNTY, ARIZONA;

THENCE ALONG THE EAST LINE OF SAID PARCEL RECORDED IN DOC. 89-0500549, SOUTH 20°23'34" WEST, A DISTANCE OF 277.00 FEET;

THENCE CONTINUING ALONG SAID EAST LINE, SOUTH 59°03'34" WEST, A DISTANCE OF 88.00 FEET;

THENCE CONTINUING ALONG SAID EAST LINE, SOUTH 37°21'34" WEST, A DISTANCE OF 269.00 FEET;

THENCE CONTINUING ALONG SAID EAST LINE, SOUTH 17°40'34" WEST, A DISTANCE OF 396.00 FEET;

THENCE CONTINUING ALONG SAID EAST LINE, SOUTH 05°10'34" WEST, A DISTANCE OF 46.73 FEET TO THE EASTERLY RIGHT OF WAY LINE OF THE SOUTH MOUNTAIN FREEWAY AS SHOWN ON THE RIGHT OF WAY PLANS FOR PROJECT NO. 202L MA 000 H5439 01R;

THENCE ALONG SAID RIGHT OF WAY LINE, THE FOLLOWING COURSES;

NORTH 33°56'57" WEST, A DISTANCE OF 321.59 FEET;

**LEGAL DESCRIPTION PAGE 2/5**

NORTH 41°40'59" WEST, A DISTANCE OF 605.56 FEET;

NORTH 38°11'16" WEST, A DISTANCE OF 893.66 FEET;

NORTH 00°03'35" EAST, A DISTANCE OF 76.81 FEET;

NORTH 45°23'53" WEST, A DISTANCE OF 1120.82 FEET;

NORTH 40°31'59" WEST, A DISTANCE OF 199.24 FEET;

NORTH 34°17'56" WEST, A DISTANCE OF 1303.03 FEET;

NORTH 89°33'45" EAST, A DISTANCE OF 270.70 FEET;

NORTH 00°50'40" EAST, A DISTANCE OF 73.01 FEET TO THE NORTH LINE OF SAID SECTION 20;

THENCE ALONG SAID NORTH LINE, NORTH 89°33'41" EAST, A DISTANCE OF 347.81 FEET;

THENCE LEAVING SAID NORTH LINE, ALONG SAID RIGHT OF WAY LINE, NORTH 73°21'18" WEST, A DISTANCE OF 434.58 FEET;

THENCE NORTH 64°35'07" WEST, A DISTANCE OF 379.26 FEET;

THENCE NORTH 41°02'22" WEST, A DISTANCE OF 190.61 FEET;

THENCE NORTH 60°27'18" WEST, A DISTANCE OF 267.09 FEET;

THENCE NORTH 50°42'33" WEST, A DISTANCE OF 704.52 FEET TO THE WEST LINE OF THE SOUTHEAST QUARTER OF SAID SECTION 17;

THENCE ALONG SAID WEST LINE, NORTH 00°17'50" EAST, A DISTANCE OF 1091.53 FEET;

THENCE LEAVING SAID WEST LINE, SOUTH 89°49'50" EAST, A DISTANCE OF 20.13 FEET TO THE BEGINNING OF A CURVE CONCAVE EASTERLY WHOSE RADIUS BEARS NORTH 72°20'51" EAST, A DISTANCE OF 1030.00 FEET;

THENCE SOUTHERLY ALONG THE ARC OF SAID CURVE, TO THE LEFT, THROUGH A CENTRAL ANGLE OF 6°29'13", AN ARC LENGTH OF 116.62 FEET TO THE BEGINNING OF A CURVE CONCAVE NORTHWESTERLY WHOSE RADIUS BEARS SOUTH 65°52'49" WEST, A DISTANCE OF 30.00 FEET;

THENCE SOUTHERLY ALONG THE ARC OF SAID CURVE, TO THE RIGHT, THROUGH A CENTRAL ANGLE OF 70°02'09", AN ARC LENGTH OF 36.67 FEET;

THENCE SOUTH 26°54'46" EAST, A DISTANCE OF 64.12 FEET TO THE BEGINNING OF A CURVE CONCAVE SOUTHERLY WHOSE RADIUS BEARS SOUTH 11°55'46" EAST, A DISTANCE OF 30.00 FEET;



**LEGAL DESCRIPTION PAGE 3/5**

THENCE EASTERLY ALONG THE ARC OF SAID CURVE, TO THE RIGHT, THROUGH A CENTRAL ANGLE OF 71°13'47", AN ARC LENGTH OF 37.30 FEET TO THE BEGINNING OF A CURVE CONCAVE NORTHEASTERLY WHOSE RADIUS BEARS NORTH 59°16'29" EAST, A DISTANCE OF 1030.00 FEET;

THENCE SOUTHEASTERLY ALONG THE ARC OF SAID CURVE, TO THE LEFT, THROUGH A CENTRAL ANGLE OF 04°30'50", AN ARC LENGTH OF 81.15 FEET;

