

28TH STREET & CAMELBACK MIXED USE

PLANNED UNIT DEVELOPMENT

Land Use and Development Standards

Case: Z-70-15-6

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CITY OF PHOENIX

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**Planning & Development
Department**



A Planned Unit Development (PUD) is intended to be a stand-alone document of zoning regulations for a particular project. Provisions not specifically regulated by the PUD are governed by the zoning ordinance. A PUD may include substantial background information to help illustrate the intent of the development. The purpose and intent statements are not requirements that will be enforced by the city. The PUD only modifies zoning ordinance regulations and does not modify other city codes or requirements. Additional public hearings may be necessary such as, but not limited to, right-of-way abandonments.

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EXECUTIVE SUMMARY

The proposed 28th Street & Camelback Mixed Use Planned Unit Development (“PUD”) at the southeast corner of 28th Street and Camelback Road (the “Property”) will establish the regulatory framework necessary to facilitate this proposed mixed-use and transitional redevelopment project within the Camelback East Village abutting the *Camelback East Primary Core Specific Plan’s* (the “*Specific Plan*”) Core Gradient. The 28th Street & Camelback Mixed Use PUD will complement, protect, and enhance upon the existing and future development occurring within this area and the Camelback East Village, as the need for Class ‘A’ employment and hospitality uses are sought after in the area. The city of Phoenix has invested significant infrastructure resources in this area towards encouraging and providing a sustaining, stable, and long-term employment market for this area. The proposed development will capitalize on and broaden economic development opportunities in this area by acting as a conduit for new businesses and a catalyst for additional sustainable and appropriate development/redevelopment in whatever form that may take (i.e. residential, retail, office, retail, etc.). The aerial map (See **Figure 1** below & **Exhibit M-1**; Context Aerial Map) and context plan/photos (See **Exhibit M-2**; Context Plan and Photos) provide a visual overview of the Property.

Figure 1



The 28th Street & Camelback Mixed Use PUD represents the evolution of this Property from a low-scale suburban style office development, which is no longer economically or physically sustainable, to a highly sought-after, Class ‘A’ employment, and tourist/business destination within the Phoenix metropolitan area. (See **Figure 2** below, Maricopa County Historical Aerial Photograph from December, 1979)

Figure 2



This PUD document is designed to create the framework to allow for flexibility while protecting the adjacent residential areas and, at the same time, promoting sustainable/thoughtful redevelopment. Thus, the 28th Street & Camelback Mixed Use PUD provides for consistent uses and development standards that will fulfill both the needs of the immediate community and the city of Phoenix.

A. PURPOSE AND INTENT

The 28th Street & Camelback Mixed Use PUD is being proposed by LaPour Partners Inc. (“LaPour”), who is currently under contract to purchase the Property. LaPour is a boutique real estate development team focusing on Class ‘A’ employment office as well as hospitality opportunities throughout the Southwest. LaPour has been active in Phoenix real estate development for the past 18 years and are a design conscious firm with a focus on defining themselves as “best in class” quality. LaPour delivers market based/designed buildings with long term relevancy for both their clients and the community.

As such, the proposed PUD will allow for the redevelopment of this Property with a mixed-use, appropriate-scaled, and high-quality/designed development comprising of a four (4) story office building with ancillary financial institution and/or retail shop uses on the first floor along with a five (5) story hotel building with ancillary meeting space, restaurant, bar and amenity (e.g. fitness center, spa, pool etc.) uses. A structured parking garage (surface level and 2 below-grade levels) will provide sufficient parking via an appropriate shared parking model. Moreover, strategically siting the buildings, windows, and if feasible preserving the existing mature trees adjacent to the residential homes as well as enhancing upon the landscaping will create openness and privacy for both nearby residences and users of the proposed development.

As previously mentioned, the Property is underutilized with obsolete and vacant office buildings, originally built in the late 1970's. The proposed *28th Street & Camelback Mixed Use PUD* encourages the redevelopment of the Property to be compatible/marketable with the surrounding area both when it is developed and in to the future. As reflected by the Conceptual Site Plan (See **Exhibit M-3**; Conceptual Site Plan) the office and hotel buildings have both been placed along the Property's Camelback Road frontage and the furthest distance possible away from the residences to the south. This design will respect the neighborhood to the south while activating the Camelback Road frontage. Moreover, the architectural palette and design intent is proposed to be contemporary and timeless as well as mindful of the Property's location as a gateway into the Camelback East Village's Primary Core immediately adjacent to the west.

The goal of the this PUD is to revitalize the Property and to further promote a sustainable concept of living, working, eating, playing, and shopping along the Camelback Road retail corridor from 24th Street east.

With that said, the purpose/intent of this PUD is as follows:

1. Work closely with homeowners and residents of the adjoining neighborhood towards creating a mutually beneficial and symbiotic development.
2. Ensure compatibility with the adjoining neighborhood by limiting permitted uses to the following:
 - i. General, professional and medical use offices – includes accessory retail shop(s) limited to 5,000 sq. ft. in total size on first floor of office building (alcohol sales not permitted);
 - ii. Hotel – includes accessory restaurant(s) and cocktail bars(s) (outdoor entertainment not permitted), meeting rooms and amenities (e.g. spa, fitness center, pool with limited use hours, etc.), and
 - iii. Financial institutions, excluding non-chartered financial institutions.
3. Preserve, buffer, and transition the proposed appropriate-scaled hotel and office development away from the existing residences by providing larger setbacks, limiting building areas (i.e. respectively locating surface and below-grade parking, as opposed

- to above-grade parking, on the south and buildings on the north portions of the Property), limiting height, limiting allowed uses, providing greater open space, and preserving/enhancing trees and landscaping along the Property's south property line.
4. Provide a significant separation between the two (2) proposed buildings to provide a view/light corridor through the Property for those abutting residences.
 5. Continue to prohibit vehicular access to and from the Property at 29th Street.
 6. Continue to enhance and improve upon the neighborhood by providing a high-quality, contemporary, and timeless design that will provide a hotel and Class 'A' offices needed in the area.
 7. Enhance upon the Property's attributes by fronting the buildings on to Camelback Road toward the open space and lake located north across Camelback Road, which will create an interesting street frontage, a more enjoyable walking experience along Camelback Road, and a premium view for the proposed hotel and office users.
 8. Promote sustainable elements associated with the redevelopment; such as the physical built environment, social connections, walkability, and economic benefits.

With that said, the *28th Street & Camelback Mixed Use PUD* will provide for development standards regarding landscaping, screening, design, open space, parking, and building setbacks to buffer/transition from the existing residential homes while still allowing future employment/hospitality opportunities within close proximity to the existing Camelback Road retail corridor, the Camelback East Village Primary Core, transit/multimodal opportunities, and recreational amenities (e.g. Piestewa Peak Park, Arizona Biltmore Golf Club, Los Olivos Park, etc.). In summation, the PUD will ensure compatibility with surrounding properties and promote redevelopment that will generate new tax revenues for the city of Phoenix and the area.

1. REGULATORY PROVISIONS

The PUD has been prepared pursuant to Section 671 of the Zoning Ordinance of city of Phoenix, Arizona (Planned Unit Development) to establish the regulatory framework for the *28th Street & Camelback Mixed Use PUD* by creating development standards specific to the context of the Property.

The PUD is a stand-alone document comprised of project specific zoning regulations, including permitted uses, conditions/limitations, development standards, building heights, landscaping, outdoor storage, driveway locations, and signage. This PUD includes substantial background information to help illustrate the intent of the proposed development. However, the purpose and intent statements are not requirements that will be enforced by the city. Moreover, zoning provisions not specifically regulated by the PUD are governed by the Zoning Ordinance of the city of Phoenix, Arizona. In the event of a conflict between a provision of the PUD and a provision of the Zoning Ordinance of the city of Phoenix, the PUD prevails. The PUD does not modify other city Code provisions or requirements.

The provisions of the PUD apply to all land located within the Property's legally described boundary. (See **Exhibit M-4**; Legal Description) Final site plan(s) for an individual development project or for the entire Property will be processed in the future through the city of Phoenix site plan review process.

2. ZONING ORDINANCE APPLICABILITY

The intended regulatory applicability of the Zoning Ordinance of the city of Phoenix, Arizona, as adopted and periodically amended is applicable to the *28th Street & Camelback Mixed Use PUD* except as modified by the standards contained within this Planned Unit Development.

B. LAND USE PLAN/OVERALL DESIGN

The Property has been designed with the best and highest standards possible in mind while considering the *Camelback East Primary Core Specific Plan's* Design Guidelines, even though the Property falls just outside the boundaries of the *Specific Plan*. However, those Design Guidelines are intended to foster an atmosphere consistent with a vision which LaPour would like to infuse within this proposed development as well as to build upon, retain, and encourage architectural variety, and uniqueness for a quality/high-class development that will be attractive for the area and the ultimate users.

1. THE CONCEPT SITE PLAN.

The Property's Conceptual Site Plan (the "Plan") has been designed to buffer the proposed buildings from single-family residential homes to the south and southeast and to create connectivity between the primary uses proposed. (See **Exhibit M-3**; Conceptual Site Plan) Key elements of the Plan include:

- The establishment of clear and well-designed entry features into the development for both pedestrians and automobiles.
- The establishment of clear pedestrian connections within and bounding the Property.
- Incorporating unique features throughout the Plan and along the buildings bases to blend with the proposed architectural vernacular and provide pleasant/restful spaces for those working, staying and/or doing business within this development.
- Preservation (where feasible) of the mature landscaping (i.e. trees) and enhancing upon said landscaping.
- Greater open space (e.g. hardscape and landscape) in order to create openness with a view corridor to the mountains. This enables a more open view and prevents a wall effect along the street frontage as well as for the abutting residences.

- The continued restriction of the Property’s access to and from 29th Street.
- Limiting openings (i.e. windows/no balconies) facing the residential homes to the south. The south building walls of the hotel nearest to the homes will not contain guestroom windows.
- Providing an open parking lot and landscaping between the two (2) proposed buildings and residential homes (i.e. minimum of 110-foot setback).
- Refuse/loading areas being adjacent to the buildings and not abutting the residential properties.
- Unique or stamped pavement features at the two (2) access points from both 28th Street and Camelback Road.
- Activating Camelback Road with outdoor patio areas.
- Incorporating artwork, site furnishings, and/or other design features that will blend/complement the design influence associated with the Village Primary Core ethos and become a gateway marker.

In addition to the above, the Plan will provide lighting that will blend with the overall design theme along with providing a safe and inviting environment for the employees, visitors, travelers, and local residents coming and going to the Property. Lighting will conform to city of Phoenix Zoning Ordinance. Special care will be taken with the surface parking lighting design and the canopy lighting to provide lighting levels that promote security, but will not spill over into the adjacent residences. As such this may entail the provision of cut-offs or other shielding methods adjacent to residential properties.

In summation, LaPour and the design team have maximized every feasible opportunity for creating a safe, functional, appealing, and stylish redevelopment plan while buffering and screening those around the Property with a mixture of landscape and hardscape details; non-invasive lighting design; building locations; and appropriately located ingress and egress points. The ultimate goal is to create an overall cohesive development environment that will be urban/pedestrian in nature and will support/identify the Property as a unique place adjacent to the Village Core Gradient area while also encouraging/supporting multi-modal transportation options.

2. THE ARCHITECTURE.

The proposed hotel and office design is reflective of the contemporary architecture located within the area and will complement the area’s design influence too. Four (4) sided architecture was a priority in creating the architectural vernacular and to provide visual interest on all sides of the buildings, as well as the use of materials consistent with the adjacent *Specific Plan* and the design influences associated with the area. The

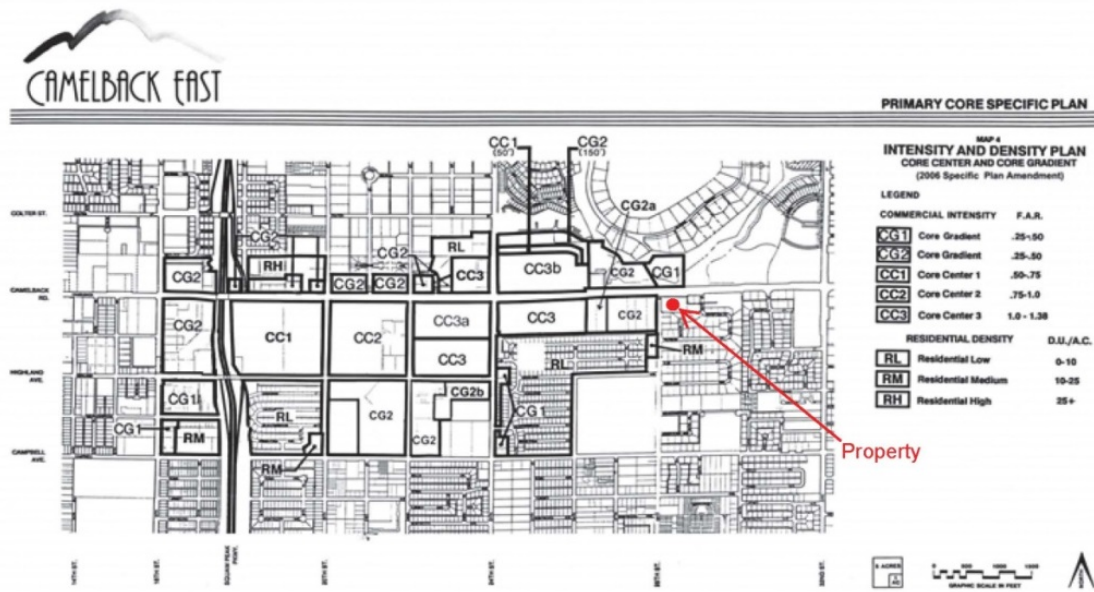
proposed buildings encompass many of the surrounding design elements, such as CMU block, significant roof overhangs, glass curtain walls, rust color metal, etc., while maintaining a unique identity. (See **Exhibit M-5**; Conceptual Elevations) Moreover, the provision of limiting openings (i.e. windows/balconies), both in number and size, facing the residential homes south of the Property was an important component in the design of these buildings, along with the preservation/incorporation of mature landscaping (e.g. trees) and the provision of greater building setbacks (i.e. approximately 110-feet), to provide as much privacy as possible adjoining properties.

The four (4) story office building (56-feet) and the five (5) story hotel (56-feet) will provide for a mix of materials, shading elements, and colors. The Conceptual Elevations use materials and massing changes to provide detail, variation, and contrast to the façades. The windows play an important design feature in addressing the unique context of each side of the façades such as: the mountain/lake side view, residential side view, and the existing offices side views (i.e. 28th Street and 29th Street). The north façades for both buildings provide clean visually open views of the mountain/lake, which is also the best light (i.e. north light). The south facing façade provides less glass and more solid forms around said windows to limit views from the office building. The hotel provides hotel room windows only within the courtyard/pool area only, which is even further setback from the existing residences. Small windows for the internal stairways to provide light are provided at the ends of this ‘u-shaped’ façade. The east and west façades (i.e. 28th Street and 29th Street) provide strategically placed shading overhangs with windows and solid forms to provide optimal views and shade. Finally, each of the building’s designs incorporate pedestrian scale details/materials that make for a more tactile and pleasant social/human connection along the ground plane of these buildings. The proposed incorporation of patio areas will also enhance the pedestrian experience particularly along Camelback Road. The end result will be a visually interesting and tactile design that incorporates some of the key objectives outlined in the *Specific Plan* for design to create a gateway design and better connectivity with the core.

3. BUILDING HEIGHT.

The proposed four (4) story office building (56-feet) and the five (5) story hotel (56-feet) will not be out of scale with surrounding context and/or the abutting *Specific Plan* (i.e. the *Specific Plan* allows up to 56-feet in the adjacent Core Gradient 2 area). The underlying zoning for the properties to the west are: Commercial Office District (C-O) / Limited Parking (P-1) *Camelback East Primary Core Specific Plan* (i.e. Core Gradient-2; CG2) and to the east Commercial Office District / General Office (C-O/G-O). (See **Exhibit M-9 & Figure 3** on the following page, *Camelback East Primary Core Specific Plan’s* Map; **Figure 4** on page 11, *Camelback East Primary Core Specific Plan’s* Development Standards Table, & **Figure 5**; city of Phoenix Zoning Map)

Figure 3

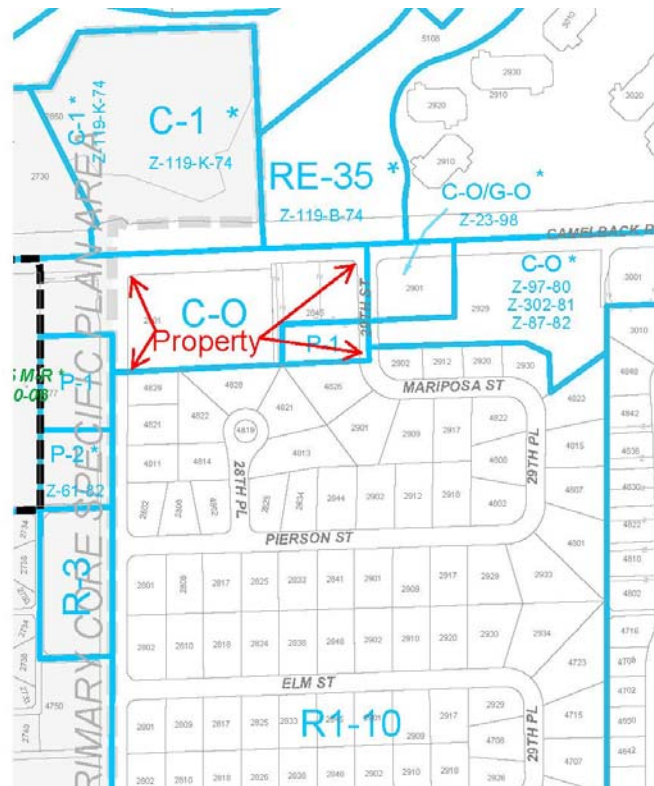


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Figure 4

Table E - Development Standards continued			
	Density (du's/acre)	Building Height	Lot Coverage
CC3a - Hines cont'		The first 56 feet of height may be used for commercial, office or retail activity	
CC3b - Biltmore Fashion Park	96.8 dwelling units per acre	<u>Commercial and residential:</u> height allowed as per Exhibit A except that building height between 112' and 140' is subject to reaoning approval 112' <u>Residential/hotel:</u> as per Exihubit A, 165' subject to rezoning approval. The first 56 feet of height may be used for commercial, office or retail activity	Per underlying zoning
CC3 - Other Sites	96.8 dwelling units per acre	<u>Commercial and residential:</u> total site is allowed 112' by right and up to 140' subject to rezoning approval	Per underlying zoning
CG2a - Bayrock	96.8 dwelling units per acre	<u>Commercial and residential:</u> per underlying zoning <u>Residential/hotel:</u> 75'3" subject to step back provisions and rezoning approval	53%
CG2b - Gray Development	96.8 dwelling units per acre	<u>Commercial:</u> per underlying zoning <u>Residential:</u> total site is allowed 56' feet as of right; up to 70' subject to step back provisions and rezoning approval	Per underlying zoning
CG1 and CG2	96.8 dwelling units per acre for development over four stories	<u>Commercial:</u> per underlying zoning <u>Residential:</u> per underlying zoning with max height of 56' allowed	Per underlying zoning

Figure 5



Moreover, the Property is currently zoned C-O and P-1. Since the Property’s C-O zoning vested prior to January 8, 1986, building heights up to 56-feet, subject to the modification of a one-story height limitation stipulation associated with zoning cases Z-54-75 and Z-93-76, could currently be achieved on the Property.

Thus, properties zoned C-O pursuant to applications filed or processed prior to January 8, 1986, such as the Property (see **Figure 5** above), could achieve building heights up to 56-feet without rezoning. The “post January 8, 1986” C-O district with either a General Office or Major Office option is allowed a height of 25-feet with an increase up to 42-feet if approved by the City Council.

Another point worth noting is that the height proposed (56-feet) is consistent with modern 4 and 5-story office and hotel buildings. In fact the office building is proposed to be 1-story less than a typical 5-story office building, which will not increase the amount of floor space. Thus, the density of people/uses onsite will remain consistent with the overall area context as discussed. Finally, the residential density allowed within the nearby Core Gradient-2 is 96.8 du/ac and up to 56-feet in height, which would be more impactful than the proposed 28th Street & Camelback Mixed Use PUD development.

The height is due to the minimum current standard and new product expectations of national, regional and local tenants within the marketplace that are seeking higher ceiling heights. This trend dates back to the 1960’s when the standard ceiling height for Class

‘A’ office space was 8’-6”’. By the turn of the century, the standard had increased to 10-feet. Most of the office buildings along the Camelback Road corridor have ceiling heights in the range of 9-10 feet. More recent buildings have been developed with 11-foot high ceilings in response to tenant demand for height.

Why taller ceilings? Taller ceilings in office buildings are not just about prestige. The current trend in open office plans with lower partition heights means that large expanses of space are visible, and taller ceilings make these work places more pleasant. The general rule of thumb is that larger rooms need taller ceilings to appear better-proportioned. Taller ceilings also allow more light to penetrate deeper into the building which is important in optimizing daylighting. This is particularly important in office buildings seeking LEED certification, which LaPour will be seeking. Finally, higher ceilings also allow for indirect pendant mounted lighting, which reduces glare and increases productivity.

In the highly competitive Class ‘A’ office market, which is the Camelback corridor, it is necessary to address tenant expectations by building office space that embodies the most current and forward thinking trends, which is currently up to 15-foot ceiling heights. Where possible, with the final office building design, LaPour will incorporate and strategically located the highest ceiling heights possible to obtain a competitive Class ‘A’ office environment. In addition to building a contemporary, sustainable and technologically advanced office building, LaPour proposes to offer the next generation of office space being sought-after by the Creative Class, biotech firms, high-tech firms, young start-ups, etc. Thus, a contemporary 4-story office building designed for today’s tenants with ceiling heights up to 15-feet in height, where feasible, necessitates the need for the building height on the Property.

4. THE LANDSCAPE DESIGN.

The primary goal of the Conceptual Landscape Plan (the “Landscape Plan”) was to include the creation of aesthetically pleasing and rewarding spaces for vehicular and pedestrians both on and off site. Thus, the proposed Landscape Plan reveals a lush/open impression with massing of trees (i.e. existing and new) providing shade along walkways and shrubs in groupings along with the use of ground coverings. (See **Exhibit M-6**; Conceptual Landscape Plan)

In addition, unity and continuity in the Landscape Plan was accomplished by selection of naturally compatible plant material associated with the “Old Phoenix” or “Arizona Biltmore” landscape theme as outlined in the abutting *Specific Plan* as well as the *Camelback East Village Conceptual Core Design Theme* along Camelback Road too. (See **Exhibit M-7**; *Camelback East Village Conceptual Core Design Theme*) The massing of trees and shrubs into groups with uneven spacing presents a more natural appearance to the viewer. Earth grading and shaping is also achieved along the street frontages in a manner that will create natural and aesthetically pleasing ground forms. Furthermore, and most importantly, the design does provide the most efficient, low-

maintenance irrigation systems with the use of grading techniques to preserve and use runoff water effectively to increase the potential for plant survival and growth.

In summation, plenty of open space with “lush” landscaping has been provided along the boundaries and throughout the Property to minimize heat gain, provide a respite for the people traversing the development, and enhance the “Old Phoenix” or “Arizona Biltmore” landscape theme proposed within the *Specific Plan*. The overall result is the most effective means of creating a pleasing, water sensitive environment that meets the many goals of the city for new development.

C. SITE CONDITIONS AND LOCATION

The 28th Street & Camelback PUD is made up of 4.87 gross acres and is comprised of two (2) parcels (i.e. 163-14-067D & 163-14-054A) located immediately adjacent to the Camelback East Village Core gradient (i.e. west and north) and approximately ½-mile east of the intersection of 24th Street and Camelback Road. The Property is generally bounded by Camelback Road, an arterial street, to the north, 28th Street, a collector street, to the west, residential homes to the south and 29th Street, local residential street, to the east. The entire Property, which is currently mostly vacant and obsolete, is owned by the same business entity and is under contract to be purchased by LaPour for the proposed redevelopment outlined. Finally, the Property slopes north to south towards the residential homes with some mature trees that LaPour hopes to preserve and enhance upon.

D. GENERAL PLAN CONFORMANCE

A significant goal of the General Plan is to encourage integration of compatible uses so as to provide a cohesive environment that is in harmony with the existing character of the general area. The subject Property is located within the Camelback East Village and abuts the *Camelback East Primary Core Specific Plan's* Core Gradient area as noted by the city of Phoenix General Plan. (See **Exhibit M-8**; General Plan Map, & **Exhibit M-9**; *Camelback East Primary Core Specific Plan Map*)

The current General Plan Land Use designation for the Property is “Commercial” land use, which accommodates retail, service, and multi-family uses, among others. The 28th Street & Camelback PUD meets several of the 2015 voter approved General Plan’s Vision, Community Benefits, and 5 Core Values. They are:

Vision (emphasis added)

“Phoenix will continue to be like no other city in the world – a place steeped in history, defined by its beautiful desert landscape, **activated by unique neighborhoods and businesses and embodied by a pervading sense of opportunity and equity.** Phoenix will become an even greater city by **building on its existing wealth of assets and by enhancing residents’ opportunities to connect to these assets and each other.** By **becoming a more “connected” city,** Phoenix **residents will benefit with enhanced levels of prosperity,** improved health and a thriving natural environment. Bringing the

great people and places of this flourishing desert metropolis together is what will solidify Phoenix's identity as the Connected Oasis.”

The above vision clearly defines the proposed 28th Street & Camelback PUD by creating mixed-use PUD development that will provide a desirable and needed employment (e.g. office) and hospitality alternative to accommodate today's and the long-term market. The 28th Street & Camelback PUD will provide for future employment/hospitality opportunities within close proximity to existing residences; enhance/build upon the existing Camelback Road retail corridor; blend and be compatible with the Camelback East Village Primary Core; encourage transit/multimodal opportunities, and support recreational amenities (e.g. Piestewa Peak Park, Arizona Biltmore Golf Club, Los Olivos Park, etc.) by providing for a hotel. In summation, the PUD will build upon the local assets in the area, ensure compatibility/connectivity, and activate the area with new businesses as well as promote additional redevelopment/reinvestment in the area that will generate new tax revenues for the city of Phoenix.

Community Benefits

Prosperity (emphasis added)

“Providing for a robust and healthy climate for both large and small businesses, enhancing the skills of our workforce, and ensuring that the costs associated with building and maintaining our great city do not place an economic burden on our residents, are just some of the challenges we face in becoming a more prosperous city. Phoenix has made strategic investments in transportation and education over the last decade that have already begun to provide for more job growth and educational opportunities for its residents. Continuing to build on these investments provides for a more prosperous future for the city.”