THENCE SOUTH 35°14'21" EAST, A DISTANCE OF 427.01 FEET TO THE BEGINNING OF A CURVE CONCAVE SOUTHWESTERLY HAVING A RADIUS OF 970.00 FEET;

THENCE SOUTHERLY ALONG THE ARC OF SAID CURVE, TO THE RIGHT, THROUGH A CENTRAL ANGLE OF 04°30'28", AN ARC LENGTH OF 76.32 FEET;

THENCE SOUTH 89°58'12" EAST, A DISTANCE OF 843.88 FEET;

THENCE NORTH 00°12'10" EAST, A DISTANCE OF 336.29 FEET;

THENCE SOUTH 89°58'07" EAST, A DISTANCE OF 1295.27 FEET;

THENCE SOUTH 00°06'34" WEST, A DISTANCE OF 336.26 FEET;

THENCE SOUTH 89°58'12" EAST, A DISTANCE OF 40.00 FEET TO THE EAST LINE OF THE SOUTHEAST QUARTER OF SAID SECTION 17;

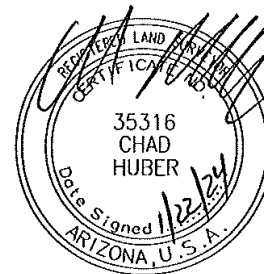
THENCE ALONG SAID EAST LINE, SOUTH 00°06'34" WEST, A DISTANCE OF 1397.13 FEET TO THE POINT OF BEGINNING.

EXCEPT THE EAST 40 FEET OF THE NORTHEAST QUARTER OF THE NORTHEAST QUARTER OF FARM UNIT "B", ACCORDING TO THE FARM UNIT PLAT;

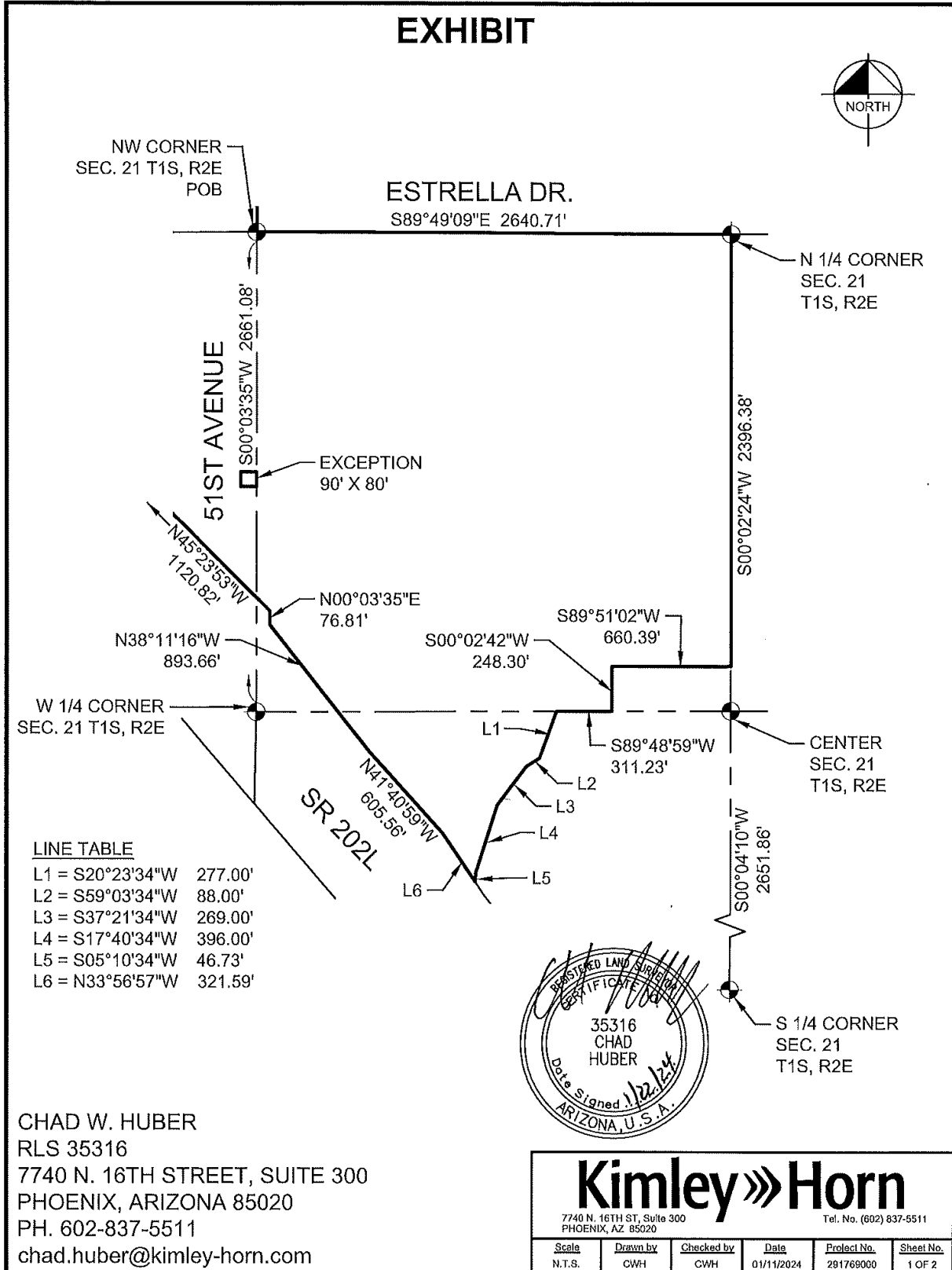
ALSO EXCEPTING THE NORTH 80 FEET OF THE EAST 90 FEET OF GLO LOT 3 OF SAID SECTION 20;