The proposed 28th Street & Camelback PUD abuts the Primary Core of the Camelback East Village and is situated along the Camelback Road retail corridor from 24th Street east, which is recognized as one of the city's premier employment/retail spines and is consider a significant concentration of employment, retail, and medium/high residential densities. The proposed mixed-use PUD provides unique opportunity to build on the city's investments in transit, streets, sewer/water, etc. to stay/live, work, and play (i.e. enjoy entertainment/recreational amenities) in the immediate area. It is anticipated that the proposed development will result in considerable, ongoing city sales tax revenue which will be created from the spending activity of those future employers/employees and hotel guests. Thus, the proposed 28th Street & Camelback PUD will not be an economic burden, but a benefit for the residents living in the area in many ways.

Health (emphasis added)

“Phoenix's beautiful desert setting provides tremendous amenities that allow residents to live an active and healthy lifestyle. Few cities in the world can have the combination of climate and open space that Phoenix enjoys. Protecting and enhancing Phoenix's natural elements will not only provide recreational opportunities but improve residents' health with cleaner air, soil and water. Phoenix also serves as the medical epicenter

for the state of Arizona. Its hospital and health care education infrastructure contribute to helping make the city healthier.”

The development and redevelopment of aging properties, as well as undesirable and vacant buildings, within the city provides greater stability and a healthier environment, because of existing infrastructure already in place and amenities/services already built and provided for by the city and private businesses. Thus, refocusing and recalibrating development to already established areas within the city is important in that it limits sprawl, auto dependency, supports infrastructure/recreational/transit use, maintains (where possible) mature trees as well as improving landscaping, reduces the “heat island” effect, and allows for the construction or remodeling of buildings/sites with greater energy/water efficiencies which helps to improve everyone’s health with cleaner air, soil, and water. Finally, the Class ‘A’ office proposed will be developed to attract medical and/or biotech users due to its location along Camelback Road and its contemporary, sustainable, and technologically advanced design paradigm by embodying the most current and forward thinking trends in order to meet tenant expectations

Environment (emphasis added)

“Creating a thriving city in the desert has taken centuries of ingenuity and innovation by generations of Phoenix residents. We will need to continue to call upon this ingenuity and innovation if we are to address the challenges we face regarding our natural resources. Phoenix has long been a leader in the environmental movement. From its commitment to having **new facilities meet or exceed Leadership in Energy and Environmental Design (LEED) standards,** to its ongoing efforts to acquire and preserve thousands of acres of desert and mountain areas. By continuing to celebrate and harness its natural assets, Phoenix will continue to build a sustainable future.

LaPour is committed to the “Leadership in Energy and Environmental Design” (LEED) Green Building Rating System which is presently a voluntary standard for developing high-performance sustainable buildings. With that said, LaPour proposes to develop the Property with the highest LEED certified rating achievable. Many of the elements associated with LEED have already been incorporated within the conceptual building design (e.g. glass/windows, materials, shade, etc.) and the Conceptual Site Plan design (e.g. preserving trees, providing shaded walkways/parking areas, capturing runoff for irrigation, etc.).

CORE VALUE: CONNECT PEOPLE & PLACES

Complete Streets (emphasis added)

“Since the founding of Phoenix, the grid system has been a critical part of our city’s physical layout. Phoenix residents love the grid system, but **want more bicycle, pedestrian and transit infrastructure.** Existing streets were built with a focus on the automobile, but other modes are equally important, and infrastructure should be repurposed to support these needs. Complete Streets are streets which are planned, designed, operated and maintained to **support and encourage walking, bicycling and**

transit use while promoting safe and effective operations for users of all ages and abilities.”

Goal: “Create a system of streets which encourage and facilitate active transportation, support investment in transit, foster social engagement and community pride, improves safety for all transportation modes, supports the local economy and property values, and **improve the livability and long-term sustainability of our region.**”

Design: **Locate parking to the rear of a site to create a more pedestrian environment, when adequate shielding from noise and light can be provided to adjacent established neighborhoods.** On-street parking in some areas may also promote a pedestrian environment.

Design: In order to balance a more sustainable transportation system, development should be designed to include **increased amenities for transit, pedestrian and bicyclists such as shade, water, seating, bus shelters, wider sidewalks, bike racks, pedestrian scale lighting and way-finding.**

The 28th Street & Camelback Mixed Use PUD represents the evolution of this Property from a low-scale suburban style office development, which is no longer economically or physically sustainable, to a highly sought-after, Class ‘A’ employment, and tourist/business destination that will provide greater pedestrian/bicycle access/opportunities, shaded pedestrian environment both onsite and offsite, an enhanced/upgraded bus shelter, and adequate shielding of the adjacent existing residential neighborhood via the parking lot placement (rear), building locations (fronting Camelback Road), and shielded/cut-off lighting.

Bicycles (emphasis added)

“Bicycles have long been a popular mode of transportation for Phoenix citizens. As a city, we will ensure the community has the proper infrastructure necessary for bicyclists to safely, and efficiently travel from home to work, from work to school, and from school to a park or other recreational activities. **Bicyclists**, from those using the infrastructure to commute, to those using bicycles as a form of recreation are in **need of a variety of different types of infrastructure**, from the macro level bike-way system, to the **micro level specifics such as bike lockers** or showers.”

Goal: “Develop the city’s bike-way system into an accessible, efficient, connected, safe and functional network which **promotes bicycling** and quick access to any destination. Establish a network of bicycle amenities at major destinations.”

Design: **“Development should include convenient bicycle parking.”**

The 28th Street & Camelback Mixed Use PUD will provide for storage lockers accessible to the office building’s occupants/employees and to hotel employees. Moreover, onsite bicycle

parking, as specified by Section G of this PUD narrative, for visitors and others will be provided. More importantly, shaded sidewalks will provide additional comfort for bicyclist.

Public Transit (emphasis added)

“Many Phoenix and surrounding community’s residents rely on Phoenix’s mass transit system as their primary source of transportation for work, school and other purposes. Our mass transit system is made up of buses and rail, but also includes our airport infrastructure, a crucial transportation link to the rest of the world. The system should be efficient, reliable, frequent and comprehensive. While the Phoenix system has and continues to provide a high level of service, improvements should be made to **encourage ridership and provide relief to the local street and freeway systems.**”

Goal: “Develop the Phoenix transit system into an efficient multi-modal transportation system which will allow for the movement of people safely and efficiently, **connecting the many activity and employment centers and neighborhoods throughout the city.** Meet the demand for the range of services needed, connecting neighborhoods to local bus routes, rapid transit, and fixed guideway transit systems.”

Design: “Develop **transit facilities** in appropriate cores, centers and **corridors to facilitate trip reductions and use of mass transit.**”

Design: “**Development should be designed** or retrofitted, as feasible, to **facilitate safe and convenient access to transit facilities by all existing and potential users.**”

The 28th Street & Camelback Mixed Use PUD will be designed and implemented to encourage transit ridership; in this case bus. Enhancements to the bus shelter and shade along Camelback Road will provide a more pleasant/safe environment for one waiting for the bus. Additional mechanisms with this PUD; such as: the office building participates in a transportation management association that sponsors trip reduction programs, will also assist in encouraging ridership.

CORE VALUE: STRENGTHEN OUR LOCAL ECONOMY

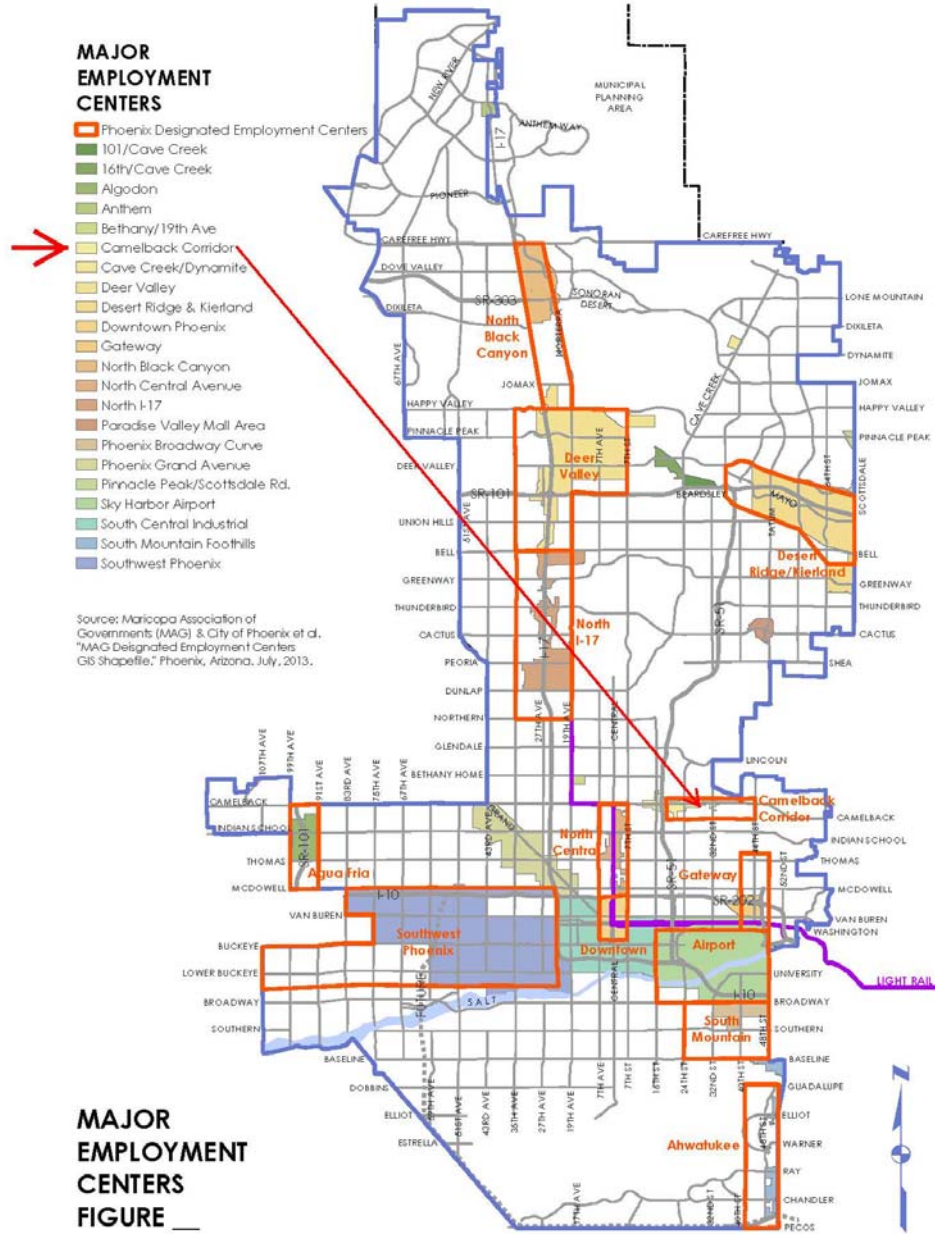
Job Creation (Employers) (emphasis added)

“For the average Phoenix resident, our economy ultimately comes down to one thing: jobs. A resident’s economy is really only as strong as its economic base. A strong job market builds on itself by creating more disposable income which in turn creates more jobs. **As a city, it is crucial that we continue to improve and support our employment sector.**”

Goal: “Our major employers and established employment centers will continue to grow and provide high quality, wealth generating employment opportunities. **Facilitate**

job creation in targeted high-growth/high-wage industry sectors and targeted trade industry sectors.”

Land Use: **“Support General Plan Land Use Map and zoning changes that will facilitate the location of employment generating uses in each of the designated employment centers.”**



The 28th Street & Camelback Mixed Use PUD is located within the city’s Major Employment Centers (i.e. Camelback Corridor) (See **Major Employment Centers Map** on the previous page) and subsequent development of the Property will contribute significantly to the job market via construction jobs initially to ongoing employment by the office users and hotel use. With that being said, LaPour proposes to offer the next generation of office space being sought-after by the Creative Class, biotech firms, high-tech firms, young start-ups, etc. in this area of the Camelback Corridor.

Entrepreneurs Emerging Enterprises (emphasis added)

“Our community’s creative entrepreneurs and emerging enterprises drive our city’s small businesses. **The world’s largest companies started with an innovative vision, a drive, and a lot of hard work from an entrepreneur.** To assist entrepreneurs, access to other small businesses, suppliers and support facilities are crucial to their success. **New businesses need support to increase the likelihood of their sustained success.**”

Goal: “Establish a robust entrepreneurial and innovative eco-system that **supports local/organic growth as well as having an appeal to attract national/global interests.**”

Land Use: **“Encourage land uses that promote the growth of entrepreneurs or new businesses in Phoenix in appropriate locations.”**

The 28th Street & Camelback Mixed Use PUD is ideally located within the Camelback Corridor and near the Camelback East Village’s Primary Core, which is a highly sought-after area for businesses, travelers, residents, and employees to be within and adjacent to such a dynamic, diverse, and growing popular area. Moreover, the potential growth and success of locally owned businesses, or any business, is driven in many ways by areas with synergy/connectivity of people, uses, and amenities that attract businesses of all kinds that cater to diverse populations; that provide for recreational amenities and/or pleasing environments, and that provide for a diverse stratum of hospitality users (i.e. budget, midlevel, resort, etc.). Thus, it is envisioned that the proposed office space, as designed, will indeed attract potential incubators of new businesses or expanding locally owned businesses. The proposed hotel will also fill a niche of providing a mid-to high-level accommodations.

CORE VALUE: CELEBRATE OUR DIVERSE COMMUNITIES & NEIGHBORHOODS

Certainty and Character (emphasis added)

“What makes a city a great place to live are its robust vibrant neighborhoods. There is a level of certainty one expects to have and quality of life one expects to maintain while living in a great city. The goals and policies that are outlined in the General Plan were created so residents have a reasonable expectation and level of certainty while living in our great city; certainty in regards to quality of life and compatibility. **The success, stability and certainty our neighborhoods can provide only strengthen our city and region’s vitality and prosperity.** A city’s identity is not only created by unique places

and spaces, but by the residents who live within its borders. The cultural diversity, rich architectural style and truly unique neighborhoods (from large lot rural communities to suburban and urban neighborhoods) help define its character.”

Goal: “Every neighborhood and community should have a level of certainty. Ensure that development, redevelopment and infrastructure supports and reinforces the character and identity of each unique community and neighborhood.”

Land Use: “Locate land uses with the greatest height and most intense uses within limits based on village character, land use needs, infrastructure and transportation system capacity.”

Land Use: “New development and expansion or redevelopment of existing development in or near residential areas should be compatible with existing uses and consistent with adopted plans.”

Design: “Create new development or redevelopment that is sensitive to the scale and character of the surrounding neighborhoods and incorporates adequate development standards to prevent negative impact(s) on the residential properties.”

Design: “Provide impact-mitigating features (such as extra width or depth, single story units, or landscape buffering) when new residential lots abut existing non-residential uses or are adjacent to arterial streets or freeway corridors. Dissimilar land uses often require additional separation or other measures to achieve compatibility.”

Design: “Require appropriate transitions/buffers between neighborhoods and adjacent uses.”

The 28th Street & Camelback Mixed Use PUD is consistent and compatible with the city’s General Plan land use designation of “Commercial;” with the city’s Major Employment Centers (i.e. Camelback Corridor), and the *Conceptual Core Design Theme* along Camelback Road which the Property falls within its boundaries. The PUD standards and proposed Conceptual Site Plan will not be out-of-scale with surrounding context, as it is designed to have the two (2) buildings setback approximately 110-feet from the residences to the south. In addition, many other mitigating items are being proposed to buffer/transition the proposed development; such as: limiting windows/no balconies along the rear of these buildings facing the residences; maintaining as many of the mature trees as feasible as well as adding trees; placing surface parking and landscaping between the buildings to provide a buffer to the residences; providing a view corridor between the two (2) buildings for light/views, and providing for greater openness and landscaping.

Safe Neighborhoods – Traffic (emphasis added)

“Residents want to live in neighborhoods that do not have cut through or high volumes of vehicular traffic. Road traffic crashes pose public health challenges. Communities wish to work with traffic engineers to mitigate and prevent serious injuries caused by vehicle to vehicle collisions and vehicle collisions with other roadway users (pedestrians, cyclists, runners).”

Goal: “The **community should be protected from the negative effects** of the volume, speed and **cut-through traffic** in neighborhoods.”

Land Use: “Minimize traffic through lower-density residential areas by **locating heavy traffic generating land uses on or near arterial streets.**”

The 28th Street & Camelback Mixed Use PUD will have no impact on the abutting residences to the south since there is no access (driveway) from the Property to 29th Street, 28th Street will only allow for north/south and east/west movements to be handled at the signalized intersection, and the driveway proposed along Camelback Road will only allow for east movement. A Traffic Impact Analysis (“TIA”) (See **Exhibit M-14**; Traffic Impact Analysis) was completed and the TIA identifies three (3) recommendations.

1. New STOP signs are recommended for exiting movements at both of the proposed driveways.
2. Install a protected/permissive left turn arrow phase for westbound left turning traffic at the intersection of 28th Street/Camelback Road.
3. The installation of warning signs with the text “NOT A THROUGH STREET” are recommended immediately east of 28th Street on Pierson Street and immediately south of Camelback Road on 29th Street to deter traffic from passing through the residential neighborhood.

With the proposed development fronting on Camelback Road and with a signalized intersection along with the recommendations noted above there will not be any adverse effects on the abutting neighborhood.

Clean Neighborhoods (emphasis added)

“Clean and well maintained neighborhoods are an indication of an area’s character and helps **support private investment to provide a safe and healthy community to live.**”

Goal: “The preservation, maintenance and improvement of property conditions should be promoted to mitigate or **eliminate deterioration or blight conditions** and to help **encourage new development and reinvestment** within our communities.”

Land Use: “Support new compatible land uses that remove extremely deteriorated structures, excessive trash and debris, and other blight in neighborhoods.”

The 28th Street & Camelback Mixed Use PUD will revitalize this Property from a low-scale 1970’s suburban style office development, which is no longer economically or physically sustainable. The Property is in a deteriorating state and challenged to find long-term, stable tenants. The Proposed redevelopment will invigorate and contribute to the continued success of the neighborhood by providing a visually appealing Property, as well as uses they can utilize or visit (e.g. hotel, restaurant, etc.).

Arts & Culture (emphasis added)

“As the city continues to grow, cultural and artistic experiences will be increasingly vital to its quality of life. It is essential to infuse arts and culture into all scales and aspects of city development. Investment in arts and culture will enrich the lives of Phoenix residents, attract businesses, build social connections, generate jobs, and bolster our economy. Arts and culture help create a livable community that enhances the built environment and respects and celebrates diversity.”

Goal: “Ensure Phoenix becomes an Arts & Culture destination by encouraging new public art projects, maintenance of existing public art, and support for arts and cultural activities throughout our communities.”

Land Use: “Encourage the provision of art in all new development for both public and private.”

The 28th Street & Camelback Mixed Use PUD proposes to incorporate artwork, site furnishings, and/or other design features that will blend/complement the design influence associated with the Village Primary Core and incorporate, as much as possible, the *Conceptual Core Design Theme* along Camelback Road to make the development a gateway marker for the core. In addition, the PUD contemplates working with the city on an upgraded, architecturally distinguishable bus shelter that blends with the development design and the *Conceptual Core Design Theme*.

CORE VALUE: BUILD THE SUSTAINABLE DESERT CITY

Green Building (emphasis added)

“Incorporating sustainable practices, materials and energy efficient projects saves energy and money while protecting our environment and contributing to our city’s sense of place. The city will be proactive in creating more resource-efficient, durable and energy efficient buildings for new city projects and for new construction and remodeling for private projects.”

Goal: “Establish Phoenix as a leader in green/sustainable building through the use of green/sustainable building techniques in private and public development.”

Design: “Encourage bioclimatic designs of buildings and approved natural materials for construction.”

Design: “Encourage high-performance building designs that conserve resources, while balancing energy-efficient, water-efficient, cost-effective and low-maintenance engineering solutions and construction products through whole building life cycle assessment.”

Design: “Promote site development and land use which protects the natural environment by preserving vegetation and surface water, minimizes disturbances to the existing terrain and greenfields, and encourages development of brownfields in synergy to our the desert climate.”

LaPour proposes to develop the Property with the highest LEED certified rating achievable. Many of the elements associated with LEED have already been incorporated within the conceptual building design; such as: providing for taller ceiling heights to allow more light to penetrate deeper into the building which is important in optimizing daylighting; focusing the majority of glass/windows towards the north; utilizing natural local materials; and incorporating shade elements on the buildings. The Conceptual Site Plan design and the PUD promote preserving mature trees as well as providing more trees for shaded walkways/parking areas, capturing runoff for irrigation, utilization (where feasible) of alternative paving materials other than asphalt, etc.

Trees & Shade (emphasis added)

“Investment in trees and shade is one of the best things Phoenix can do to improve the city’s overall health, prosperity and environment. By integrating trees and shade into the built environment, issues such as storm water management and the urban heat island can be addressed.”

Goal: “Create a network of trees and shade that integrate with the built environment to conserve ecosystem functions and provide associated benefits to residents.”

Design: “Integrate trees and shade into the design of new development and redevelopment projects throughout Phoenix.”

Design: “Plant drought tolerant vegetation and preserve existing mature trees in new development and redevelopment.”

Design: “New development should minimize surface parking areas and provide an abundance of shade through either trees or structures on any planned parking areas.”

The 28th Street & Camelback Mixed Use PUD's Conceptual Landscape Plan creates an aesthetically pleasing and rewarding spaces for vehicular and pedestrians both on and off site. Thus, the proposed landscape plan reveals a lush/open impression with massing of trees (i.e. existing and new) providing shade along walkways and shrubs in groupings along with the use of ground coverings. (See **Exhibit M-6**; Conceptual Landscape Plan) Furthermore, the massing of trees and shrubs into groups with uneven spacing presents a more natural appearance to the viewer. Most importantly, the design does provide the most efficient, low-maintenance irrigation systems with the use of grading techniques to preserve and use runoff water effectively to increase the potential for plant survival and growth.

It should be noted, the parking lot area will also be well shaded with both trees and parking canopies as well as by the buildings themselves.

Finally, landscaping has been provided along the boundaries and throughout the Property to minimize heat gain, provide a respite for the people traversing the development, and enhance the "Old Phoenix" or "Arizona Biltmore" landscape theme proposed in the *Specific Plan*. The overall result is a pleasing water sensitive environment.

Energy Infrastructure (emphasis added)

"Phoenix has taken the charge to promote sustainable change by reducing energy consumption through public private partnerships, thus making our energy supply cleaner and more affordable. With endless amounts of sunshine and an abundance of local materials, Phoenix's workforce is better equipped than most. **A central strategy for improving our energy system is to reduce energy consumption in existing buildings, which is the most cost-effective way to reduce Green House Gas emissions.** Efficiency improvements will save money and energy, while also creating skilled, local jobs."

Goal: "**Continually seek and promote energy efficiency** and emerging energy strategies."

Design: "**Provide incentives such as parking reductions** or density bonuses for **project's that incorporate energy efficient designs** or alternative energy infrastructure."

The 28th Street & Camelback Mixed Use PUD proposes many of the elements associated with energy efficiency, promoting alternative modes of transportation, and providing for a comfortable pleasing environment both indoors and outdoors. As mentioned, the conceptual building design provides for taller ceiling heights to allow more light to penetrate deeper into the building which is important in optimizing daylighting; focusing the majority of glass/windows towards the north; utilizing natural local materials; and incorporating shade elements on the buildings. Moreover, the utilization of shade trees to also provide cooling along the lower floors of the building. Finally, LaPour proposes to develop the Property with the highest LEED certified rating achievable.

E. ZONING AND LAND USE COMPATIBILITY

The proposed rezoning is compatible with the adjacent zoning pattern and the surrounding uses. (See **Exhibit M-1**; Context Aerial Map, **Exhibit M-8**; General Plan Map, **Exhibit M-9**; *Camelback East Primary Core Specific Plan* Map, & **Table 1** below) Additional mitigating design standards, limited uses, and building/landscape setbacks proposed within this PUD will also help to blend/transition the Property with its surroundings.

The 28th Street & Camelback PUD request clearly represents the highest and best use for the Property as it provides opportunities for a greater/marketable level of uses/buildings and re-establishes the Property for the immediate neighborhood and enhances upon the Village core. The common elements proposed in this PUD will provide for a comprehensive mid-urban development/environment that will encourage interaction between uses and beyond as well as reinvestment/employment.

Table 1 Surrounding General Plan Designations, Zoning Districts and Land Uses

Location	General Plan	Zoning	Existing Land Use
PUD	Commercial	Prior to January 8, 1986 Commercial Office District (C-O) & Limited Parking District (P-1)	1970's, 1-story via stipulation with rezoning, limited office type uses with surface parking.
North	Commercial (Village Core Gradient; CG1) & Residential (3.5 -5 du/ac)	Neighborhood Retail District (C-1) & Single-Family Residence District (RE-35)	Open space/lake (RE-35) & 3-story office building with surface parking.
South	Residential (3.5 -5 du/ac)	Single-Family Residence District (R1-10)	Existing single-family neighborhood (1 and 2-stories).
East	Commercial	Commercial Office District (C-O) & Commercial Office District (C-O) / General Office District (G-O)	2-story offices with surface parking.
West	Commercial (Village Core Gradient; CG2)	Commercial Office District (C-O) & Limited Parking District (P-1)	3-story office building with surface parking.

F. LIST OF USES

The following limited list of uses (See **Table 2** on the following page) defines permitted uses and permitted uses subject to certain conditions/limitations from the city of Phoenix's Zoning Ordinance. The proposed limited list of uses (i.e. hybrid C-1 and C-O) along with the necessary development standards provides appropriate transition between the more intense commercial activities and Camelback Road for the nearby residential homes. Moreover, the proposed uses were developed with an understanding of the adjacent residential homes and the neighborhood's concerns. Thus, the PUD provides a compatible/appropriate list of uses and accessory uses that accomplishes those concerns, but is still a viable/marketable development (i.e. office and hotel). The *28th Street & Camelback PUD* provides for an optimal level of uses and development standards that are compatible with the Property, surrounding area, and vision for the development.

1. Permitted Uses.

Permitted Principal Uses

- a) Uses specifically permitted as set forth in **Table 2**.

Permitted Uses Subject to Conditions/Limitations Standards

- a) Uses specifically permitted subject to conditions/limitations as set forth in **Table 2**. Performance Standards are specific for each individual use as defined. Some uses subject to conditions/limitations may also require a use permit, which is processed in accordance with Sections 307 and 502 of the city of Phoenix Zoning Ordinance.

2. Unless otherwise specifically provided elsewhere in the PUD, the following are additional allowances within the PUD and restrictions that shall apply.

- a) Temporary uses shall be permitted pursuant to regulations and standards for temporary uses specified by Section 708 of the city of Phoenix Zoning Ordinance. Administrative temporary use permit regulations, procedures, and standards specified by Section 708 of the Zoning Ordinance shall apply to temporary outdoor storage and staging use areas for durations up to one year.
- b) Promotional events shall be permitted pursuant to regulations and standards for promotional events specified by Section 708 of the city of Phoenix Zoning Ordinance.
- c) The outdoor use of a public address system shall be prohibited.

d) Loading Dock areas

- i. A dock area shall be considered as the area immediately adjacent to the building where trucks and/or trailers are parked for the purpose of loading and unloading to and from the building.
- ii. A dock area shall be screened from public streets and adjoining residential uses with landscaping or a wall, or a combination of both, and located so as to not front onto a public street, an outdoor patio, or an outdoor amenity (i.e. pool) area. A dock area shall also not be located within 100 feet of the main entrance of either the office or hotel buildings.
- iii. Loading dock use shall be limited to the hours between 7:30 a.m. to 8:00 p.m., Monday through Friday and use of loading dock areas shall be prohibited on Saturdays, Sundays and national/Federal holidays.

e) All accessory uses shall only be permitted on the 1st floor of any building.

TABLE 2

Permitted Principal Uses		Conditions/Limitations
1.	General, professional, and medical offices.	<p>Medical office uses include, but are not limited to the following: surgi-centers, dialysis centers, and urgent care centers, so long as there are not any overnight stays.</p> <p>Medical and dental laboratories in excess of 3,200 square feet shall be permitted only upon securing a use permit; and provided, that the entrance shall be only from within the interior walls of the building.</p> <p>Accessory retail shop(s) limited to 5,000 square feet of gross leasable area in total size (alcohol sales not permitted). Shop(s) include the following, but are not limited to the following, gift shops, flower shops, snack bars, pharmacy, office supplies, shoe repair, etc. conducted for the convenience of the employees,</p>

Permitted Principal Uses		Conditions/Limitations
		<p>patients, patrons, or visitors to the premises.</p> <p>Accessory day care center not to exceed 2,000 square feet of gross leasable area for convenience of the employees of the premises.</p> <p>Accessory conference center in conjunction with an individual tenant or for the purpose of serving the entire office building. The conference center's floor area shall not exceed ten percent (10%) of the office building's total gross leasable floor area.</p> <p>Accessory cafeteria (only for employees within office building). The cafeteria's floor area shall not exceed fifteen percent (15%) of the office building's total gross leasable floor area.</p>
2.	Hotel.	<p>Accessory restaurant(s), cocktail bar(s), outdoor patio(s), meeting rooms, spa, fitness center, pool, and gift shop are subject to the following conditions:</p> <ol style="list-style-type: none"> 1.) All necessary city and State liquor licensing process and procedures shall apply; 2.) Any outdoor patio or pool shall be setback a minimum of 110-feet from the south property line; 3.) The combined area of the outdoor patio(s) located along Camelback Road for food and beverage service shall be limited to maximum of one-thousand (1,000) square feet;

Permitted Principal Uses		Conditions/Limitations
		<p>4.) The outdoor pool's combined area for food and beverage service shall be limited to maximum of six-thousand (6,000) square feet;</p> <p>5.) Live outdoor entertainment shall not be permitted;</p> <p>6.) Food and beverage service within any outdoor patio located along Camelback Road shall be limited to the hours of 7:00 a.m. to 1:00 a.m.;</p> <p>7.) Use hours for the outdoor pool, including food and beverage service, shall be limited to the hours of 7:00 a.m. to 10:00 p.m.;</p> <p>8.) Employees shall have unobstructed views into any outdoor patio or outdoor pool area or shall be present to monitor patron activities, and</p> <p>9.) In accordance with Zoning Ordinance Section 307A.7.g and 307A.7.h, the Zoning Administrator upon a finding that there has been material noncompliance with one or more of the above conditions may modify such conditions, may impose additional conditions, or may revoke the ability to provide outdoor uses.</p> <p>10.)The maximum amplified sound level shall not exceed fifty-five (55) dBA at any property line.</p>

Permitted Principal Uses		Conditions/Limitations
3.	Financial institutions.	<p>Non-chartered financial institutions shall not be permitted.</p> <p>Accessory freestanding or attached to the building automated teller machine shall be allowed on the property. Any external automated teller machine shall be setback a minimum of 100-feet from the south property line and shall be reviewed at the time of site plan development review approval for proper location and circulation.</p> <p>Any drive-through facilities shall be setback a minimum of 100-feet from the south property line.</p> <p>A maximum of one (1) external automated teller machine or drive-through facility shall be permitted within 330-feet of the west property line.</p>

G. DEVELOPMENT STANDARDS

The proposed development standards are consistent/appropriate with the surrounding area which accommodates the uses proposed along with these low-rise buildings. The proposed development standards also include meaningful landscaping standards, bicycle parking standards, shade, amenities, building massing, and pedestrian oriented design techniques associated with the *Camelback East Primary Core Specific Plan's* Design Guidelines, *Conceptual Core Design Theme* along Camelback Road, and the city of Phoenix Zoning Ordinance Design Guidelines, *Section 507, Tab A* to ensure compatibility with the adjacent properties while still being a unique signature development. For illustrative purposes, a comparative zoning standards table is included as an exhibit to show the contrast, and improvements, between the existing C-O and P-1 zoning districts and the proposed PUD zoning's development standards. (See **Exhibit M-11**; Comparative Zoning Standards Table)

With that being said, the proposed primary use buildings, four (4) story office building and five (5) story hotel building, will not be out-of-scale with surrounding context and the Property is being designed to have the two (2) buildings setback approximately 110-feet from the residences to the south. The *28th Street & Camelback Mixed Use PUD* and concept designs propose the following:

1. Limiting windows/balconies along the rear of these buildings facing the residences.
2. Maintaining as many mature trees as possible and feasible as well as adding trees.
3. Placing surface parking and landscaping between the buildings and residences to the south to provide a buffer.
4. Providing a view corridor between the two (2) buildings for light/views.
5. Providing for greater openness and landscaping.
6. Restricting the Property's access to and from 29th Street, the only local street adjoining the Property.

The residential density allowed within the abutting Core Gradients 1 and 2 of the *Specific Plan* to the west and north of the Property allows for 96.8 du/ac and up to 56-feet in height. Thus, the density of people/uses onsite will remain consistent with the overall area context now and in the future.

1. SETBACK, BUILDING HEIGHT, LOT COVERAGE, PARKING DIMENSIONS, OFF-STREET LOADING, VEHICULAR ACCESS & REFUSE ENCLOSURE STANDARDS

- a. Maximum perimeter building setback along Camelback Road: 22-feet.
- b. Minimum perimeter building setbacks for primary use (office building and hotel) buildings and accessory structures (parking garage and parking canopies) unless otherwise specified:
 - i. Front (Camelback Road): 18-feet for up ninety-five percent (95%) of frontage; 10-feet for up to five percent (5%) of frontage.
 - ii. Street Sides (28th and 29th Streets): 10-feet.
 - iii. Rear (south): 110-feet for office and hotel buildings; 10-feet for parking garage and parking canopies.
- c. Minimum perimeter landscape setbacks:
 - i. Front (Camelback Road): 18-feet for up ninety-five percent (95%) of frontage; 10-feet for up to five percent (5%) of frontage.

- ii. Street Sides (28th and 29th Streets): 10-feet.
 - iii. Rear (south): 10-feet (inclusive of vehicle overhang no greater than 30-inches).
- d. Maximum heights:
- i. Primary use (office and hotel) buildings: 56-feet. **Note:** A height of 58.8-feet shall be allowed or up to a five percent (5%) allowance to the 56-foot height proposed in order to provide some flexibility as envisioned by the PUD, if necessary, to accommodate unknown future site conditions, design, or building constraints as the project is developed.
 - ii. Parking garage to be located at or below finished grade, excluding screen walls for surface level parking and safety and screen walls adjoining ramps and/or stairs to and from below-grade parking levels.
 - iii. Accessory structures, including parking canopies: 12-feet.
- e. Maximum projections:
- i. Open projections, such as a roof overhang, an attached awning, a building architectural feature, a balcony, a patio and a freestanding shade structure, may project up to fifty percent (50%) into the required perimeter building and landscape setbacks along Camelback Road and 28th and 29th Streets for no more than a combined total of fifty percent (50%) of the applicable street frontage width of a primary use building.
 - ii. Open projections, such as a roof overhang, an attached awning, a building architectural feature, a patio and a freestanding shade structure, may project up to 10-feet into the required rear yard perimeter building setback for primary use buildings for no more than a combined total of fifty percent (50%) of the width of a primary use building.
 - iii. Closed projections, such as a bay window, oriel, entrance, or vestibule 10-feet in width or less, may project up to 3-feet into any required building or landscape setback, excluding the rear perimeter yard landscape setback, for no more than a combined total of fifty percent (50%) of the applicable width of a primary use building.
- f. Maximum lot coverage:
- i. Eight-five percent (85%) overall total (includes underground structured parking). **Note:** A maximum eighty-nine and one-quarter percent (89.25%) shall be allowed or up to a five percent (5%) allowance to the eight-five percent (85%) overall total proposed in order to provide some flexibility as

envisioned by the PUD, if necessary, to accommodate unknown future site conditions, design, or building constraints as the project is developed.

- ii. Forty percent (40%) total for primary use buildings. **Note:** A maximum forty-two percent (42%) shall be allowed or up to a five percent (5%) allowance to the forty percent (40%) total proposed in order to provide some flexibility as envisioned by the PUD, if necessary, to accommodate unknown future site conditions, design, or building constraints as the project is developed.
- iii. Ten percent (10%) total for accessory structures, including parking canopies. **Note:** A maximum ten and one-half percent (10.5%) shall be allowed or up to a five percent (5%) allowance to the ten percent (10%) total proposed in order to provide some flexibility as envisioned by the PUD, if necessary, to accommodate unknown future site conditions, design, or building constraints as the project is developed.
- g. Minimum separation between primary use buildings: 85-feet, excluding the hotel building's porte cochere.
- h. In order to accommodate an appropriate surface and structure parking design on a Property of limited size, to ensure the provision of an appropriate amount parking, and to limit the unnecessary use of materials that contribute to the urban heat island effect, the following standards shall apply:
 - i. Minimum drive aisle width of 24-feet.
 - ii. Minimum combined parking space and drive aisle width depth of 60-feet for double loaded aisles.
 - iii. Minimum combined parking space and drive aisle width depth of 41-feet for single loaded aisles.
 - iv. Off-street loading spaces: Minimum of one (1) loading space per building and minimum three (3) loading spaces total. A minimum of one (1) loading space per building shall meet the size requirements as stated in Section 702. Additional loading spaces must be a minimum size of 9.5 feet by 18 feet.
- i. Refuse enclosure locations shall be setback a minimum of 75-feet from the south property line.
- j. Vehicular access to and from 29th Street shall be prohibited.

2. LANDSCAPE STANDARDS.

a. General Requirements

- i. Where not in conflict with the development plan, preserve all viable large mature trees (4-inch caliper size and larger) in place on the Property. Includes trees in the right-of-way and along the street frontages of the development.
- ii. Materials listed on the Phoenix low-water use plant list shall be used within the right-of-ways. The selected plant material on the Arizona Department of Water Resources Phoenix AMA-3550 list shall contribute to the landscape design theme as discussed in the *Specific Plan* and *Conceptual Core Design Theme* along Camelback Road as much as possible.
- iii. If necessary to accommodate public utility easement conditions, alternative landscape design solutions (e.g. modified landscape materials, use of root ball barriers etc.) deemed consistent with a pedestrian environment and this PUD may be approved by Planning and Development Department staff.
- iv. A minimum 2’-5” radius shall be clear of hardscape around the base of the tree within the right-of-way.
- v. A minimum of fifty percent (50%) living vegetation ground coverage shall be provided within planting areas.
- vi. Plant materials to be used on the Property shall be based on the following PUD plant list below.

<u>Trees</u>	<u>Trees (cont.)</u>
Cercidium microphyllum	Fruitless Olive
Foothills Palo Verde	Olneya tesota
Cercidium praecox	Ironwood
Palo Brea	Prosopsis hybrid 'Phoenix'
Prosopsis juliflora	Phoenix Mesquite
Native Mesquite	Prosopsis hybrid
Sophora secundiflora	Hybrid Thornless Mesquite
Texas Mountain Laurel	Pistacia chinensis
Acacia willardiana	Chinese Pistache
Palo Blanco	Quercus virginiana
Bauhinia lunarioides	Southern Live Oak
Anacacho Orchid	Ulmus Parviflolia
Dahlbergia sissoo	Chinese elm
Sissoo Tree	Palms
Olea europaea “Swan Hill”	

<p><u>Accents</u> Agave sp. Aloe barbadensis 'Yellow' Yellow Blooming Aloe Asclepias subulata Desert Milkweed Diets bicolor Fortnight Lily Bouteloua curtipendula Side Oats Grama Dasyilirion wheeleri Desert Spoon Euphorbia antisyphilitica Candelilla Hesperaloe parviflora Red Yucca</p>	<p><u>Accents (cont.)</u> Hesperaloe funifera Giant Hesperaloe Muhlenbergia sp. Opuntia ficus indica Indian Fig Pachycerus marginatus Mexican Fence Post Pedilanthus microcarpus Slipper Plant Sanservieria trifasciata Mother-In-Law's Tongue Yucca rupicola Twisted Leaf Yucca Yucca sp.</p>
<p><u>Shrubs</u> Calliandra eriophylla Native Fairy Duster Cordia boissieri Texas Olive Dodonaea viscosa Hopbush Encelia farinosa Brittlebush Ericameria laricifolia Turpentine Bush Euphorbia rigida Gopher Plant Justicia californica Chuparosa Justicia spicigera Mexican Honeysuckle Simmondsia chinensis Jojoba Tecoma stans Yellow Bells Jasminum sambac Arabian Jasmine Lantana montevidensis Lantana</p>	<p><u>Shrubs (cont.)</u> Leucophyllum sp. Portulacaria afra Elephant's Food Ruellia brittoniana Ruellia Russelia equisetiformis Coral Fountain Viguiera deltoidea Goldeneye</p> <p><u>Groundcovers/Vines</u> Annuals Bougainvillea sp. Ficus pumila Creeping Fig Ipomoea batatas Sweet Potato Vine Parthenocissus quinquefolia Virginia Creeper Setcreasea pallida Purple Heart Wedelia trilobata Yellow Dot Zephyranthes candida White Rain Lily</p>

vii. General landscape guidelines.

- No vines or spreading ground cover plant material within 5-feet of curb or sidewalk.
- No shrubs with a mature height of 3-feet to be installed within 10-feet of a curb of the right-of-way.

b. Streetscape Requirements

i. Camelback Road

A) Double row of 3-inch (70% of required trees) and 4-inch (30% of required trees) caliper trees shall be planted along Camelback Road 25-feet on center or equivalent groupings to shade the sidewalk. The rows shall be placed parallel on either side of a detached sidewalk with a minimum five-foot wide landscape area located between the sidewalk and curb and when possible, the rows of trees shall be staggered, to provide for maximum shade.

- 1) If an outdoor patio area is located along Camelback Road which impedes providing a double staggered row of trees on the Property, then that area shall be exempt from providing trees on that one side. Relocating those required trees on the Property along Camelback Road or by the patio area to provide shade for the patio shall be required.

B) Camelback Road Theme Tree shall be used, which are: Native Mesquite, Medjool Date Palm, and Swanhill Olive. A similar alternative may be approved by Planning and Development Department staff.

- 2) The outer (street side) row of trees shall be exclusively of the “theme” variety; and the inner row shall use the theme tree to supplement existing trees along the sidewalk. The intention is not to remove existing, mature trees to create the “inner row” if such trees are located in the way of the formal street trees then use the “theme” trees to finish out the row and create a strong landscape transition.

C) Minimum of six (6) 5-gallon shrubs per tree.

ii. 28th Street

A) Trees planted 25-feet on center or equivalent groupings.

- 1) Minimum 2-inch caliper (40% of required).

- 2) Minimum 3-inch caliper (30% of required trees).
 - 3) Minimum 4-inch caliper (30% of required trees).
 - 4) Minimum of six (6), 5-gallon shrubs per tree.
- iii. 29th Street
- A) Large canopy evergreen shade trees planted 25-feet on center or equivalent groupings.
 - 1) Minimum 3-inch caliper (100% of required).
 - B) Minimum of six (6), 5-gallon shrubs per tree.
- iv. Perimeter Property Line Requirements (not adjacent to a street).
- A) Minimum 10-foot landscaped setback.
 - B) Large canopy evergreen shade trees planted 25-feet on center or equivalent groupings.
 - 1) Minimum 3-inch caliper (100% of required trees).
 - C) Minimum six (6) 5-gallon shrubs per tree.
- v. Adjacent to Building Requirements.
- A) Building facades within 100-feet of the public right-of-way or adjacent to public entries to the building. (Measured from inside face of curb to inside face of curb.)
 - 1) Minimum twenty-five percent (25%) of the exterior wall length shall be treated with either a landscaped planter a minimum 5-feet in width or an arcade or equivalent feature.
 - B) Trees planted 25-feet on center or equivalent groupings.
 - 1) Minimum 2-inch caliper (70% of required trees).
 - 2) Minimum 3-inch caliper (30% of required trees).
 - 3) Minimum six (6), 5-gallon shrubs per tree.
- vi. Parking Lot Area Requirements for Surface Parking Area.

- A) Interior surface area, exclusive of perimeter landscaping and all required setbacks, shall be a minimum ten percent (10%).
 - 1) Minimum interior dimension 5-feet (length and width).
- B) Landscaped planters at the ends of each row of parking and located approximately every 110-feet. Landscape planters shall not exceed the length of a standard city of Phoenix parking stall.
 - 1) Landscaped planters, single row of parking, shall be a minimum 120 sq. ft. as measured from the inside face of curb to inside face of curb and shall provide for one (1) tree.
 - 2) Landscaped planters, double row of parking, shall be a minimum 240 sq. ft. as measured from the inside face of curb to inside face of curb and shall provide for two (2) trees.
 - 3) Vehicles may overhang landscape areas or sidewalks by 30-inches.
- C) All other linear landscape areas shall be planted with trees 25-feet on center or equivalent groupings.
- D) Required trees.
 - 1) Minimum 2-inch caliper (70% of required trees).
 - 2) Minimum 3-inch caliper (30% of required trees).
- E) Required shrubs.
 - 1) Minimum six (6) 5-gallon shrubs per tree.

3. LIGHTING STANDARDS

Lighting will blend and accentuate the overall design theme along with providing a safe and inviting environment for the employees, visitors, travelers, and local residents.

- a. All site lighting shall conform to the city of Phoenix Zoning Ordinance.
- b. Special care will be taken with the surface parking lighting design and the canopy lighting to provide lighting levels that promote security, but will not spill over into the adjacent residences.
 - i. Lighting should be shielded with cut-off fixtures and deflectors to direct light downward and limit on-site lighting levels to a maximum of one 1-foot candle at the property lines.
 - ii. Parking canopy lighting shall be shielded from the adjacent residences.

- iii. Illuminative gases, LED lights and fiber optics should not be placed on the rear of structures facing the residential district.
- iv. Parking lot and security lighting shall not exceed a maximum of fifteen 15-feet in height including lamp, pole, and base within 110-feet of a residential district.
- v. All pole-mounted lighting within the parking area shall be provided with dimmers and motion sensors that are activated from 9:00 p.m. to sunrise.
- vi. LED lighting shall be used for parking lot and security lighting.

4. PARKING STANDARDS.

The proposed development, specifically the hotel and office uses proposed, provides for a symbiotic relationship due to the nature of these uses and today's younger generation of using alternative modes of transportation. The hotel use deals with travelers who may not have a car, utilize car service (e.g. Uber, etc.) or taxis, utilize transit (e.g. bus) or other modes of transportation (e.g. bicycling). Moreover, the office use is geared towards the Creative Class and corporations that view more urban, mixed-use, and multi-modal transportation areas as ideal for a healthy/balanced work environment that many younger, as well as older, people are looking for in jobs and job relocations. Thus, the office development will balance and adjust parking needs based on this new paradigm of commuting. More importantly, the hotel's busiest activity (i.e. check-in/check-out) typically occurs after or before "peak" office hours, typically 8:00 AM to 5:00 PM. Tenants of the office building will also be encouraged to participate in a transportation management association that sponsors trip reductions.

The Conceptual Site Plan has been designed to support and enhance the ease of pedestrian movement with walkways and dense landscaping, designated carpool parking areas, and transit ridership with an enhanced designed bus shelter. The adjacent residential areas and the abutting Camelback East Village Primary Core also provides for opportunities to live, work, and play within close proximity, which benefits the *28th Street & Camelback PUD* towards fulfilling these ideals.

With that being said, because of the nature of the existing or proposed use(s) and the overall site design (i.e. internal capture) along with the existence/enhancement of pedestrian movement, transit, proximity to the Village core, proposed onsite underground parking garage, valet parking, and existing uses within the overall area adherence to strict parking requirements specified by Section 702 of the Zoning Ordinance is not necessary. A nominal reduction in parking will not increase on-street parking in adjacent neighborhoods, but will benefit the area by not creating more surface parking (i.e. heat island), but more openness. More importantly, an overconstructed and unused/bland parking garage provides no benefit.

The following shall apply to the *28th Street & Camelback PUD*.

- a) Pursuant to Section 702.E.2.c., a parking reduction of up to fifteen percent (15%) based on a shared parking model may be granted by the Planning and Development Department Traffic Engineer.
 - i. Must be approved under a combined site plan for the Property.
 - ii. An applicant may petition the Planning and Development Traffic Engineer for review of parking situations which do not correlate with one of the standard uses in the model.
 - iii. The Planning and Development Director or his designee may periodically modify the shared parking model to improve it as a predictor of parking demand based on national or local research, including site observations.
- b) Pursuant to Section 702.E.2.c., a parking reduction greater than fifteen percent (15%) based on a shared parking model and due to the nature of the existing or proposed use(s) or the existence of pedestrian, mass transit, or service trips may be allowed subject to the following:
 - i. Must obtain a use permit in accordance with the standards and procedures of Section 307 of the city of Phoenix's Zoning Ordinance.
 - ii. Must be approved under a combined site plan for the Property.
 - iii. An applicant may petition the Planning and Development Traffic Engineer for review of parking situations which do not correlate with one of the standard uses in the model.
 - iv. Predictor(s) of parking demand based on national or local research, including site observations, shall be provided.
 - v. A total of five percent (5%) of the parking spaces shall be designated and marked carpool parking spaces.
 - vi. Valet parking may be provided for the hotel or restaurant uses.
- c) Bicycle parking.
 - i. Provide a minimum total of twenty (20) bicycle parking spaces accessible to each of the buildings on the Property.
 - ii. Provide a minimum total of five (5) bicycle storage lockers accessible to each of the buildings on the Property or within the below grade parking structure.

- iii. Bicycle parking required may be placed within the street frontage setback(s) and may be allowed within the right-of-way subject to review by the Planning and Development Department Traffic Engineer.
- iv. Bicycle parking should comply with ADA requirements and not impede on-site pedestrian access. A clearance of at least 4-feet in width must be provided for pedestrian access.
- v. Bike racks and/or storage areas shall be located within 50-feet of building entry points.
- vi. A bicycle rack should be a minimum of 30-inches from a wall or other obstruction.
- vii. The minimum length for a bicycle parking space shall be 72-inches.

H. DESIGN GUIDELINES

Unless specifically modified herein, the *28th Street & Camelback PUD* shall conform to the city of Phoenix Zoning Ordinance Design Guidelines, *Section 507, Tab A*.

1. No building setback provision required. The buildings shall front along Camelback Road with a building/landscape setbacks that will provide for interesting architectural projections, massing, and materials. In addition, there will be the potential to have patio areas and dense landscaping along the base of the buildings to provide both visual and pedestrian scaled interest.
2. As mentioned, the *28th Street & Camelback PUD* has provided for many of the design elements associated within the *Camelback East Primary Core Specific Plan's* Design Guidelines even though the Property does not fall within the this plan's designation. A comparison chart has been provided as an exhibit to this PUD book which outlines those design elements and how they are being met within the PUD's proposed design. (See **Exhibit M-12**; Comparison Chart *Camelback East Primary Core Specific Plan's* Design Guidelines vs the *28th Street & Camelback PUD's* Design Guidelines)
3. Specific to the *28th Street & Camelback PUD* are the following design guidelines.
 - a. Plants within the right-of-way shall be from the ADWR plant list, along with utilizing landscape design theme intent and characteristics associated with the *Camelback East Primary Core Specific Plan's* Design Guidelines. Trees are to be arranged in a more formal triangulated spacing along with shrub massing with a more evergreen feel that reflects the character of the area.
 - b. Where possible, the use of "Old Phoenix" or "Arizona Biltmore" plants as discussed within the *Camelback East Primary Core Specific Plan's* Design Guidelines or plants with similar characteristics pursuant to the plant list associated with this PUD shall be

used. Additional plants on the PUD plant list include drought tolerant evergreen shrubs and ground covers to be used in bold mass plantings to reflect the historic character, along with a balance of striking texture and flower color. Annuals for seasonal color shall be provided, where appropriate, in architectural pots/planters within pedestrian areas.

- c. A water feature shall be within the shared central courtyard between the buildings near the entry points or the proposed “grove” area as identified on the landscape plans. (See **Exhibit M-6**; Conceptual Landscape Plan) Incorporating art (e.g. sculpture, decorative paving, decorative trellis, etc.) within the central courtyard may be utilized in replace of the water feature.
- d. The proposed design shall include shade trellises, trees, vertical landscaping, benches, tables/chairs, and a porte cochere associated with the hotel building within a shared central courtyard or “grove” area as identified on the on the landscape plans (See **Exhibit M-6**; Conceptual Landscape Plan) between the two (2) buildings.
- e. A large courtyard at the center of the Property shall link the two (2) buildings' main entries. The area shall be connected by pedestrian walkways that shall be at a consistent floor finish elevation for greater continuity and connectedness.
- f. A detached public sidewalk, except by the existing bus stop, shall be installed along Camelback Road and shall access the Property's main pedestrian core, which shall lead to the front door of the hotel and office buildings.
- g. The hotel lobby shall be connected to the office lobby by way of a continuous pedestrian walkway including the amenities listed (e.g. benches, trellis, etc.). There shall be no grade change along this continuous path, and vehicular drive/drop-off shall be ramped to meet this floor elevation.
- h. Pedestrian access between the neighborhood and the building's entrances shall be safely and conveniently provided by a continuous sidewalk around the Property's three (3) sides. A continuous sidewalk along 28th Street, Camelback Road, and 29th Street shall be provided to facilitate pedestrian connectivity to the Property. These sidewalks shall connect with building entry points on the Property's north side.
- i. The proposed design includes a surface parking lot that is conveniently located and visually unobtrusive. The following elements below shall be included with the final surface parking lot design, they are:
 - (1) The surface parking lot shall be located at the back (south) side of the Property, which will effectively screen it from view from Camelback Road by the office and hotel buildings. The courtyard between the buildings and the Property boundaries (i.e. 28th and 29th Streets) shall also provide landscaping and trees for screening of the surface parking lot.