ALSO EXCEPTING THE WEST 15 FEET OF THE EAST 55 FEET OF GLO LOT 3 OF SAID SECTION 20.

ALSO EXCEPTING THEREFROM THE WEST 75.00 FEET OF SAID SECTION 21.



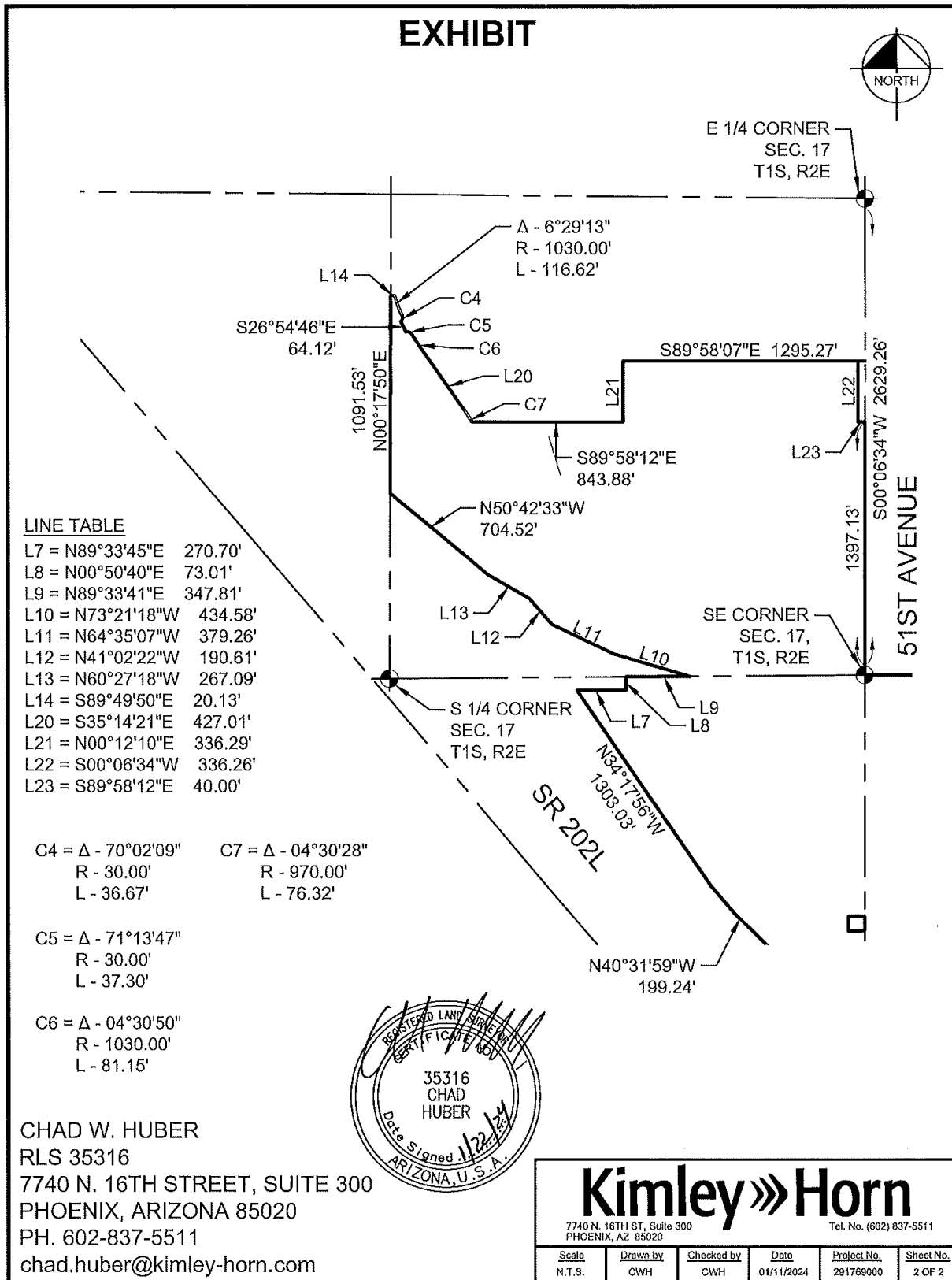
LEGAL DESCRIPTION PAGE 4/5



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LEGAL DESCRIPTION PAGE 5/5



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