- (2) All surface parking spaces shall be within 150-feet of sidewalks leading to the buildings entrances. Designated walkways, drive aisles excluded, shall be shaded by trees, parking canopies, or trellises.
 - (3) The surface level parking lot, exclusive of perimeter landscape setbacks and sidewalks, shall be a maximum depth of 100-feet.
 - (4) The proposed design shall include a minimum of fifteen percent (15%) or greater landscaping in the surface parking lot through landscape islands, perimeter trees, and groundcovers.
- j. With the exception of the surface parking lot, the office and hotel buildings required parking shall be assigned to the below grade parking structure. The number of below grade parking spaces for the uses shall be at a minimum eighty percent (80%) of the required parking.
 - k. The parking structure for the Property shall be entirely below grade, so it is visually unobtrusive. Ingress and egress ramps shall be located at the far western property line for convenient access to 28th Street and Camelback Road.
 - l. The proposed buildings shall establish a strong and relatively continuous building frontage along the Camelback Road street frontage.
 - m. A minimum thirty percent (30%) glazing shall be provided along Camelback Road and 28th Street.
 - n. Limit openings (e.g. windows/balconies) facing the residential homes to the south. No balconies facing residential homes shall be provided. Any south facing hotel building walls within 150-feet of the southern property line shall not contain guestroom windows. (See **Exhibit M-5**; Conceptual Elevations)
 - o. Occupiable outdoor space above the 2nd floor shall be limited to only the east and west elevations of the office building and only the north elevation of the hotel building. (See **Exhibit M-5**; Conceptual Elevations)
 - p. The base of the building shall be differentiated from the remainder of the building with floor to ceiling glazing that shall be set back behind a ground level colonnade. There shall also be a material change from the first level ceiling height and up.
 - q. Loading areas shall be designed to access the building from the south and shall not face public streets.
 - r. Refuse areas shall be screened by a minimum 6-foot high solid wall with gates to appear integral with the building design.
 - s. Mechanical equipment shall be roof mounted with architectural screening from public view.

- t. East facing hotel room windows shall provide louvers or other window screening devices to provide enhanced privacy for nearby residences. The exterior louvers or other window screening devices shall extend a minimum of 24-inches from the face of the building wall as approved by the Planning and Development Department.
- u. Non-reflective glass shall be provided for all south, west, and east facing windows.
- v. An 8-foot high solid masonry wall shall be provided along the southern property line.
- w. A 6-foot high solid masonry wall shall be provided along the eastern property line for the southern 110-feet of the property. The wall shall be located outside of the required landscape setback and serve to screen parking, loading, refuse, and public areas from the adjacent right-of-way.
- x. Any parapets shall be limited to a maximum height of 8-feet for the western building and 6-feet for the eastern building.

I. SIGNAGE

The main objective of the signage program is to allow for adequate identification and wayfinding signage. The *28th Street & Camelback PUD* will create the avenue to encourage the creative development of signage that is vibrant, charismatic and potentially unique in its approach. A general framework is provided to ensure that the designs are appropriate for the project as a whole. Signage should not only include text that identifies the name of the business, but also incorporates the use of graphics and forms that provide identity, as appropriate, for users. Included in this PUD are a Conceptual Sign Plan and Conceptual Elevations identifying signage locations. (See **Exhibit M-13**; Conceptual Sign Plan)

Exterior site signs shall generally conform to the requirements of Section 705 of the Phoenix Zoning Ordinance, except where modified by the PUD regulations contained herein. Signs that are oriented interior to the site will not be regulated by the staff, unless otherwise specified herein, except to the extent that electrical permits may be required by the city of Phoenix. All signs shall be regulated by the PUD guidelines for signs under the sole discretion of the property owner(s) and the construction rules as adopted by the city of Phoenix.

This section shall not apply to signs erected or maintained by a governmental body including, but not limited to, traffic signs, warning signs, railroad crossing signs, and signs of a noncommercial nature required by public laws, Ordinances, or Statutes.

Finally, the signage has been designed with the best and highest standards possible in mind while also considering some of the *Camelback East Primary Core Specific Plan's* Design Guidelines, even though the Property falls just outside the boundaries of the *Specific Plan*. However, the Design Guidelines below have been utilized to provide a visually appealing sign plan and to blend with the area.

Design Guidelines from the Specific Plan.

1. Character (Section D.4.1 of the *Specific Plan*).
 - A. (Section D.4.1.1 of the *Specific Plan*) Quality, durable materials, compatible with site and building design, should be incorporated in the sign design and construction. Materials such as masonry, concrete, stainless steel, natural rock or stone should be incorporated. Unless compatible with the building design, visible aluminum should be used on a minimal basis. Internally illuminated panels are discouraged. Routed or individual letters are preferred. There shall be consistency in the quality and durability of materials between the panels used on the sign.

2. Location (Section D.4.2 of the *Specific Plan*).
 - A. (Section D.4.2.1 of the *Specific Plan*) Where a sign is blocked by an abutting building a building identification sign shall be placed at the lowest possible height that will provide visibility from the abutting arterial or collector street, while the street address numerals, if used, may be placed no higher than the story immediately above the building identification sign.

Rationale: Locating an office building, hotel or residential building so as to abut the pedestrian spine should not be discouraged by a lack of visibility of the building's identification signs from adjacent arterial and collector streets.

- B. (Section D.4.2.2 of the *Specific Plan*) Retail buildings shall have no wall signs above a height of 56-feet.

Rationale: The entire facades of the upper stories of retail buildings are usually windowless and therefore can accommodate wall signs without creating visual conflict with the architectural elements of the facades.

- C. (Section D.4.2.3 of the *Specific Plan*) The maximum sign height of a ground sign in the Core allowable after design review and/or by a comprehensive sign plan shall be twenty (20) feet.

Rationale: Monument signs of greater height are excessively high in the intensively developed and integrated automobile and pedestrian environments of the Core, and are out of character with the heights of existing monument signs for the largest buildings and building complexes in the Core.

- D. (Section D.4.2.4 of the *Specific Plan*) Multi-family residential buildings, hotels and parking structure shall have wall signs no higher than the top of the second story. Hotels may have wall signs up to a height of 56-feet.

Rationale: Multi-family residential, hotels and parking structures appear cluttered when the patterns of openings are disrupted with signage. Placing signs above the

second story of a multi-family residential building, hotel or parking structure converts the building's image from that of a suburban core building to that of a downtown core building.

The above guidelines from the *Specific Plan* have been incorporated into the sign proposal for the PUD. This includes the proposed use of durable materials (e.g. masonry, stainless steel, etc.) to match the building architecture and site design. In addition, the proposed monument sign for the office building at the immediate corner of 28th Street and Camelback Road will have routed-out copy and both proposed monument signs will be less than 20-feet in height. Finally, signage will be placed on the lowest visible portion of the proposed office building, which is the 4th floor level due to the provision of providing significant trees and their spacing to provide shade and a pleasant pedestrian/ground floor environment. Thus, these trees will block the 2nd and a portion of the 3rd floor levels. Moreover, the hotel is allowed to have wall signs up to a height of 56-feet.

1. ALLOWABLE SIGNS

These provisions are meant to ensure a consistency in signage for the uses and tenants' unique identities, as well as protect the surrounding neighborhoods by maintaining the suburban residential feel of the area south and mid-urban feel along Camelback Road.

An amendment to this PUD shall not be required for signage that would otherwise be approved by the city of Phoenix through a use permit, comprehensive sign plan, etc.

The following signs, as well as signs otherwise permitted by the city of Phoenix pursuant to Section 705, shall be permitted with the exception of animated or electronic message display signs. Any automated teller machine signage shall be limited to west property line and western 300-feet of the north property line frontages.

GROUND SIGNAGE TYPES

a. Ground Signs (Primary Identification/Monument)

- i. There shall be two (2) monument signs, one (1) east of the shared entrance along Camelback Road and the other at the corner of 28th Street and Camelback Road.
- ii. These signs shall contain no more than ten (10) items of information, and may identify the hotel, office building and tenants within.
- iii. Height shall be limited up to a maximum of 20-feet in height.
- iv. Sign area shall be limited up to a maximum of one-hundred and fifty (150) square feet in area.
- v. Monuments signs may be free form, spherical, sculptural or non-planar. Such signs are allowed twice the amount of designated sign area, where as sign area is calculated as fifty percent (50%) of the sum of the area of the four (4) vertical sides of the smallest cube that will encompass the sign.

Note: This calculation gives the same amount of sign area as a double-faced monument sign.

- vi. Where applicable, ground signs shall adhere to site visibility triangle requirements.

b. Wayfinding/Directional/Directory Signs

Wayfinding and directional/directory signage shall be seamlessly integrated with the overall project identity signage design, using colors and materials that are appropriate for their signage type and use.

- i. There shall be no more than two (2) directional signs per driveway entrance. There shall be no limit on the number of directional signs interior to a site.
- ii. Height shall be limited up to a maximum of 4-feet in height.
- iii. Sign area shall be limited up to a maximum of eight (8) square feet in area.
- iv. Where applicable, wayfinding/directional signs shall adhere to site visibility triangle requirements.

BUILDING(S) SIGNAGE

All signage types allowed within Section 705 of the Phoenix Zoning Ordinance, plus those outlined below are allowed for tenant identification, to complement the architectural design and specific office and hotel use. Any illuminated building identification signs above the 2nd floor of each building shall be limited to the north side of the building. One (1) identification sign shall be permitted on the west side of each building within 100-feet of the north property line. The identification sign on the west side of the eastern building may be illuminated, but shall be shielded to prevent visibility from the south. No building mounted signs shall be permitted on the south side of any building.

a. Hotel Building Wall Identification

For the hotel building's wall identification signage at the upper level, signage shall be allowed by right based on the following criteria:

- i. A wall sign shall be placed in the top ten percent (10%) of the wall to which it is attached and shall not exceed eighty percent (80%) of the width of the building face to which it is attached. (See **Exhibit M-13**; Conceptual Sign Plan – Specifically the hotel building elevations illustrating the above signage locations.)

- ii. Signs shall be limited to a maximum of three (3) per building and a maximum of one (1) sign per building elevation. Signage shall be restricted to identification of the building/user.
- iii. Signs shall be reverse pan channel letters and may have white reverse or halo illumination.
- iv. No hotel building wall signage shall be permitted above 56-feet in height.

b. Building Wall Identification (Below 56-feet)

The aggregate square footage for wall signage shall be determined by one (1) and one-half (1½) square foot per one (1) lineal foot of building frontage, per elevation.

Building ID: C2.01. (See Exhibit M-13; Conceptual Sign Plan regarding building ID C2.01's locations.)

Hotel/office building identification signs shall be designed to fit the building elevation architecture at the street/pedestrian level, and overall environment in scale, placement and style. Designs shall complement the building color and finishes and provide contrast for readability, and the project's level of quality.

- i. The hotel user shall be allowed to use their corporate graphic standards in logo/letter style, color and design proportions.

Canopy Signs: C2.02. (See Exhibit M-13; Conceptual Sign Plan regarding building ID C2.02's locations.)

Signs on canopies shall be considered wall signs and shall be subject to the requirements established for wall signs. However, lettering which does not exceed seven (7) inches in height and is displayed on the edge of a canopy or awning hanging perpendicular to the ground shall not be counted against the allowable signable area of a wall frontage.

- i. No portion of any canopy or awning shall be less than 8-feet above the level of the sidewalk or other surface over which it projects. Where there is vehicle traffic, no portion of any canopy or awning shall be less than 10-feet above the vehicle maneuvering area.
- ii. Canopies (rigid surfaces) may have hanging, surface mounted, top mounted or projecting letters off all surfaces of the structure.
- iii. Signs may be illuminated indirectly or internally.

Landscape/screen wall: C2.03. (See Exhibit M-13; Conceptual Sign Plan regarding Landscape/screen wall C2.03's locations.)

A landscape/screen wall may be freestanding or erected against, and/or a continuation of the building.

- i. No more than one (1) sign shall be permitted to be attached to a landscape/screen wall and shall be restricted to identification of the hotel and office building. The sign area shall cover no more than twenty-five percent (25%) of the area of the screen wall.
- ii. Landscape/screen wall signage shall be counted as wall signage.
- iii. Hotel identification signs may be located on landscape/screen walls within the Property boundaries and shall be considered on-site signage, even though it is not on a direct wall of the hotel building.
- iv. Signs may be illuminated indirectly or internally.

c. Office/Commercial Tenant Building Signage

Up to six (6) major tenants may be identified on the building. Four (4) on the north side of the building facing Camelback Road, one (1) on the east side of the building and one on the west elevation of the building. Major tenants shall be determined by the property owner(s) and/or its assignee(s).

- i. The aggregate square footage of all business/tenant signage shall not exceed one (1) and one-half (1½) square foot per one (1) lineal foot of building frontage, per elevation.
- ii. Sign placement shall be restricted to the designated sign envelope of the building and shall be no higher than the top of the 4th story. In order to provide proper identification and visibility, tenant wall signs may be placed on walls of the building not adjacent to a leased space.

Tenant signage shall meet the following requirements:

1. Letter height shall be a maximum of 2-feet high.
2. Minimum allowed signage shall be fifty (50) square feet.
3. Tenant signs shall maintain a minimum of 6-foot spacing in-between all other tenant signs.
4. Tenants shall be allowed to use their corporate graphic standards in logo/letter style, color and design proportions.
5. Signs shall be reverse pan channel letters and may have white reverse or halo illumination.

d. Window Signage/Graphics

Window signs shall not exceed thirty percent (30%) of each window area located on the ground floor of a building. Window areas separated by muntins or mullions shall be considered as one continuous windowpane. Window signage shall not be assessed as wall signs. No window signs shall be permitted for south and east facing windows.

TEMPORARY SIGNAGE

a. Banners

Standards - All signs for grand openings or temporary business identification while awaiting permanent signage shall be subject to the following standards and requirements:

- i. Banners shall be attached to a solid structure in a secure manner.
- ii. Banners shall have a minimum clearance of 8-feet above grade when placed above an area open for the common or general use of the public.
- iii. Banners shall be vented as required to ensure they will withstand wind pressure from any direction applied to the projected exposed area.
- iv. Banners shall not project above the roofline.

b. Temporary Events

- i. Such signs shall be erected no more than two (2) days prior to the event and shall be removed no more than one (1) day after the event.
- ii. Total duration of use for such signs shall not exceed ten (10) days.
- iii. Such signs are limited to no more than four (4) events at any one (1) single building in a calendar year.

c. Grand Openings/Business Identification

- i. Temporary business identification is limited to one (1) sign per street front.
- ii. Such signs shall be erected for a period not to exceed thirty (30) days.
- iii. Permits for grand openings shall be issued only if a valid building permit for construction or alteration of the building or suite for that location has been issued, or a valid application for a certificate of occupancy has been made for the address in question.

d. Flags

- i. Flags which do not require a sign permit which may include, but is not limited to, the following: flags of a nation, state, county, city, political entity or religious institution. However, the display of more than two (2) flags of this or any other nation, state, county, city or other political entity or any church or religious organization shall not be permitted.
- ii. One corporate flag per building.

e. Construction, property sale, lease or rental signs

Construction and property sale, lease or rental signs are permitted subject to the requirements listed in Table D-2 of the city of Phoenix Sign Ordinance.

- i. Information and safety signage as required by the City and or contractor.
- ii. Directional and information signage.
- iii. Developer/Contractor trailer graphics.
- iv. Construction barricade/fencing signs.
- v. Onsite identification, phone number and information of the developer/contractor.

J. SUSTAINABILITY

There are many ways to define sustainability, but the simplest and most fundamental is the ability to "sustain" oneself or project or the "the capacity to endure." The proposed *28th Street & Camelback PUD* and the development propose to do just that: to "endure" for many years as a premier, high-quality, and high-class development that will stand the "test" of time. That is why LaPour is committed to the "Leadership in Energy and Environmental Design" (LEED) Green Building Rating System is presently a voluntary standard for developing high-performance sustainable buildings. LaPour proposes to develop the Property with the highest LEED certified rating(s) achievable. Why LEED?

- LEED is transparent.
- LEED saves time and resources by providing a comprehensive set of tools for use.
- LEED provides a tool for quantifying and benchmarking green building program outcomes.
- LEED is flexible.
- LEED is revised every two (2) years to continually improve performance thresholds in pace with changes in building technologies and markets.

To that end, the 28th Street & Camelback PUD already encourages many sustainability principles throughout. However, at a minimum, the implementation the following additional sustainability principles below shall be required, with italicized items being enforceable by the city.

- The provision of recycling collection area(s) onsite shall be identified on the site plan at the time of the site plan review process.
- *Shade a minimum of seventy-five (75%) of the public sidewalks, excluding driveways, with trees or other means.* Other means of shading may include, but are not all inclusive, the buildings themselves or shade structures. Shade structures may be within the right-of-way only upon issuance of an encroachment permit as administered by the city of Phoenix, and if applicable, approval from affected public utility companies by the city. Compliance with the percentage shading shall be measured at noon (12:00 p.m.) of the June/Summer Solstice and based on the tree's maturity.
- Subject to the approval of Public Transit Department staff, provide an upgraded, architecturally distinguishable bus shelter that blends with the development and *Conceptual Core Design Theme* shall be provided.
- Design for effective water usage and conservation methods by using low flow plumbing fixtures using minimal amounts of potable water.
- Design for the capture of gray water discharge from buildings for reuse for landscape irrigation.
- Incorporate “Smart” irrigation control systems into the landscape design
- *Use roofing materials that specify a high Solar Reflectance Index for a minimum of seventy-five percent (75%) of the roof surface area; including parking canopies.*
- *Shade a minimum of fifty percent (50%) of the surface parking spaces using vegetation, low sloped structural shading with a minimum SRI of 29, or a similar material that achieves additional shading with high reflectivity or through the use of shade structures.* Excludes driveways and aisles.
- *Provide a minimum of twenty percent (20%) of permeable paving, structured soil/grass, open grid paving or similar material which allows for water to permeate the surface to promote natural drainage and filtration.* The calculation shall include on-site pedestrian walkways, private surface driveways and parking stalls. Shall not include loading areas. Alternative pavement surfaces require approval by the Zoning Administrator.

K. INFRASTRUCTURE

The 28th Street & Camelback PUD is a redevelopment project, which creates an opportunity to tap into the existing built infrastructure while spurring further economic activity closer to the city's urbanized areas or in this case the Camelback East Village Primary Core. This type of development is also encouraged by the city of Phoenix, because the redevelopment of property within developed areas reduces the amount and distance that people travel in their cars; utilizes what is already built; provides and encourages better access to public transit or multimodal forms of transportation (e.g. walking, biking, and bus); improves air quality; greater energy efficiency, and can even help reduce the "heat-island" effect. Finally, by increasing the mix and diversity of uses within walking distance of residential homes as well as employment areas helps in the creation of stable, healthy, and complete neighborhoods.

1. Circulation System (Traffic).

The 28th Street & Camelback PUD has great access from a major arterial street (i.e. Camelback Road) as well as a collector street (i.e. 28th Street) with a signalized intersection at 28th Street and Camelback Road. Moreover, the Property is well served by an internal drive aisle, which divides the property providing efficient internal circulation. Any necessary street dedications, improvements, which include pavement, driveway curb cuts, attached or detached sidewalks, landscaping, and streetlights adjacent to this Property will be reviewed/discussed during the rezoning and site planning processes.

A traffic impact study was completed by Southwest Traffic Engineering, LLC to establish an understanding of the current and potential future traffic as development occurs. (See **Exhibit M-14**) The study determined the existing streets are sufficient to accommodate the level of traffic proposed.

2. Grading and Drainage.

Water retention for the development may be provided on individual properties and/or within common retention areas servicing the overall development. Necessary legal agreements will be required if utilizing common retention areas, if split into two (2) lots. Retention areas may be either by surface basins or underground storage and will be determined at the time of building permit submittals.

3. Water and Sewer Service.

Development will be adequately served by the existing water and sewer systems. Infrastructure capacity requirements will be provided during the site plan review process and will comply with city of Phoenix Water and Sewer Design Standards, Codes and Policies.

L. PHASING PLAN

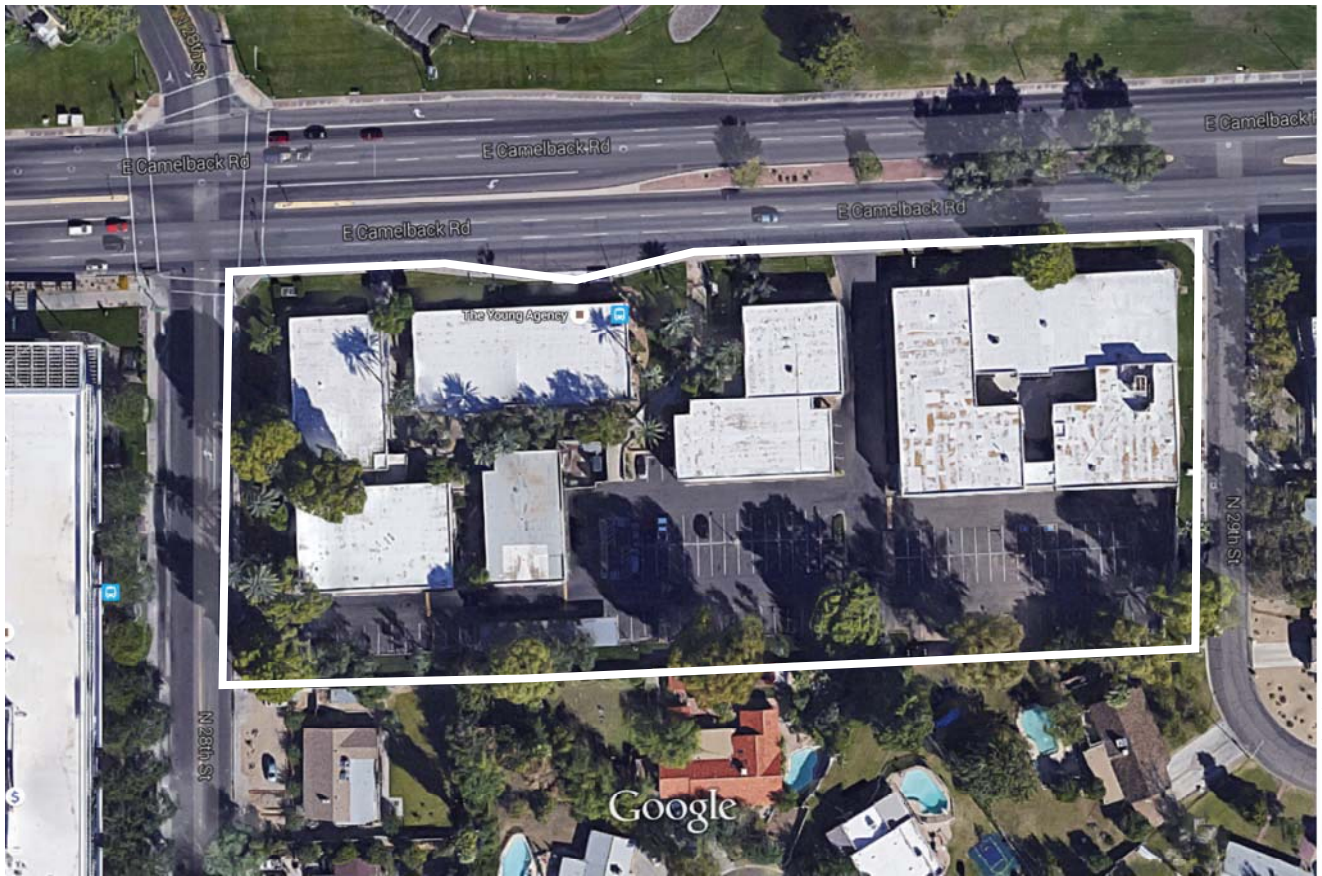
Final site plan(s) for an individual development project or for the entire Property will be processed in the future through the city of Phoenix's site plan development review process. Any necessary phasing of infrastructure improvements, landscaping or development shall be determined and shown at the time of the site plan development review process.

M. EXHIBITS

1. Context Aerial Map (**page 56**)
2. Context Plan and Photos (**page 58**)
3. Conceptual Site Plan (**page 69**)
4. Legal Description (**page 74**)
5. Conceptual Elevations (**page 76**)
6. Conceptual Landscape Plan (**page 85**)
7. Camelback East Village Conceptual Core Design Theme (**page 99**)
8. General Plan Map (**page 116**)
9. Camelback East Village Specific Core Plan Map (**page 118**)
10. Zoning Maps (Existing and Proposed) (**page 120**)
11. Comparative Zoning Standards Table (**page 122**)
12. Comparison Chart Camelback East Primary Core Specific Plan's Design Guidelines vs the 28th Street & Camelback PUD's Design Guidelines (**page 125**)
13. Conceptual Sign Plan (**page 130**)
14. Traffic Impact Analysis (**page 138**)

M-1

Context Aerial Map



Imagery ©2015 Google, Map data ©2015 Google 50 ft

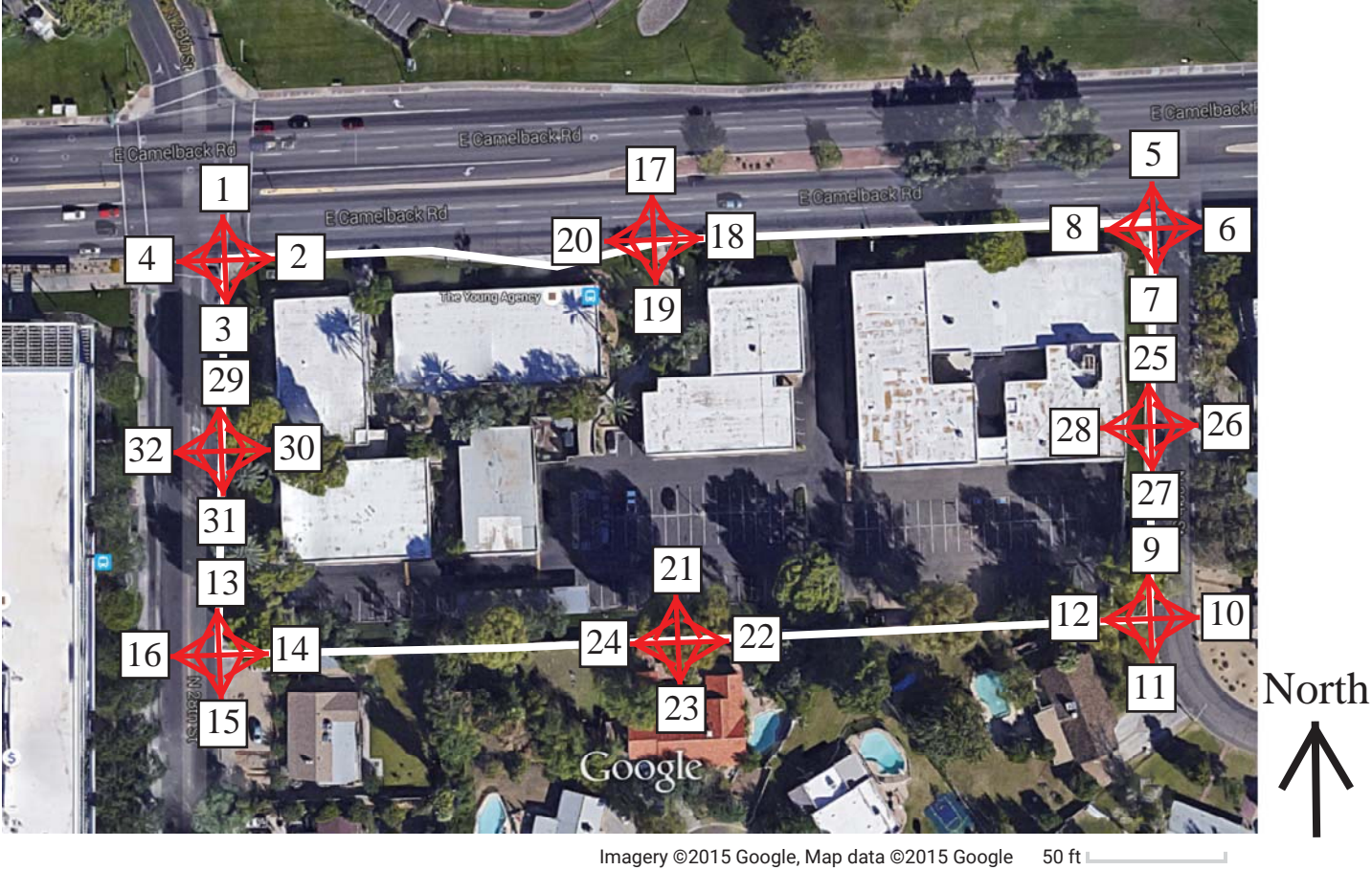


Parcels: 163-14-054A & 163-14-067D

M-2

Context Plan and Photos
for
28th St and Camelback

Context Aerial Map



Parcels: 163-14-054A & 163-14-067D

Photo 1



Photo 2



Photo 3



Photo 4



Photo 5



Photo 6



Photo 7



Photo 8



Photo 9



Photo 10



Photo 11



Photo 12



Photo 13



Photo 14



Photo 15



Photo 16

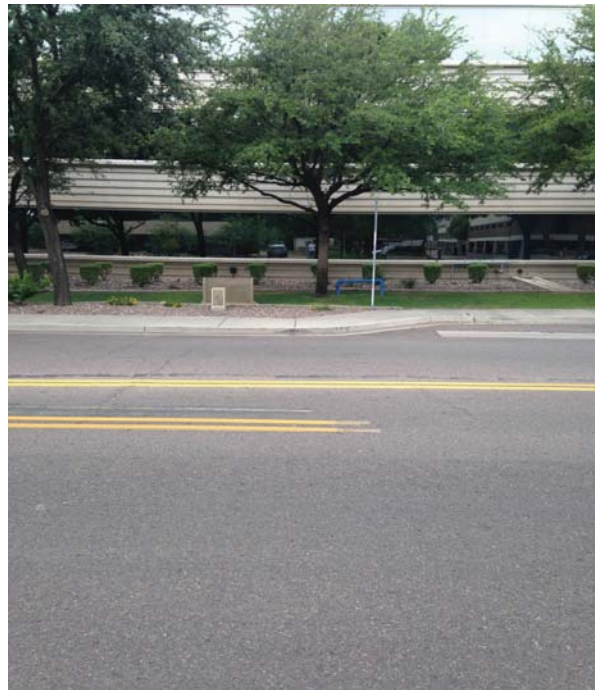


Photo 17



Photo 18



Photo 19



Photo 20



Photo 21



Photo 22



Photo 23



Photo 24



Photo 25



Photo 26



Photo 27



Photo 28



Photo 29



Photo 30



Photo 31



Photo 32



M-3

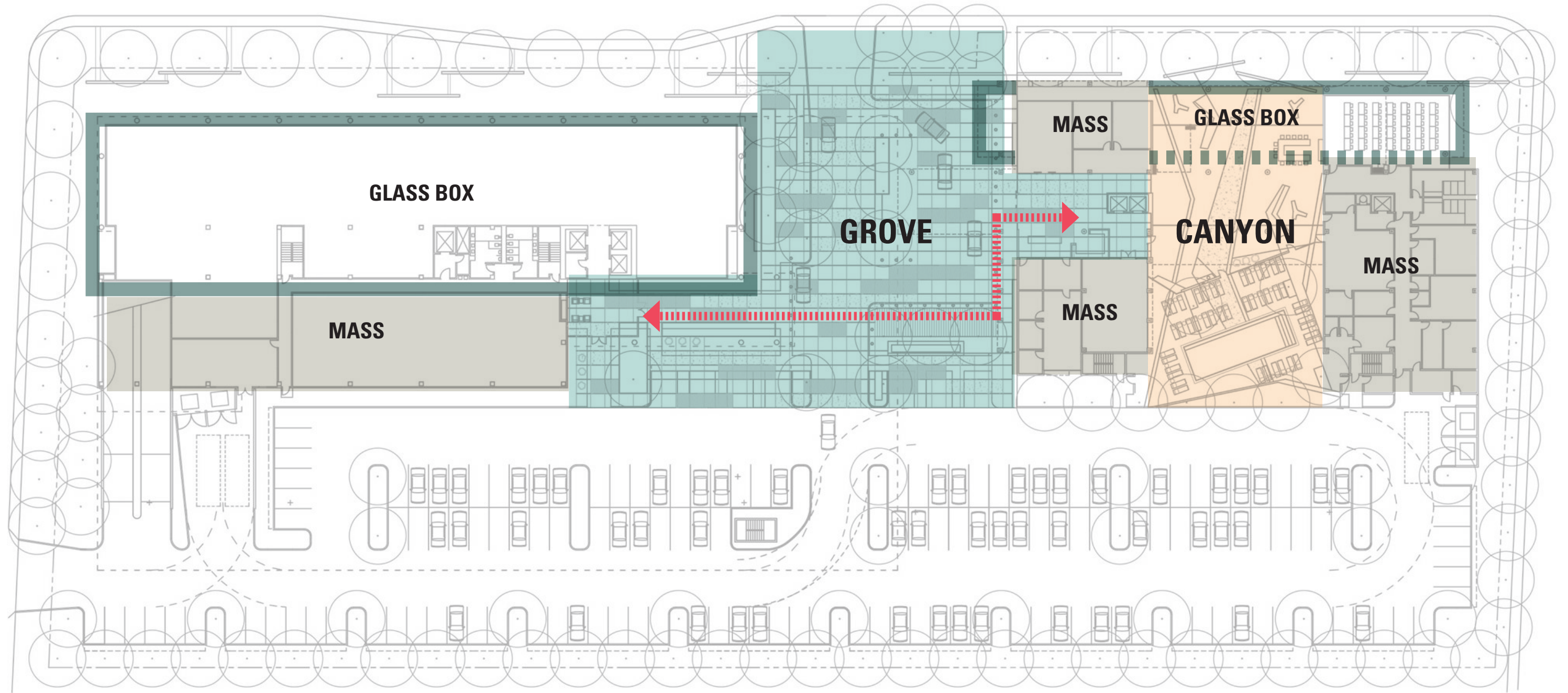


GROVE



CANYON

CONCEPT DIAGRAM



M-4

LEGAL DESCRIPTION
2801/2845 E. CAMELBACK ROAD

TRACT 'A' AND LOTS 7 THROUGH 9 OF GIBRALTER ESTATES PLAT 4, ACCORDING TO THE PLAT OF RECORD IN THE OFFICE OF THE COUNTY RECORDER OF MARICOPA COUNTY, ARIZONA AND A PORTION OF LAND LOCATED IN THE NORTHEAST QUARTER OF SECTION 23, TOWNSHIP 2 NORTH, RANGE 3 EAST OF THE GILA AND SALT RIVER MERIDIAN, MARICOPA COUNTY, ARIZONA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A FOUND BRASS CAP IN A HANDHOLE AT THE NORTH QUARTER CORNER OF SAID SECTION 23;

THENCE NORTH 87 DEGREES 30 MINUTES 00 SECONDS EAST, ALONG THE NORTH LINE OF SAID SECTION 23, A DISTANCE OF 675.51 FEET TO A FOUND BRASS CAP FLUSH AT THE INTERSECTION OF 29TH STREET;

THENCE SOUTH 00 DEGREES 00 MINUTES 32 SECONDS EAST, ALONG THE MONUMENT LINE OF SAID 29TH STREET, A DISTANCE OF 308.04 FEET;

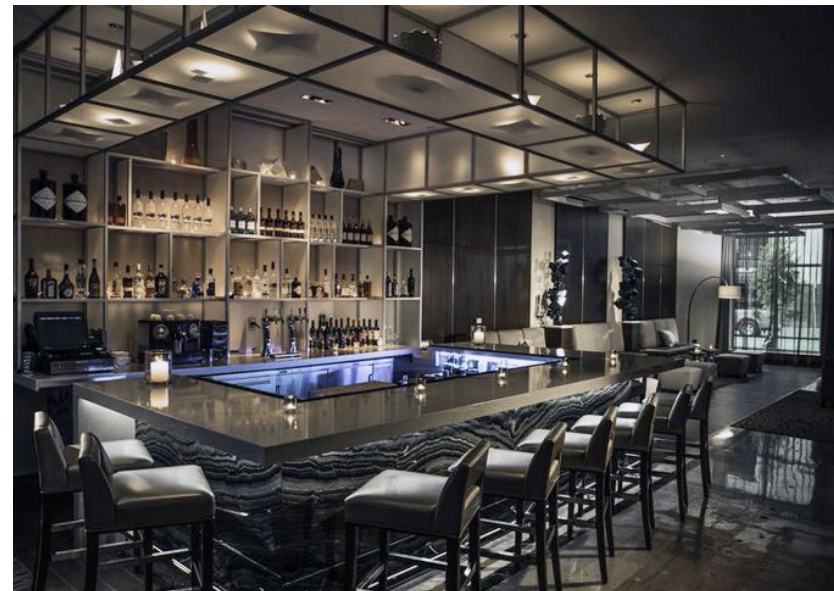
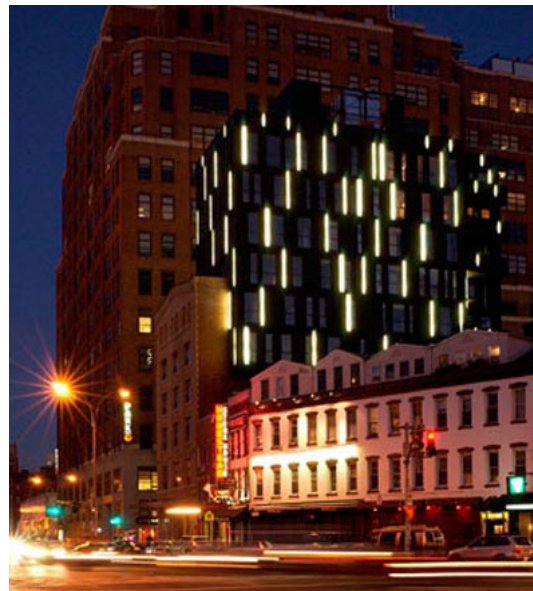
THENCE SOUTH 87 DEGREES 30 MINUTES 00 SECONDS WEST, LEAVING SAID MONUMENT LINE, A DISTANCE OF 675.54 FEET TO A POINT ON THE MONUMENT LINE OF 28TH STREET;

THENCE NORTH 00 DEGREES 00 MINUTES 16 SECONDS WEST, ALONG SAID MONUMENT LINE, A DISTANCE OF 308.04 FEET TO THE **POINT OF BEGINNING** OF THE PARCEL HEREIN DESCRIBED.

SAID PARCEL CONTAINS 207,889 SQUARE FEET OR 4.772 ACRES, MORE OR LESS.

M-5

ARCHITECTURAL PALETTE







OFFICE SOUTHEAST CORNER



LAPOUR

CAMELBACK MIXED USE
OCTOBER 13, 2015

TRUEFORM RSP
landscape architecture studio

M-6

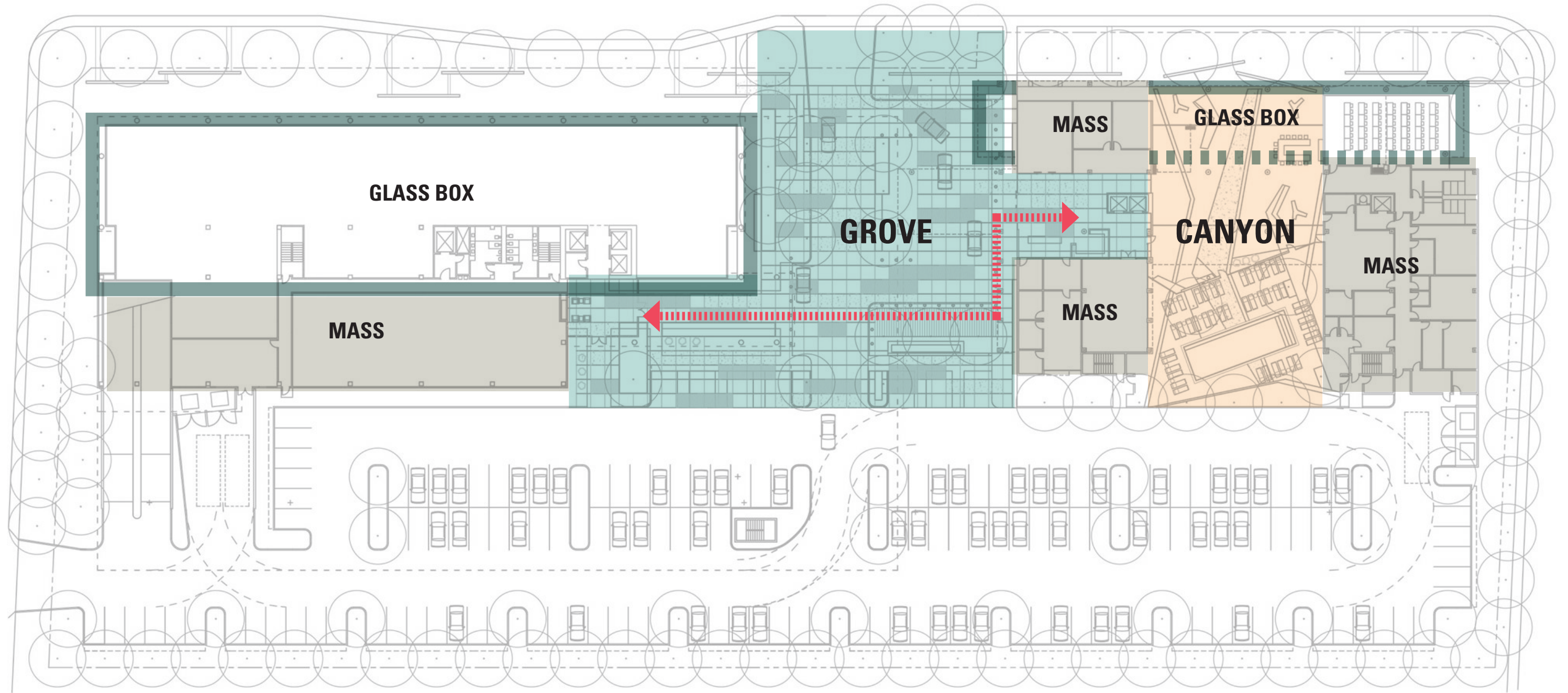


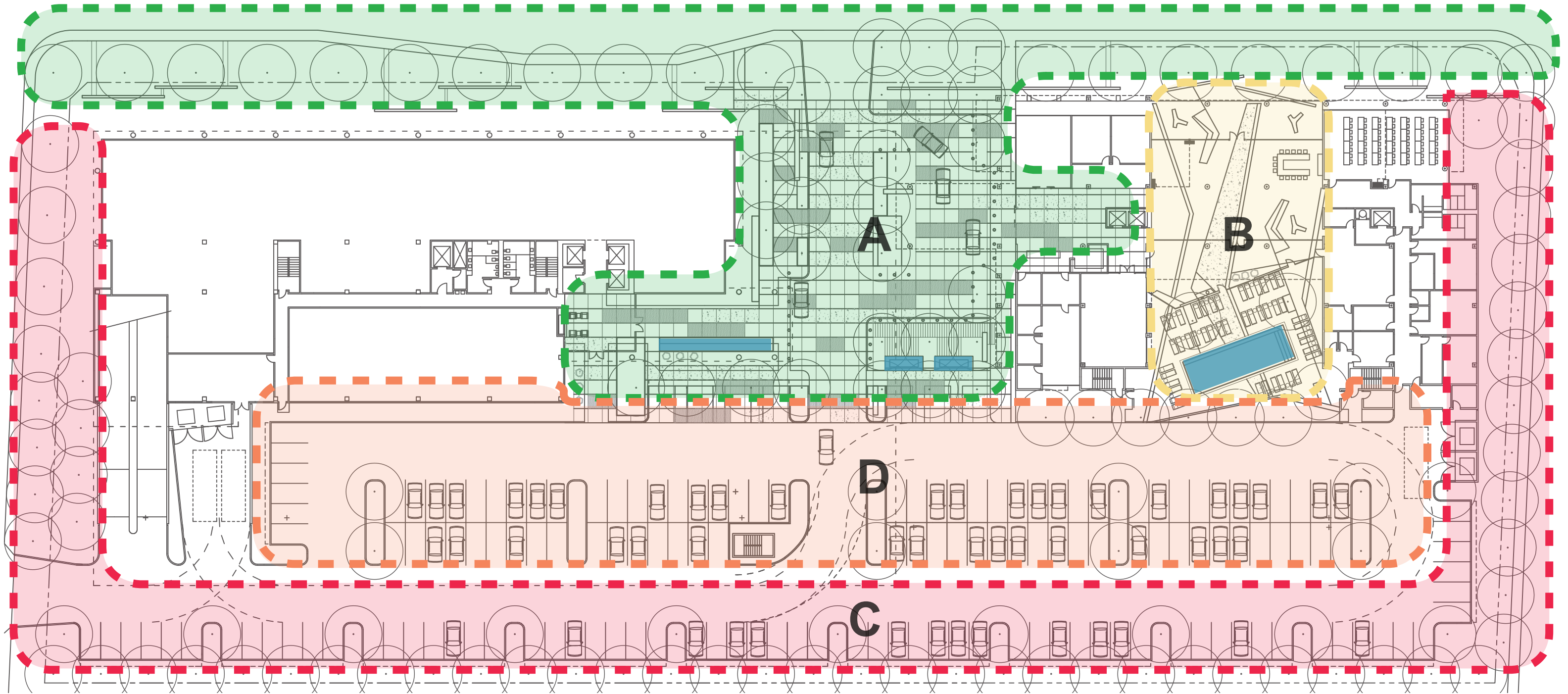
GROVE



CANYON

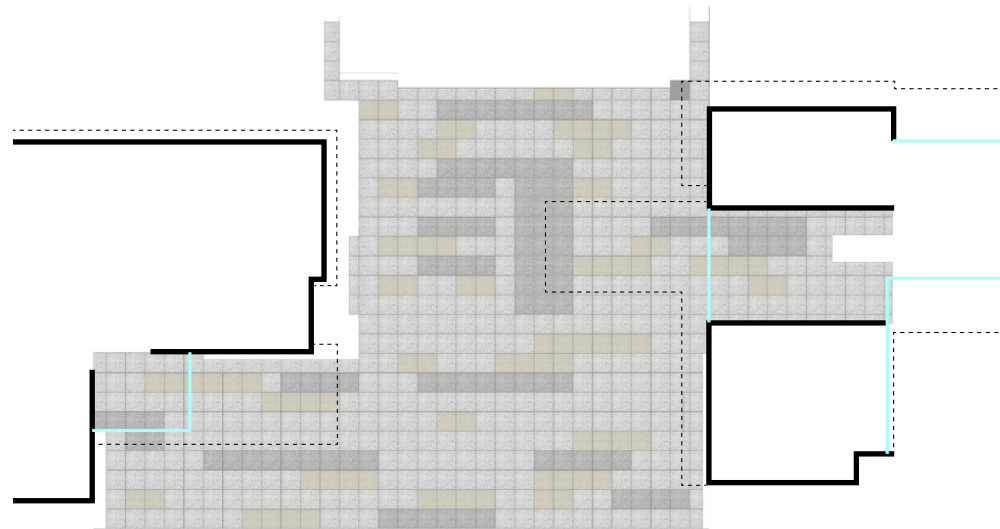
CONCEPT DIAGRAM



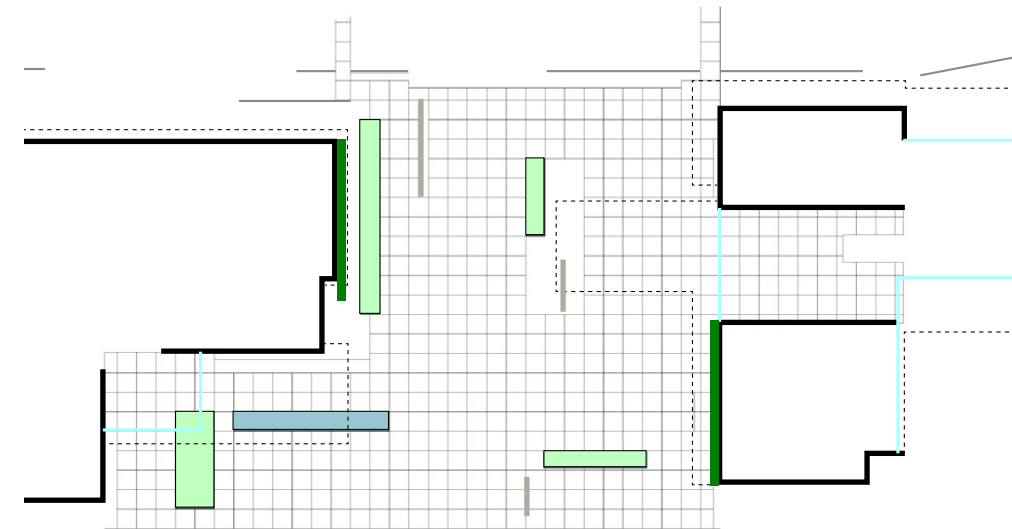


- - - A BUILDING AMENITY ZONE/ENTRY DRIVE
- - - B BUILDING AMENITY ZONE, POOL LOUNGE
- - - C PARKING AND EDGE CONDITIONS
- - - D BUILDING PERIMETER AND PARKING

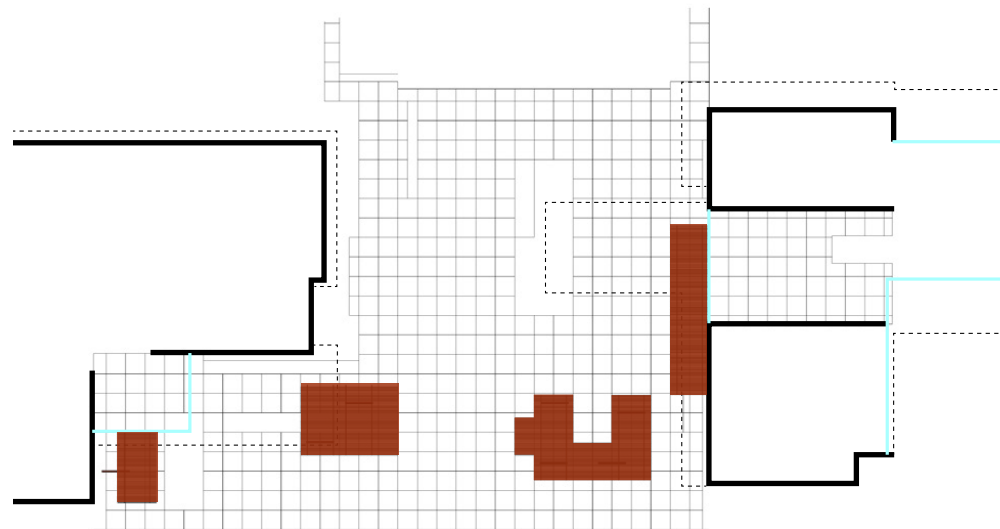
PLAZA CONNECTION



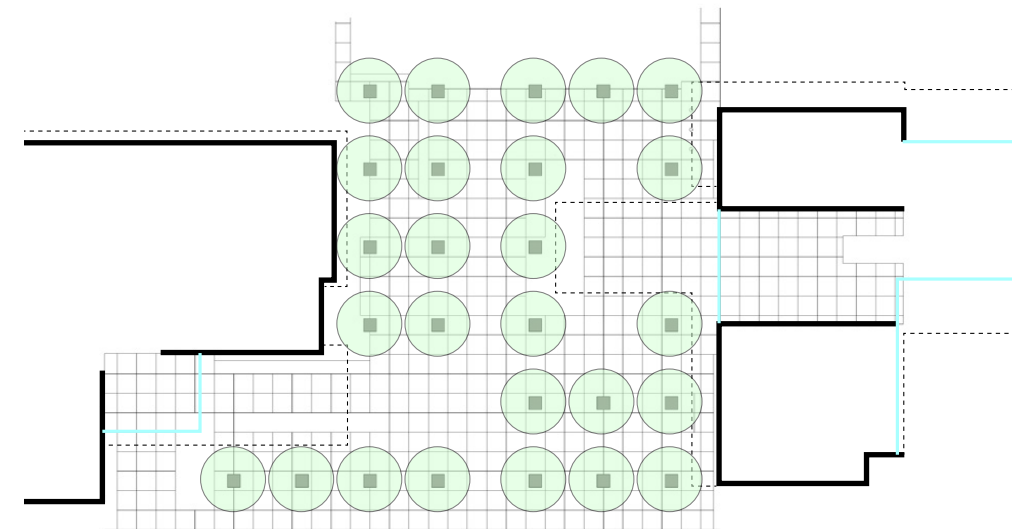
VERTICAL LANDSCAPE



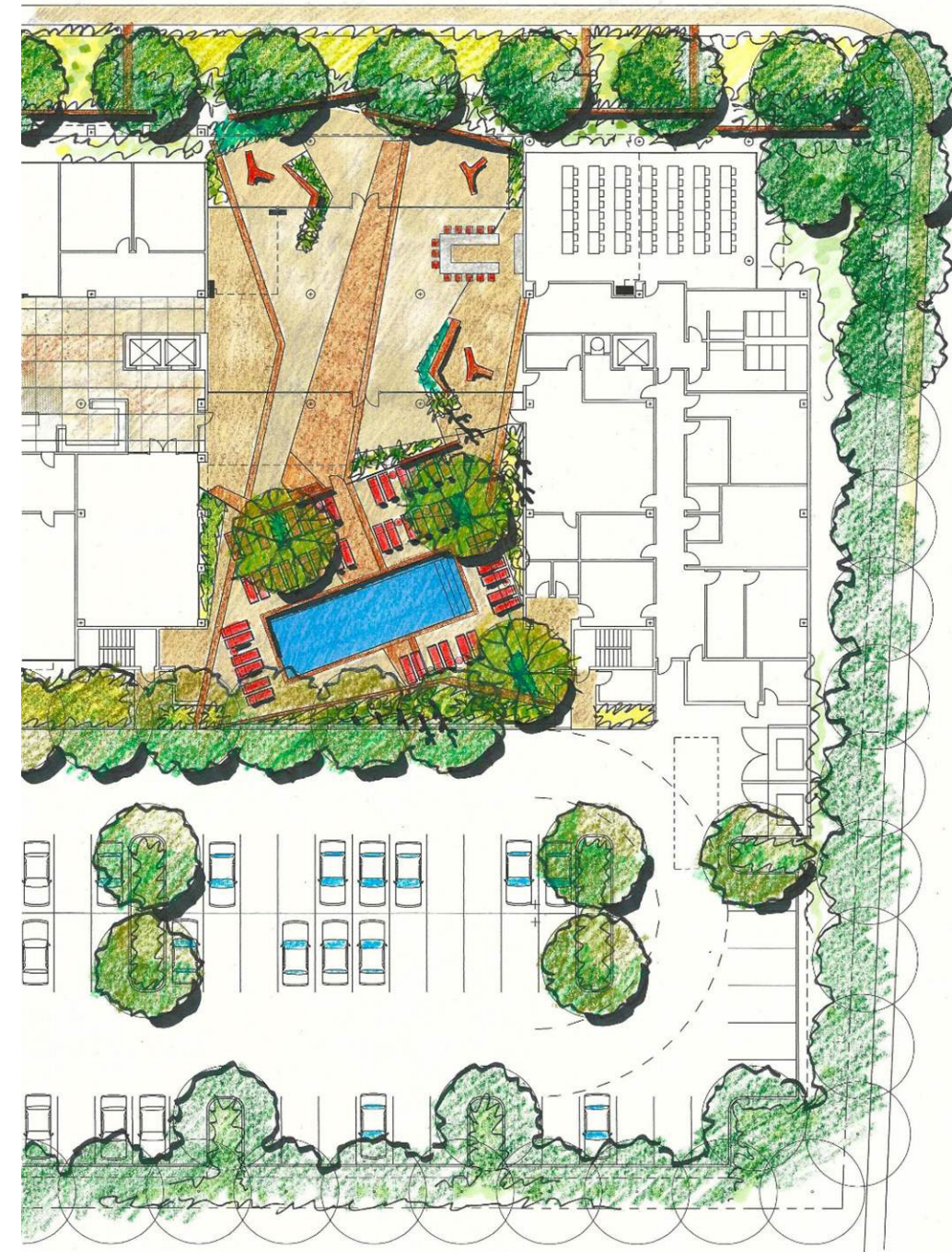
SHADE TRELLIS



GROVE



GROVE + CANYON LANDSCAPE

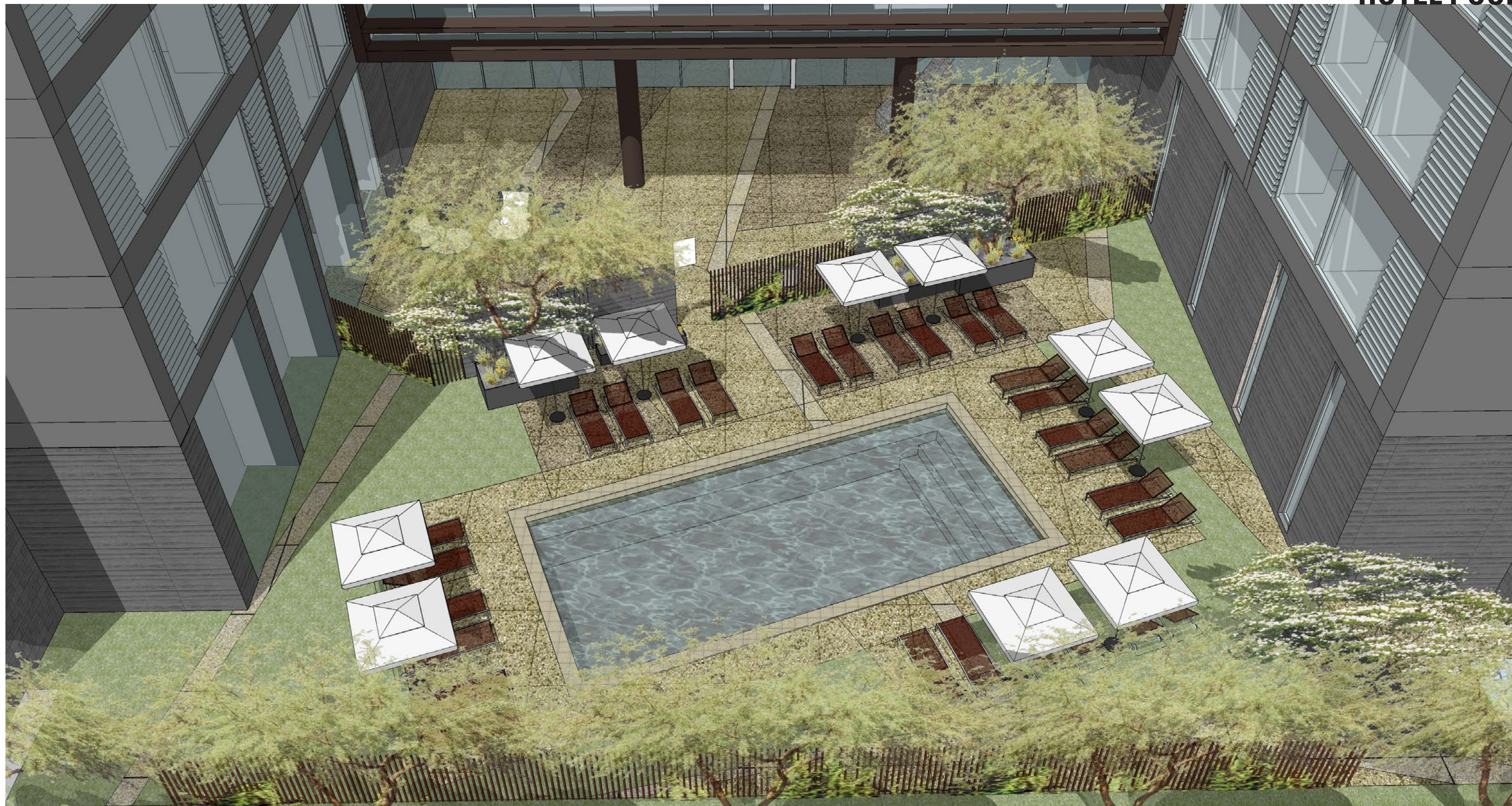


ENTRY PERSPECTIVE

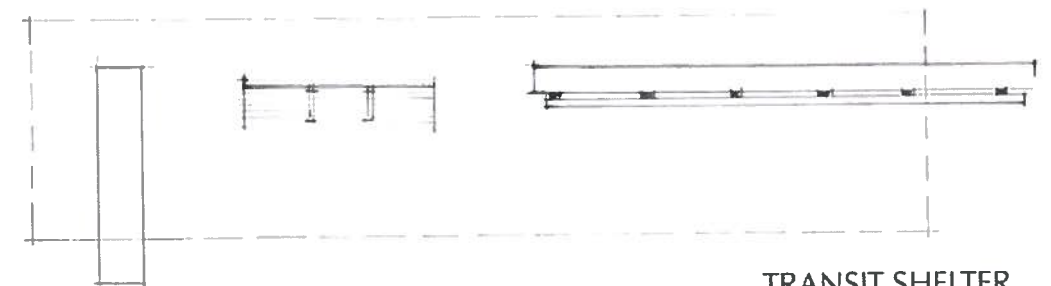
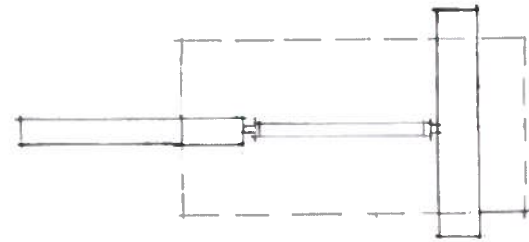
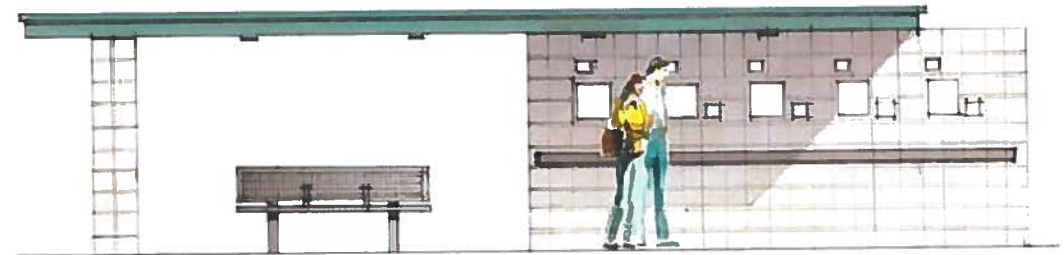
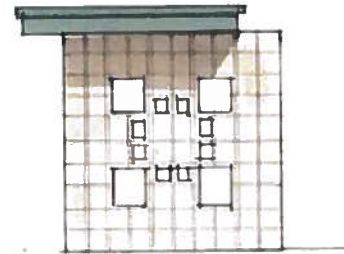
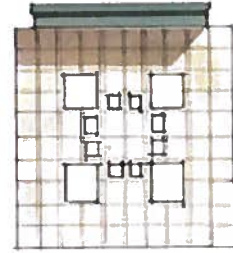
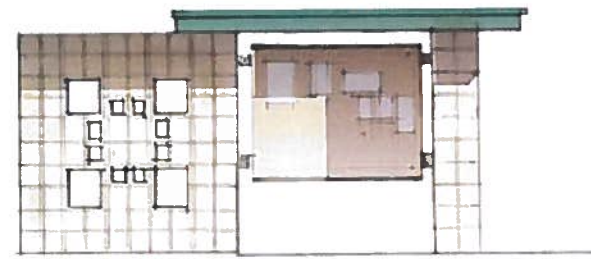


OFFICE ENTRY CANOPY





M-7



KIOSK

TRANSIT SHELTER

CAMELBACK EAST PRIMARY CORE: KIOSK & TRANSIT SHELTER

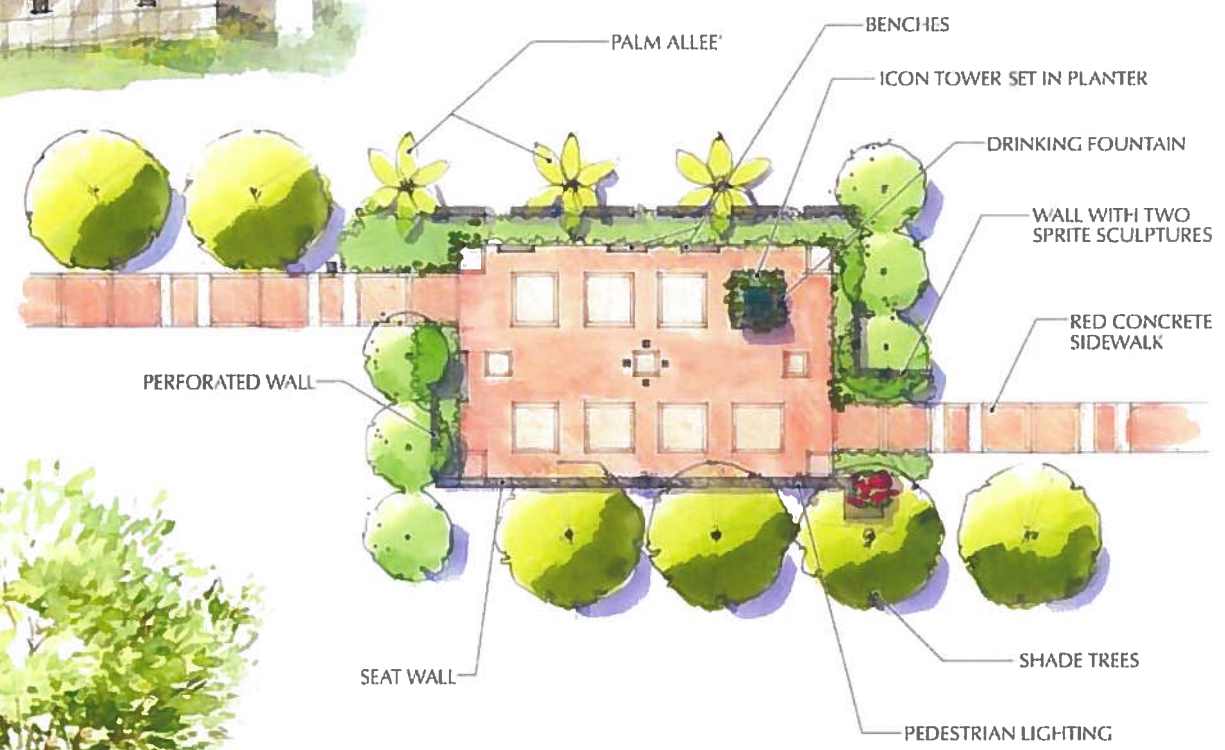


CAMELBACK EAST PRIMARY CORE: **FOUNTAIN NIGHT VIEW**

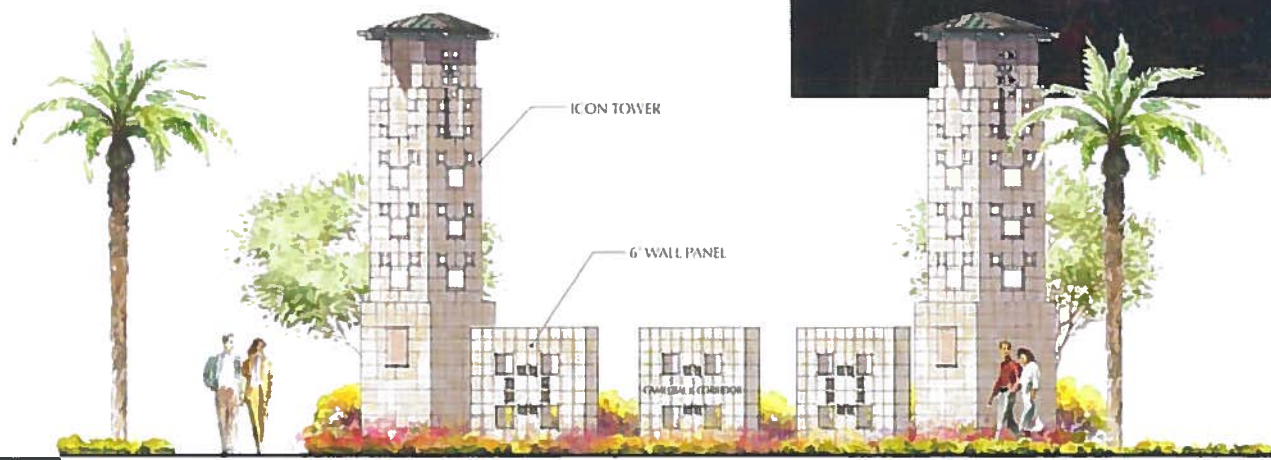
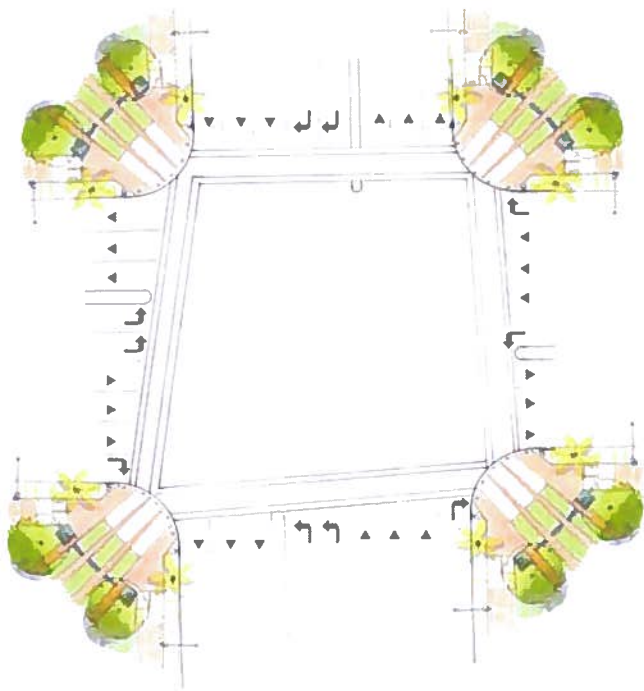


CAMELBACK EAST PRIMARY CORE: FOUNTAIN & SHADE STRUCTURE

GREY PICKETT
landscape architects | community design



CAMELBACK EAST PRIMARY CORE: **OUTDOOR ROOM**



CAMELBACK EAST PRIMARY CORE: CORE INTERSECTION



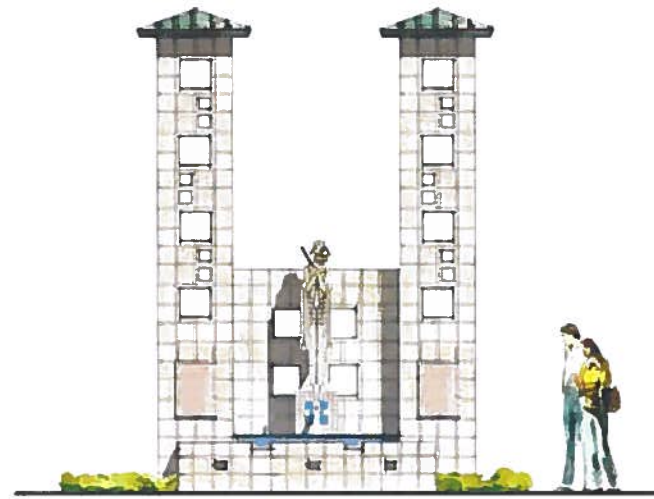
CAMELBACK EAST PRIMARY CORE: CORE GATEWAY



CAMELBACK EAST PRIMARY CORE: PIESTEWA GATEWAY



CORE INTERSECTION



FOUNTAIN



SIDEWALK STANDARD



SIDE VIEW

TRANSIT SHELTER

FRONT VIEW



FRONT VIEW

INFORMATION KIOSK

SIDE VIEW

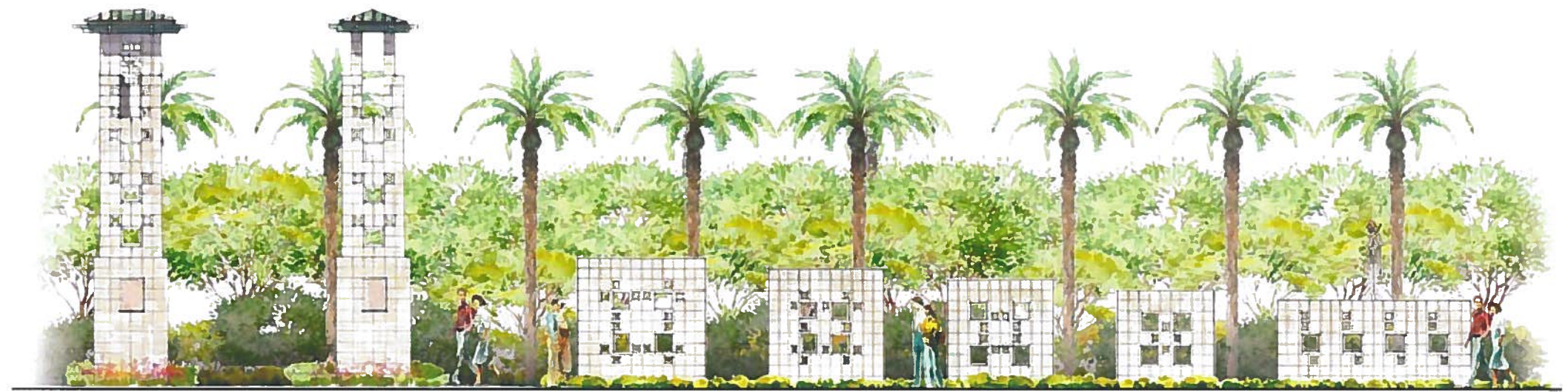


PEDESTRIAN BRIDGE

CAMELBACK EAST PRIMARY CORE: ASSEMBLIES



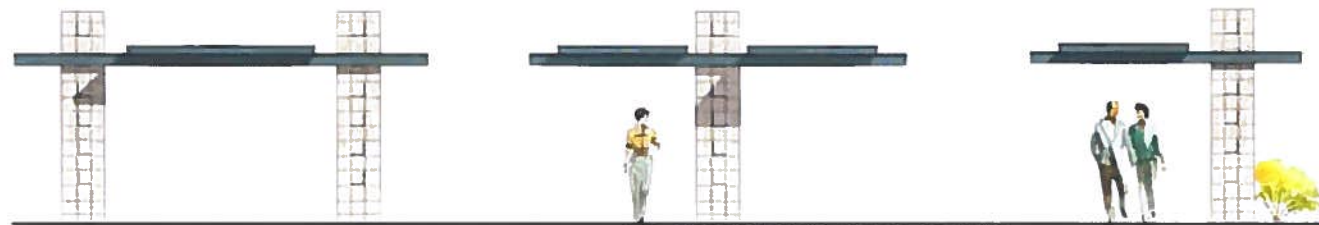
GATEWAY ICON



SIDE VIEW 14' TOWER FRONT VIEW 8' WALL 6'-8" WALL 6' WALL 5'-4" WALL



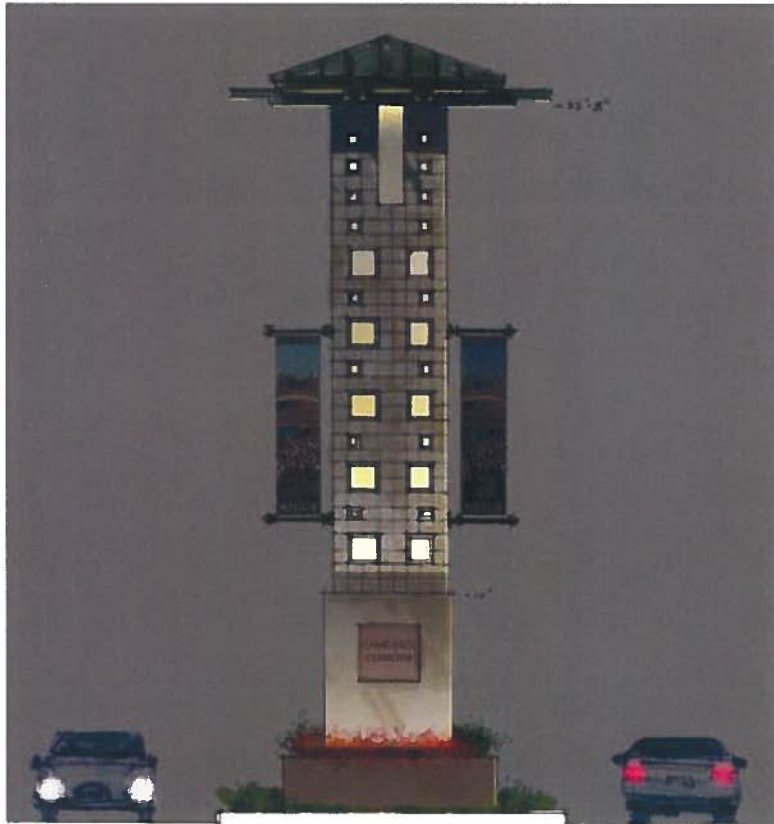
4'-8" WALL WITH SPRITES 4' WALL 3'-4" WALL 2'-8" WALL 2' WALL WITH PLANTER URN 1'-4" WALL



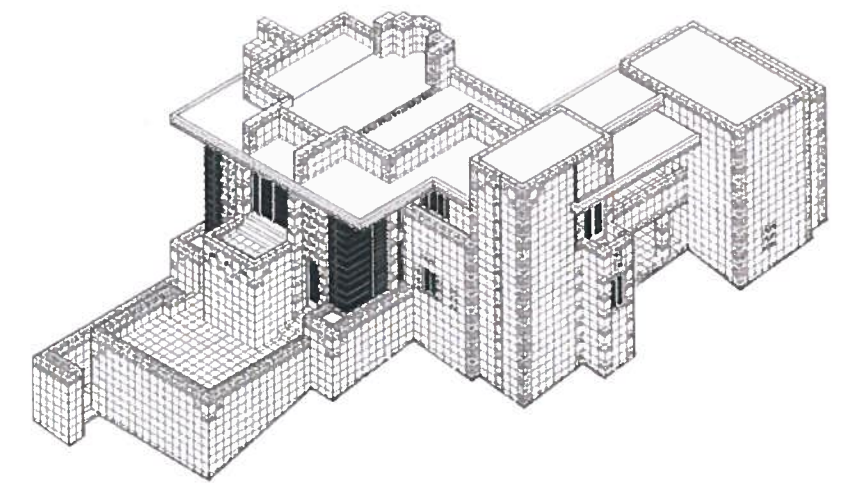
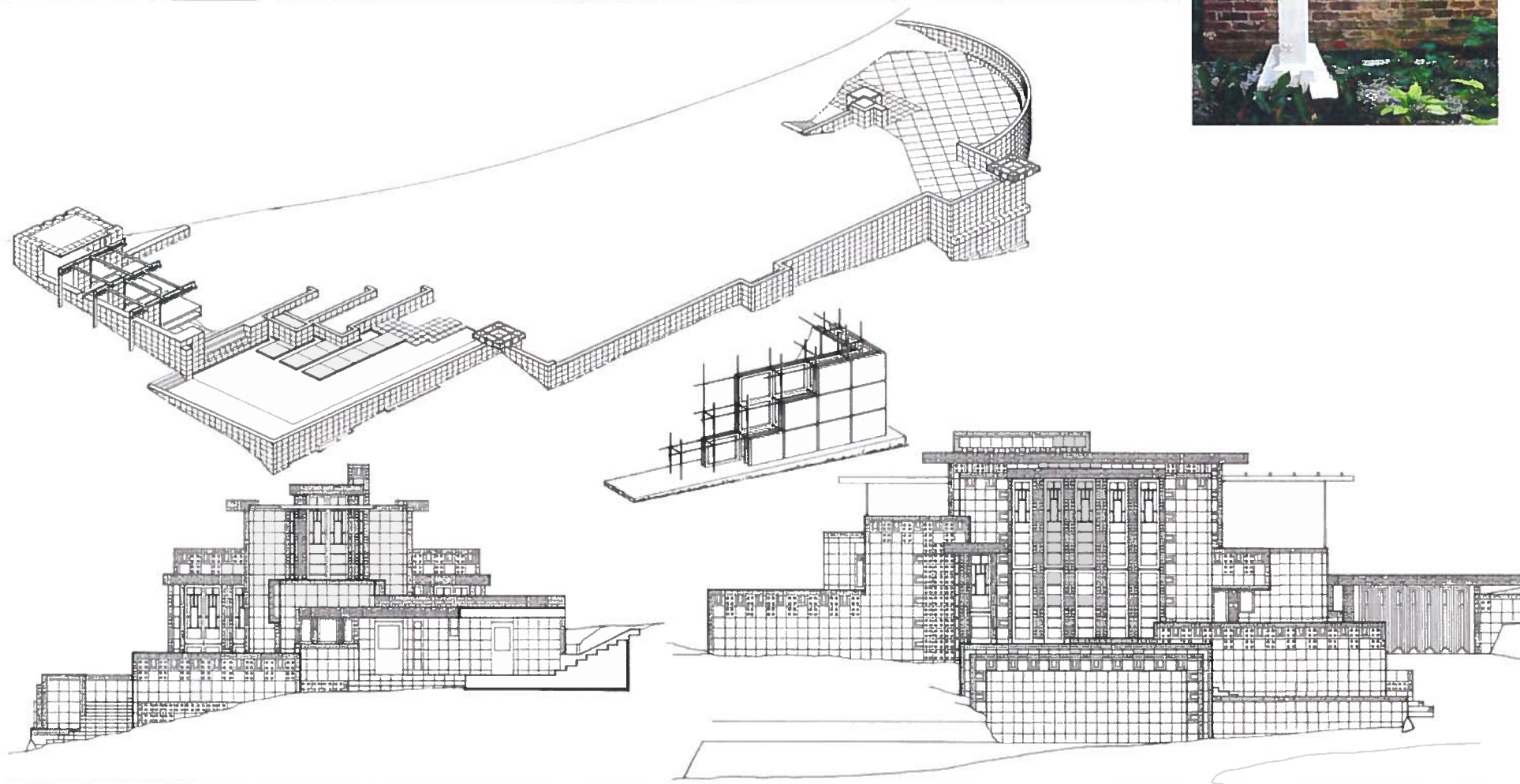
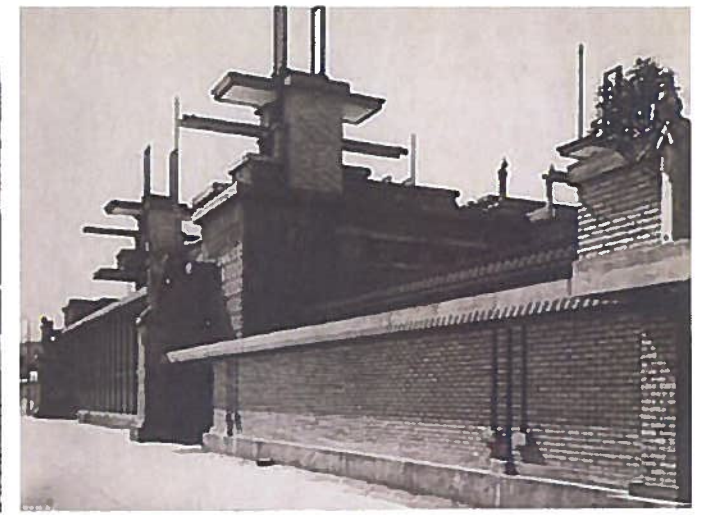
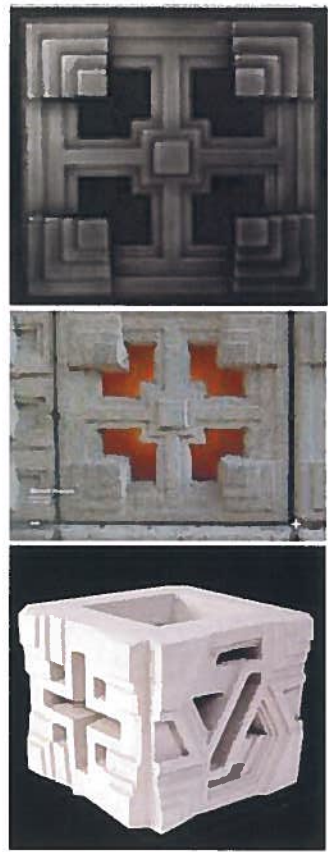
SHADE STRUCTURES



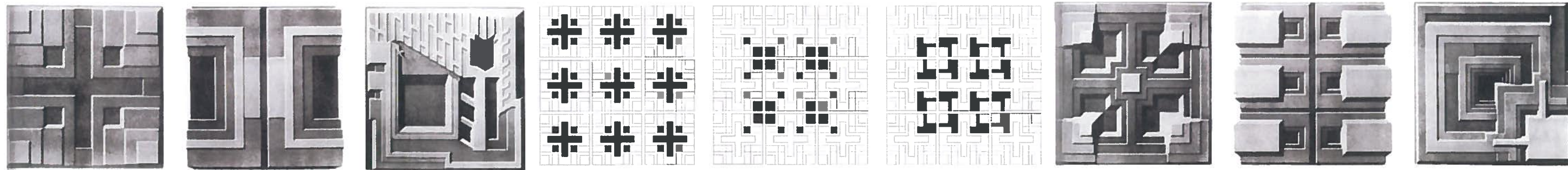
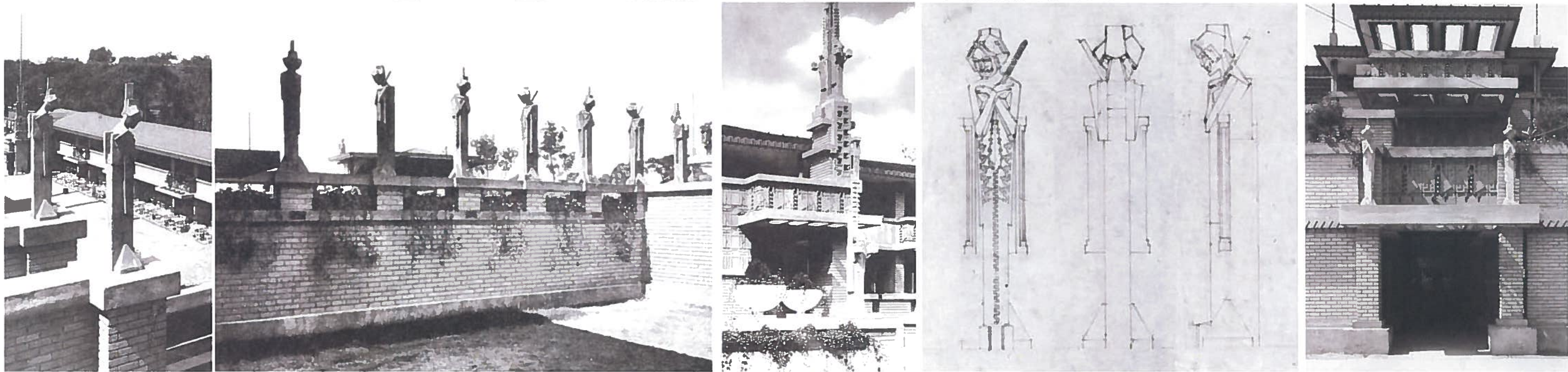
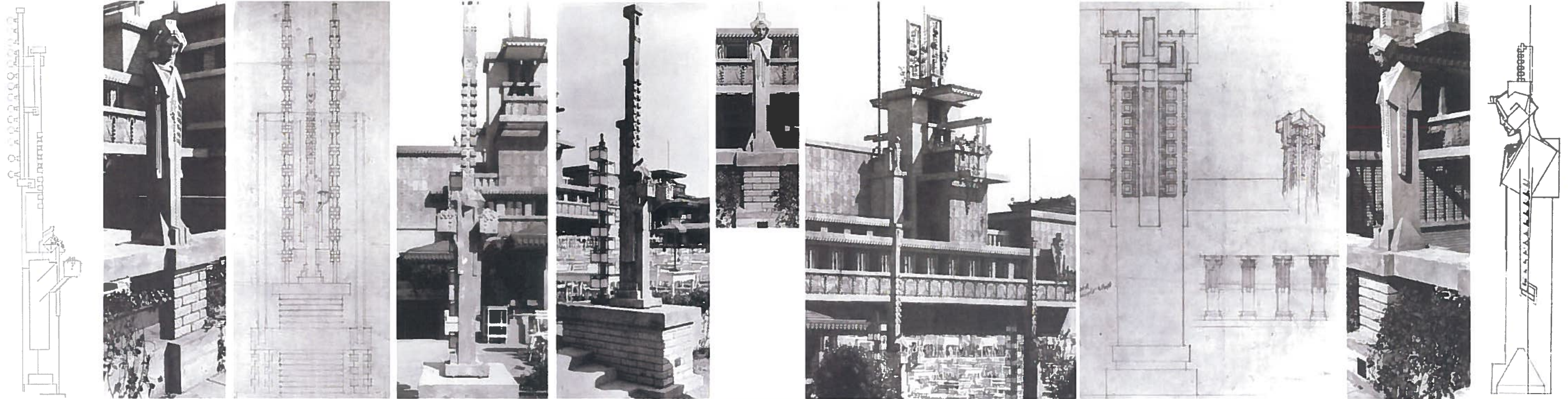
NEIGHBORHOOD ENTIERES 3'-4" SIDEWALK FIN WALL PEDESTRIAN LIGHT COMBINED LIGHT STREET LIGHT



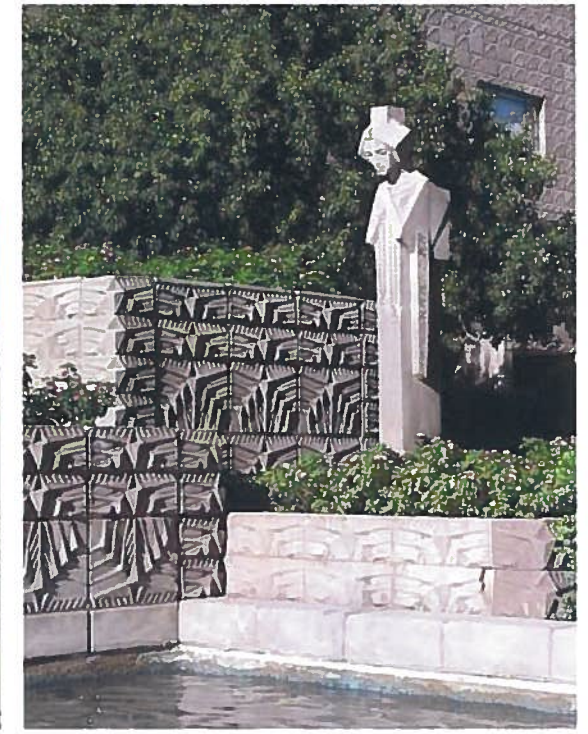
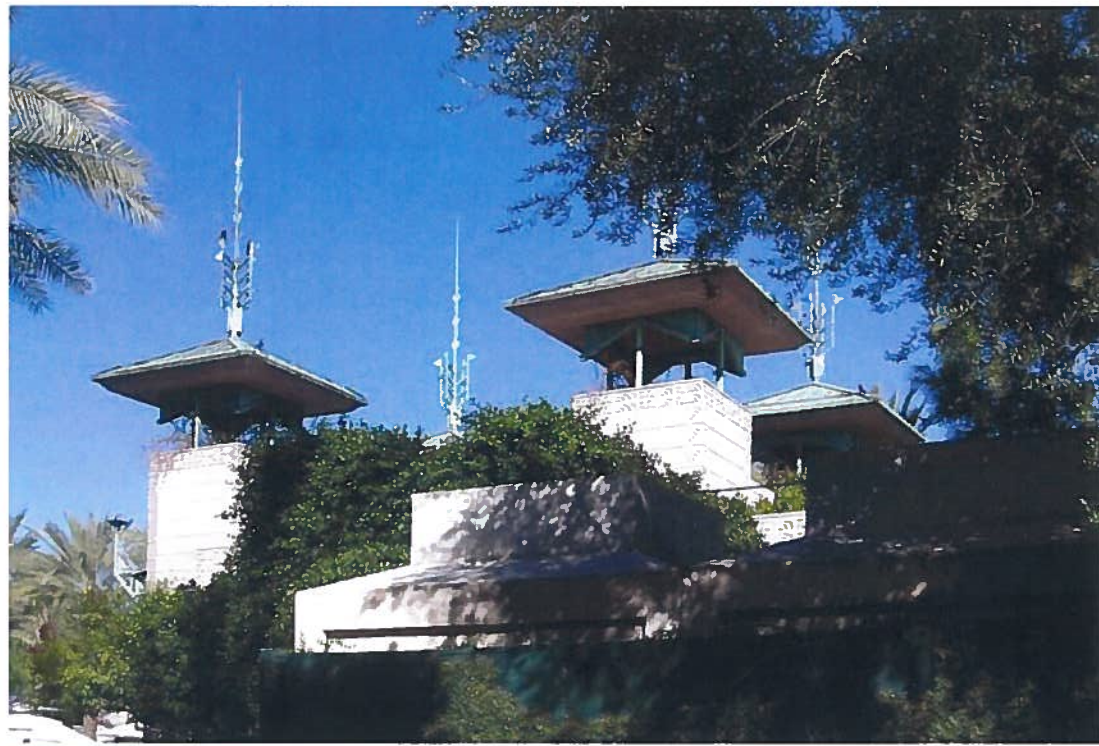
CAMELBACK EAST PRIMARY CORE: KIT OF PARTS



CAMELBACK EAST PRIMARY CORE: **BLOCK AND SPRITES**

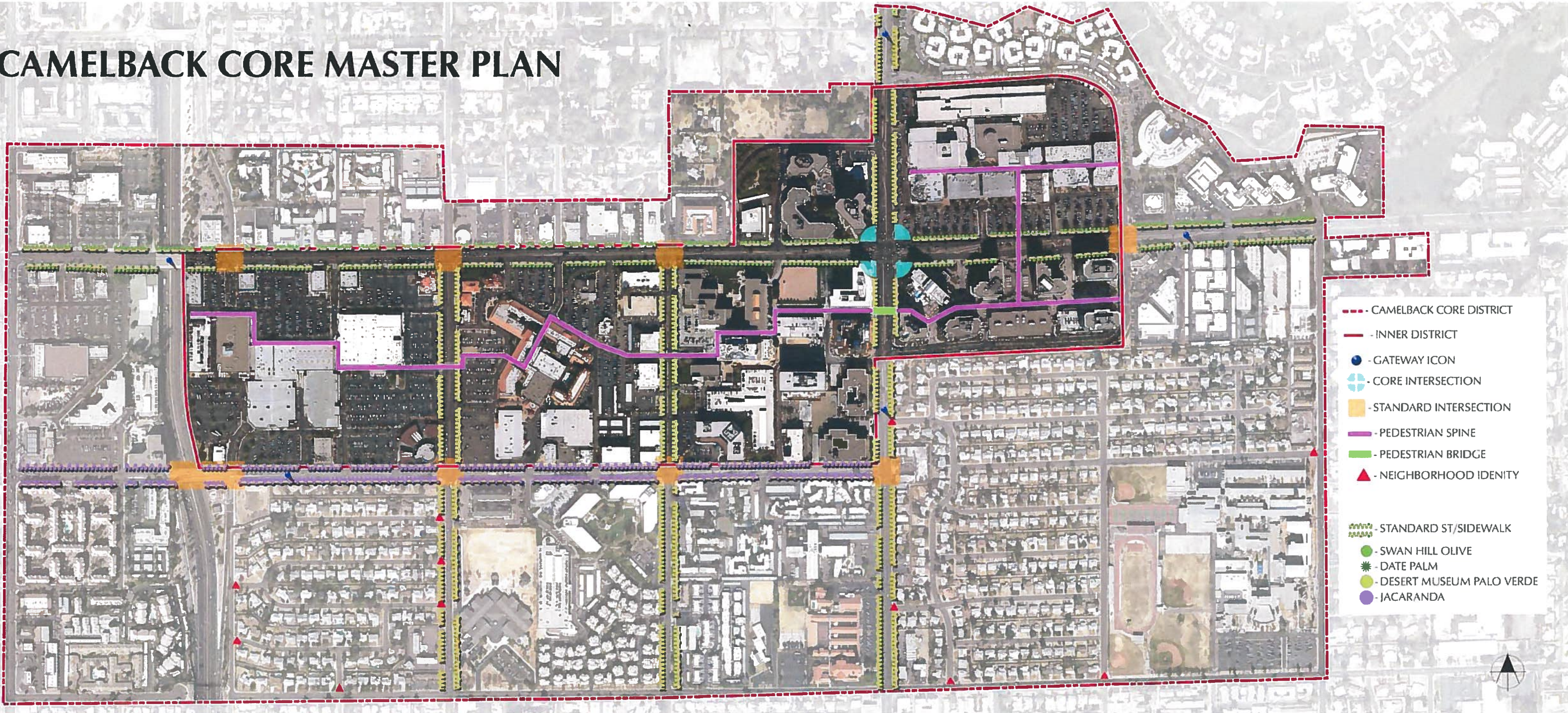


CAMELBACK EAST PRIMARY CORE: MIDWAY GARDENS



CAMELBACK EAST PRIMARY CORE: **BILTMORE INFLUENCE**

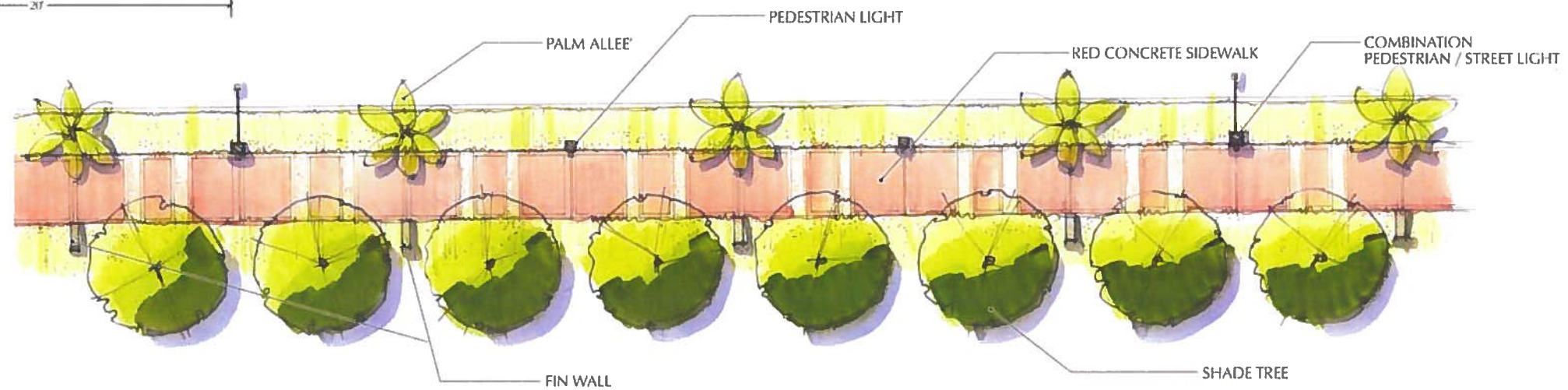
CAMELBACK CORE MASTER PLAN



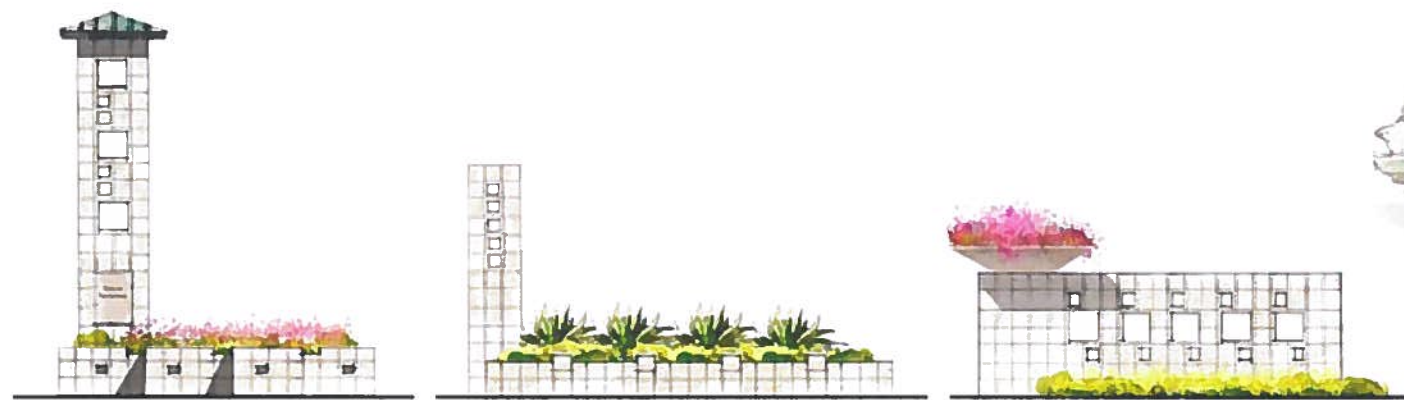
- - - CAMELBACK CORE DISTRICT
- - - INNER DISTRICT
- GATEWAY ICON
- ⊕ CORE INTERSECTION
- STANDARD INTERSECTION
- PEDESTRIAN SPINE
- PEDESTRIAN BRIDGE
- ▲ NEIGHBORHOOD IDENTITY

- STANDARD ST/SIDEWALK
- SWAN HILL OLIVE
- ✱ DATE PALM
- DESERT MUSEUM PALO VERDE
- JACARANDA

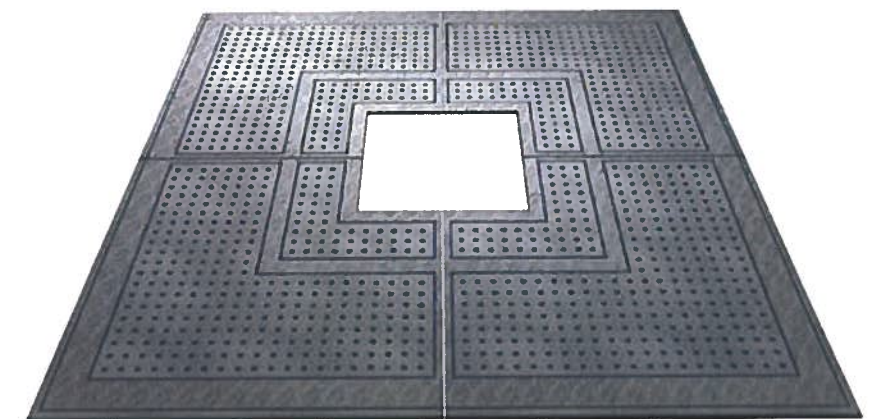




CAMELBACK EAST PRIMARY CORE: STANDARD SIDEWALK



CAMELBACK EAST PRIMARY CORE: NEIGHBORHOOD ENTRY WALLS



CAMELBACK EAST PRIMARY CORE: SITE FURNISHINGS

M-8

LEGEND

City of Phoenix General Plan Map

LAND USE

- 0 to 1 du/acre - Large Lot
 - 1 to 2 du/acre - Large Lot
 - 2 to 3.5 du/acre - Traditional Lot
 - 3.5 to 5 du/acre - Traditional Lot
 - 5 to 10 du/acre - Traditional Lot
 - 10 to 15 du/acre - Higher density attached townhouses, condos, or apartments
 - 15+ du/acre - Higher density attached townhouses, condos, or apartments
 - Parks/Open Space - Publicly Owned
 - Parks/Open Space - Privately Owned
 - Future Parks/Open Space or 1 du/acre
 - Mixed Use Agricultural
 - Nurseries/Flower Gardens with alternative 3.5 to 5 du/acre
 - Commercial
 - Mixed Use (MU)
 - Mixed Use (Areas C, D and Northwest Area only)
 - Industrial
 - Commerce / Business Park
 - Public/Quasi-Public
 - Floodplain
 - Undesignated Area
- NOTE:**
Movement within large lot, traditional lot and higher density residential categories does not require a General Plan amendment.
- Proposed Park in Area
 - Proposed School in Area
 - R Resort (See NOTES: below)
 - 10 Density Cap
 - Density Cap Limit
- ← Commercial
 - Commercial
- * Primary Core
 - * Secondary Core
 - Canal, Watercourse, Wash
 - Existing Railroad
 - Arterial and Collector Streets
 - Future Transportation
 - Light Rail
 - Laveen Conveyance Channel
 - Infrastructure Limit Line (North Black Canyon)

NOTES:

1 Striped areas designate optional uses corresponding to colors shown. Crosshatched areas designate areas in transition. Color in crosshatch is the color to, color between the crosshatch is the color from.



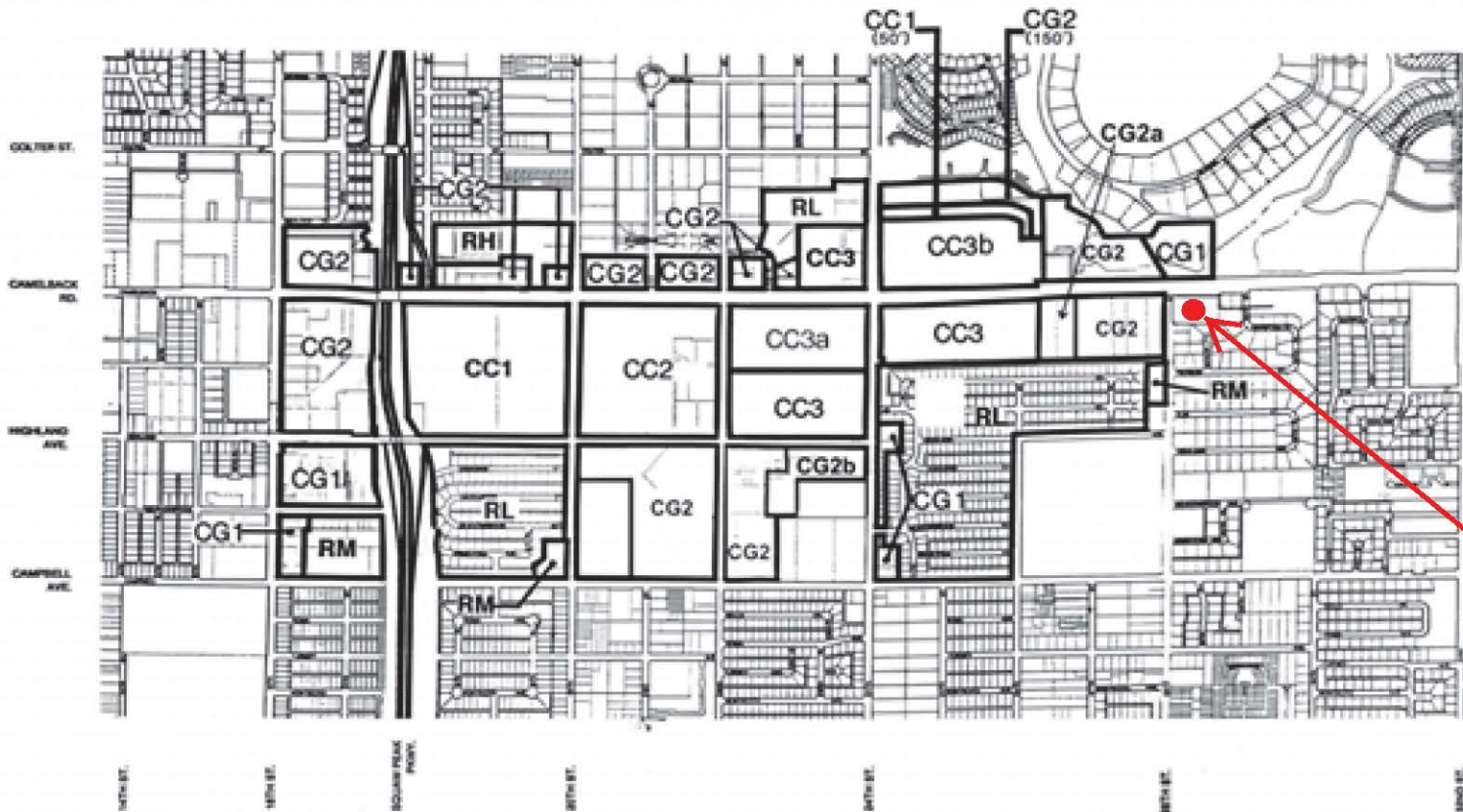
M-9

MAP 4
INTENSITY AND DENSITY PLAN
 CORE CENTER AND CORE GRADIENT
 (2006 Specific Plan Amendment)

LEGEND

COMMERCIAL INTENSITY	F.A.R.
CG1 Core Gradient	.25-.50
CG2 Core Gradient	.25-.50
CC1 Core Center 1	.50-.75
CC2 Core Center 2	.75-1.0
CC3 Core Center 3	1.0 - 1.38

RESIDENTIAL DENSITY	D.U./A.C.
RL Residential Low	0-10
RM Residential Medium	10-25
RH Residential High	25+

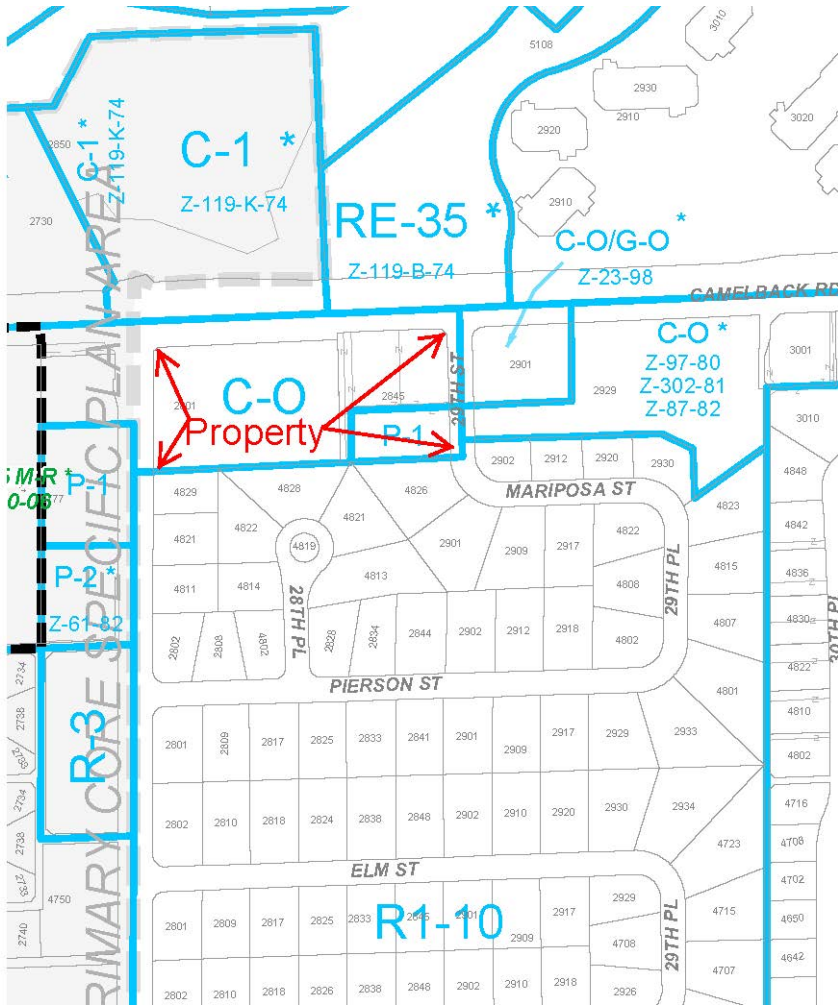


Property

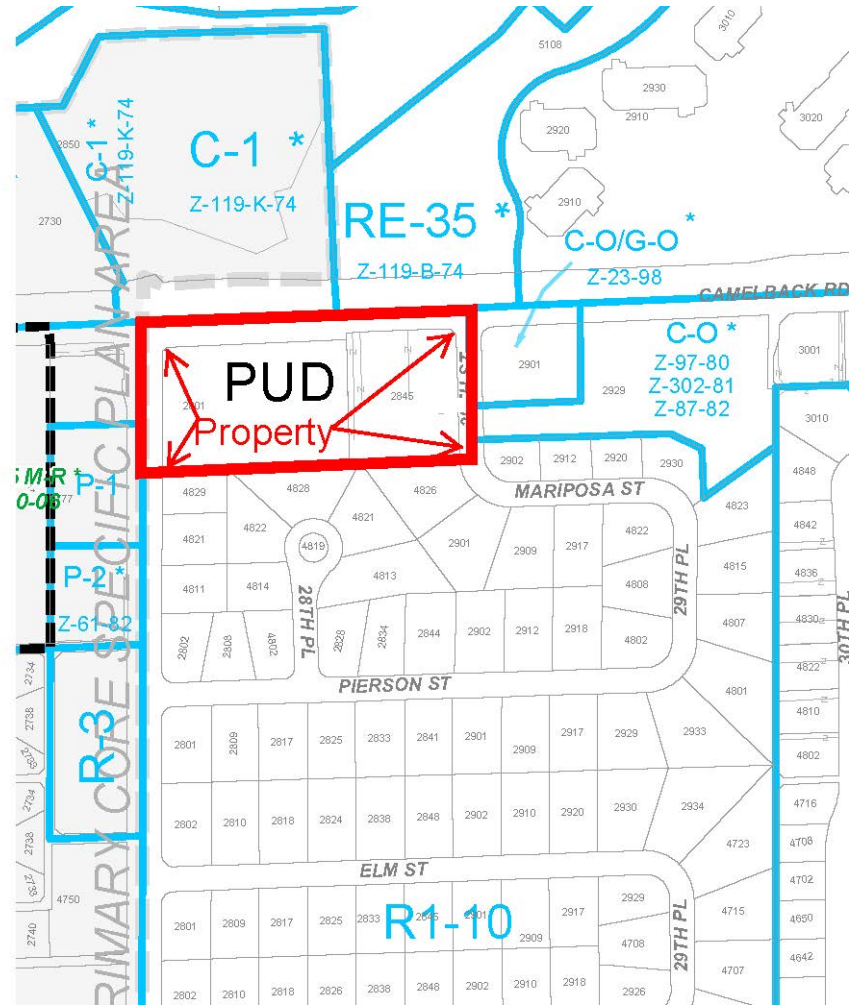


M-10

Existing Zoning Map



Proposed Zoning Map



M-11

Comparative Zoning Standards Table

	C-O	P-1	PUD
Max. Building Setback	N/A	N/A	Front (Camelback Road): 22-feet.
Min. Building Setbacks	Front: 20-feet Side: 5-feet Rear: 15-feet	Front: N/A Side: N/A Rear: N/A	Front (Camelback Road): 18-feet for up to 95 percent of frontage; 10-feet for up to 5 percent of frontage. Street Sides (28 th and 29 th Streets): 10-feet. Rear (south): 110-feet for buildings; 10-feet for parking garage and parking canopies.
Min. Landscape Setbacks	Front: 20-feet Side: 5-feet Rear: 15-feet	Front: 20-feet Side: 5-feet Rear: 5-feet	Front (Camelback Road): 18-feet for up to 95 percent of frontage; 10-feet for up to 5 percent of frontage. Street Sides (28 th and 29 th Streets): 10-feet. Rear (south): 10-feet (inclusive of vehicle overhang no greater than 30-inches).
Min. Building Separation	N/A	N/A	85-feet for primary use buildings; excluding the buildings porte cochere.
Max. Building Height	56-feet>(*Zoned prior to 1986 when city Zoning Ordinance was amended.)	N/A	56-feet Note: A height of 58.8-feet shall be allowed or up to a five percent (5%) allowance to the 56-feet height proposed in order to provide some flexibility as envisioned by the PUD, if necessary, to accommodate unknown future site conditions, design, or building constraints as the project is developed.

Comparative Zoning Standards Table

(cont.)	C-O	P-1	PUD
Max. Lot Coverage	50%	N/A	<p>85% overall total (includes structured parking).</p> <p>Note: A maximum eighty-nine and one-quarter percent (89.25%) shall be allowed or up to a five percent (5%) allowance to the eight-five percent (85%) overall total proposed.*</p> <p>40% total for primary use buildings.</p> <p>Note: A maximum forty-two percent (42%) shall be allowed or up to a five percent (5%) allowance to the forty percent (40%) total proposed.*</p> <p>10% total for accessory structures, including parking canopies.</p> <p>Note: A maximum ten and one-half percent (10.5%) shall be allowed or up to a five percent (5%) allowance to the ten percent (10%) total proposed.*</p> <p>*Said provision is necessary in order to provide some flexibility as envisioned by the PUD, if necessary, to accommodate unknown future site conditions, design, or building constraints as the project is developed.</p>
Min. Drive Aisle Width	26-feet		24-feet
Min. Combined Parking space and drive aisle width depth	Combined depth of 62-feet for a double loaded aisle and 43-feet for a single loaded aisle		<p>60-feet for double loaded aisle.</p> <p>41-feet for single loaded aisle.</p> <p>Vehicles may overhang landscape areas or sidewalks by 30-inches.</p>
Off-street Loading Spaces	1 Space* (*Based on the existing office development.)	N/A	<p>Minimum 1 loading space per building.</p> <p>Maximum of three (3) loading spaces total for the development.</p>

M-12

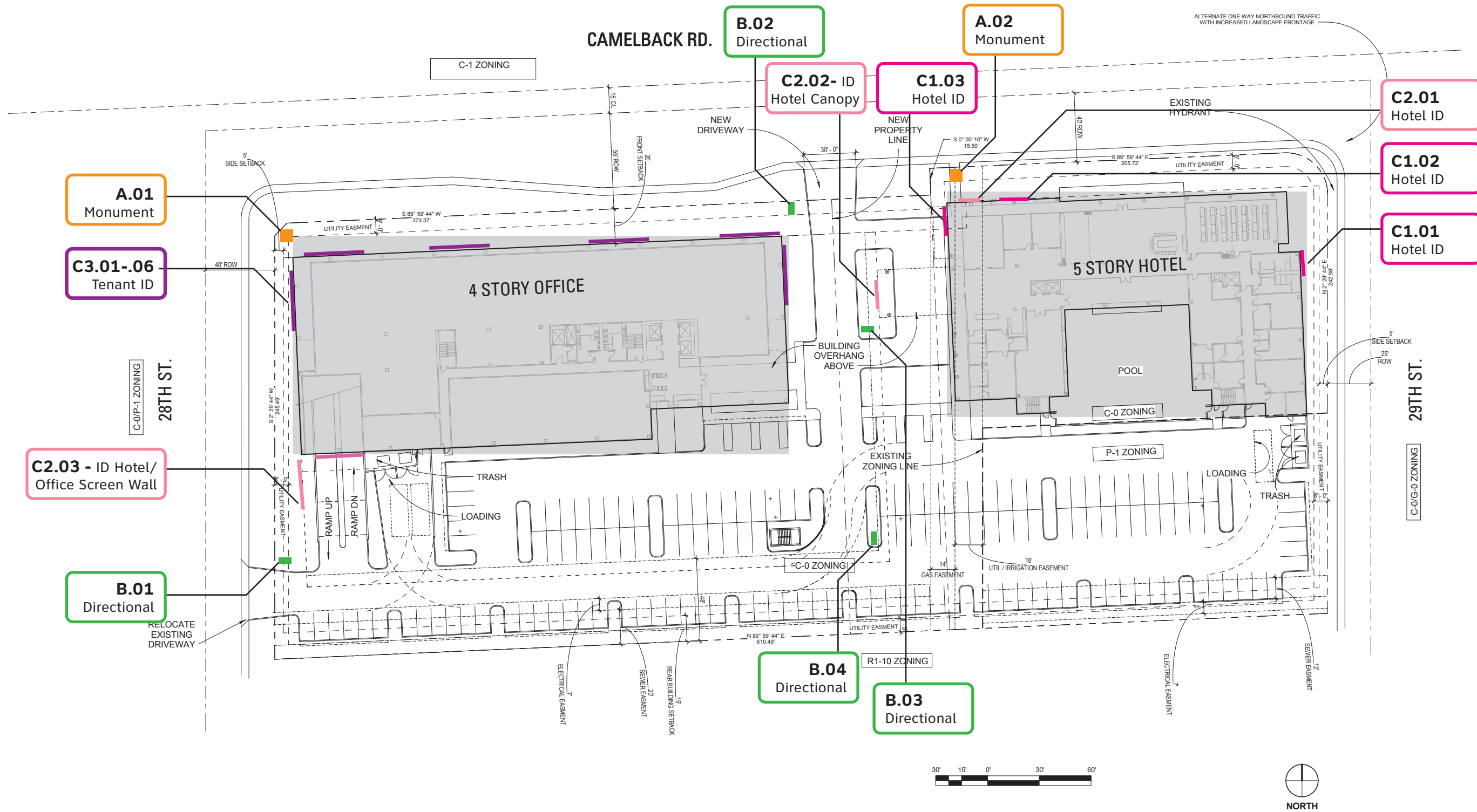
Reference Section (Camelback East Primary Core Specific Plan)	Item	Section Text (Camelback East Primary Core Specific Plan)	Demonstration of Compliance (28 th Street & Camelback Road PUD)
A. Site Development	3.1.1	<i>Materials listed on the Phoenix low-water use plant list should be used within the right-of-way. However, the selected plant material should contribute to the Biltmore landscape design theme.</i>	Plants within the right-of-way will be from the ADWR plant list, along with the Biltmore landscape design theme intent and characteristics. Trees are to be arranged in a more formal triangulated spacing, along with shrub massing with a more evergreen trait that reflects the Biltmore character.
A. Site Development	3.1.2	<i>On private property, the existing “Old Phoenix” or “Arizona Biltmore” landscape theme should be continued and strengthened in the Core Center and Core Gradient through use of plant materials such as [those listed below]. These and other “historic” plant materials should be used in combination with native plants and other low-water use landscape materials. This landscape theme features cacti and other native plants as exotics, setting them in a framework of plant materials historically used in the Core area. TREES: jacaranda, palms, olive trees, eucalyptus, pepper trees, juniper, mesquite, palo verde; SHRUBS: oleanders, privet, boxwood, pyracantha, catclaw, bougainvillea; ACCENTS: roses, annuals, jasmine, specimen cacti; GROUNDCOVERS: turf</i>	The selection of "Old Phoenix" or "Arizona Biltmore" plants (see landscape narrative with extended plant list including plants with "historic" characteristics, as well as Mesquites trees, Olive Trees, Palo Verde trees, bougainvillea, jasmine and specimen cacti). Additional plants on the list include drought tolerant evergreen shrubs and ground covers to be used in bold mass plantings to reflect the historic character, along with a balance of striking texture and flower color. Annuals for seasonal color to be provided in architectural pots/planters in pedestrian zones.
A. Site Development	3.4.2	<i>Locate fountains and/or other water features at those points along the pedestrian spine where the highest concentrations of pedestrians are anticipated engage in activities other than just walking through.</i>	The proposed water features are within the main central pedestrian zone between the buildings near the entry points – called “the Grove.”
A. Site Development	4.1	<i>Shade and street furnishings should be provided for the comfort and convenience of pedestrians especially within and/or adjoining the pedestrian spine, (see Map 8 of Specific Plan). Furnishings should include pedestrian lighting fixtures and one or more of the following: drinking fountains, benches, trash receptacles, information kiosks</i>	The proposed design includes shade trellises, trees, vertical landscaping, benches, tables and chairs, and water features within a shared central courtyard between the buildings –called “the Grove.”
A. Site Development	4.4	<i>Along the defined pedestrian spine, active, pedestrian outdoor public spaces (a plaza, courtyard, garden, “outdoor room,” or a promenade) should be provided within private developments and connected with pedestrian walkways.</i>	A large courtyard at the center of the site will link the two buildings' main entries. All will be connected by pedestrian walkways that will be at a consistent floor finish elevation for greater continuity and connectedness.
A. Site Development	5.1	<i>Continuous sidewalks or other pedestrian walkways linking private developments with side walks along public streets should be provided.</i>	The public sidewalk along Camelback Road will access the property's main pedestrian core, which will lead to the front door of the hotel and office buildings.
A. Site Development	5.2	<i>A continuous walkway system shall be as shown on Map 8, pedestrian facilities plan. The pedestrian spine should always be continuous and provide connectivity between pedestrian destinations, even if the alignment is modified.</i>	The hotel lobby will be connected to the office lobby by way of a continuous pedestrian walkway including the amenities listed above. There will be no level change along this continuous path, and vehicular drive and drop off will be ramped to meet this floor elevation.
A. Site Development	5.3	<i>Safe and convenient means of pedestrian access should be created between neighborhoods and the Core.A. Access via other than public streets should be provided if requested by the affected home owners association, with consent of adjacent properties and if security measures for the neighborhood are addressed.B. Access via public streets should be provided in the form of a continuous side walk connecting all residential areas within or adjacent to the Core Center or Gradient to Camelback Road.</i>	Pedestrian access between the neighborhood and the building's entrances will be safely and conveniently provided by a continuous sidewalk around the property's three sides. For added safety and visual interest along arterial Camelback Road, the sidewalk will be detached from the road for the length of the site with the exception of the bus stop. This item is not applicable to this development, since the lot is adjacent to individual properties to the south rather than a neighborhood common path. A continuous sidewalk along 28th, Camelback, and 29th will be provided. These will connect with building entry points on the site's north side.
A. Site Development	6.1.1	<i>[Surface] Parking must be conveniently located, but visually unobtrusive. A. Parking lots must be screened from street view with landscaping, berms, walls, grade changes or a combination of these elements.</i>	The proposed design includes a surface parking lot that is conveniently located and visually unobtrusive. The following portions of this section will be met: The surface parking lot is located at the back (south) side of the site, effectively screened from view from

Reference Section (Camelback East Primary Core Specific Plan)	Item	Section Text (Camelback East Primary Core Specific Plan)	Demonstration of Compliance (28 th Street & Camelback Road PUD)
		<p><i>B. For office and retail commercial projects, every parking space shall be no greater than 150-feet from a sidewalk leading to the building or from a building entrance, and unshaded segments of walkways shall not exceed 15-feet except at a driveway crossing.</i></p> <p><i>C. For all new development a minimum of 15% of parking lot area must be landscaped with trees and shrubs. For all expansions to existing development, remodeling, or additions, a minimum of 10% of parking lot area must be landscaped. Minimum width of landscape area is five-feet (seven with parking overhangs).</i></p>	<p>Camelback Road by the office and hotel buildings. The courtyard between the buildings and the site edges (28th and 29th Streets), will provide landscaping and trees for screening.</p> <p>All surface parking spaces are within 150-feet of sidewalks leading to building entrances. Designated walkways (drive aisles excluded) are shaded by trees or trellises.</p> <p>The proposed design includes 15% or greater landscaping in the surface parking lot through landscape islands and perimeter trees and groundcover.</p>
A. Site Development	6.1.2	<p><i>Within the Core Center, all [surface] parking for office uses should meet the following standards.</i></p> <p><i>A. If such parking exceeds 50 spaces, 80 percent of the total number of parking spaces required for office use must be within above grade or below grade parking structures.</i></p>	<p>The surface parking count will be 155, exceeding the 50 space limitation. With the exception of office visitor parking, all office parking required will be assigned to the two level below grade parking structure. The number of below grade parking spaces for office use will exceed the 80% requirement.</p>
A. Site Development	6.2.1	<p><i>Parking [Structure] must be conveniently located, but visually unobtrusive</i></p> <p><i>A. Parking structures must be either architecturally integrated with adjoining buildings or should be designed with architectural features similar to adjoining buildings for visual continuity.</i></p>	<p>The parking structure for this site is proposed to be entirely below grade, thus it is visually unobtrusive. Its ingress and egress ramp is located at the far west edge of the site for convenient access off 28th Street.</p>
A. Site Development	6.4.1	<p><i>Secure bicycle parking areas must be provided, preferably in supervised enclosures or in bicycle lockers. The provision of bicycle parking may be phased per approval of the Traffic Management association. However, a minimum of 25% of the required bicycle parking shall be provided at the time of construction.</i></p> <p><i>A. Bicycle parking must be located within parking structures or other secure covered areas for employees and within convenient distance of primary commercial and community facility destinations.</i></p> <p><i>B. Bicycle parking spaces must be provided at a ratio of one space per 10,000 square-feet of retail uses and one space per 15,000 square feet of office or other non-residential uses.</i></p> <p><i>C. Bicycle parking spaces must be either within lockers, a secure enclosure or other facility designed for use of high-security locks, for securing both wheels and frame and offering support (per Appendix J, Arizona Bicycle Facilities Planning and Design Guidelines, 1988). A minimum space of two-feet by six-feet per bicycle should be provided.</i></p>	<p>Bicycle parking areas will be provided for employee use in the below grade parking structure.</p> <p>Bicycle parking areas will be located on the first level of below grade parking structure and close to the passenger elevator for convenient access to the main lobby.</p> <p>At one space per 15,000 SF, there are 7.7 required bike spaces. The design will provide a minimum total of 20 bicycle spaces accessible to each of the buildings.</p> <p>Bike racks will be provided for securing both wheels and frame. A space of two-feet by six-feet will be provided per space.</p> <p>The design will provide a minimum total of five (5) bicycle storage lockers accessible to each of the buildings.</p>
A. Site Development	8	<p><i>Citywide lighting standards apply to all light sources in each phase of development.</i></p>	<p>Item is acknowledged by the design team and will be complied with as part of the building design development.</p>
A. Site Development	10.3	<p><i>...Separation of sidewalks from the roadway is required except at bus stops and pedestrian street crossings or where access to the curb is necessary, or on the north side of Camelback, between 24th and 26th streets, to maintain the established, mature olive tree theme. If a transit shelter is provided, the distance between curb and right-of-way line shall be increased by the width of the transit shelter if the shelter is placed between curb and sidewalk. (See also Design Guideline A.10.10)</i></p>	<p>The sidewalk along Camelback will be detached from the roadway, as shown on the site plans.</p>

Reference Section (Camelback East Primary Core Specific Plan)	Item	Section Text (Camelback East Primary Core Specific Plan)	Demonstration of Compliance (28 th Street & Camelback Road PUD)
B. Building Design / Construction	1.1	<p><i>A strong and relatively continuous building frontage adjacent to public streets and the pedestrian spine should be provided. A minimum 30% of the lot frontage should include buildings at the set back line of each lot* along the defined pedestrian spine, with the exception of intersecting walk ways and outdoor rooms, the frontage should be continuous along at least one side of the defined pedestrian spine. On the Colonnade (CCI) site this is most important along 20th Street.</i></p> <p><i>(* "lot" means the project development lot for zoning.)</i></p> <p><i>A. Parking fields should not exceed 250-feet in depth. If greater than 250-feet then 30 % of the frontage on a public street should have building abutting the street.</i></p>	<p>The proposed buildings will establish a strong and relatively continuous building frontage along the Camelback Road street frontage.</p> <p>The surface level parking field will be a maximum depth of 99-feet, thus it is well below the 250 foot limitation.</p>
B. Building Design / Construction	1.3.B	<p><i>In the design of pedestrian-oriented plazas and courtyards, hard surfaces should be shaded from mid-May through mid-October.</i></p>	<p>The courtyard's hard surfaces will be shaded through the use of trees, shade trellises, a porte-cochere, and vertical landscaping.</p>
B. Building Design / Construction	4.1	<p><i>Design elements at ground level, especially along the pedestrian spine, should be created with sufficient variety and detail to be of interest to pedestrians by providing three or more of the following:</i></p> <p><i>A. Architectural details and articulation.</i></p> <p><i>B. Visual links to open spaces within developments.</i></p> <p><i>C. Ground level building facades that incorporate a substantial portion of their frontage as transparent elements.</i></p> <p><i>D. People-active areas and uses (balconies, terraces, patios, etc.).</i></p> <p><i>E. Intersections/movement options for pedestrians should be frequent, avoiding long, blank walls.</i></p>	<p>The proposed design will comply with the following listed items:</p> <p>Will provide open space within the development at ground level</p> <p>Will provide high span clear glazed storefront at ground level.</p> <p>Will provide courtyard spaces with seating and shading elements, both manmade and vegetative, in addition to a hotel patio with pool and landscaping.</p>
B. Building Design / Construction	4.2	<p><i>Highly visible and direct pedestrian access to buildings, transit waiting areas and businesses should be provided from the public streets, the pedestrian spine, and internal plazas.</i></p> <p><i>A. Building entries should be architecturally prominent. B. Ground floor retail and service businesses located within 50-feet of a public street should have direct access from the public street. C. Barriers such as walls and hedges between buildings and bus stops should be avoided. D. Provide at least one building or site entrance per block per public street or 300-feet whichever is less. E. All buildings over four stories in height should be designed with a building base that is differentiated from the remainder of the building in order to relate to a pedestrian scale. F. Walls without windows or shadow boxes shall not exceed thirty percent of the area of the first 12-feet of the height of the building within 100-feet facing a street or designated pedestrian spine or outdoor room.</i></p>	<p>The proposed design complies with the following six (6) listed items:</p> <ol style="list-style-type: none"> 1. A large outdoor space will be provided between the buildings. This space will include a vehicle drop-off including porte-cochere at the hotel, shading elements at the office building, trees, fountains, and architecturally articulated facades with building address signage. Conceptually, this space is known as "the Grove," and the pavement elevation will be consistent all the way across, for pedestrian convenience. 2. Direct access will be provided to each of the building entrances by way of a sidewalk connection to the north. 3. There will not be any barriers between the building and bus stop. 4. There will be a site entrance and entrance to each building at roughly the center of the site and the distance to the block edges (28th and 29th Streets) will not exceed 300-feet.

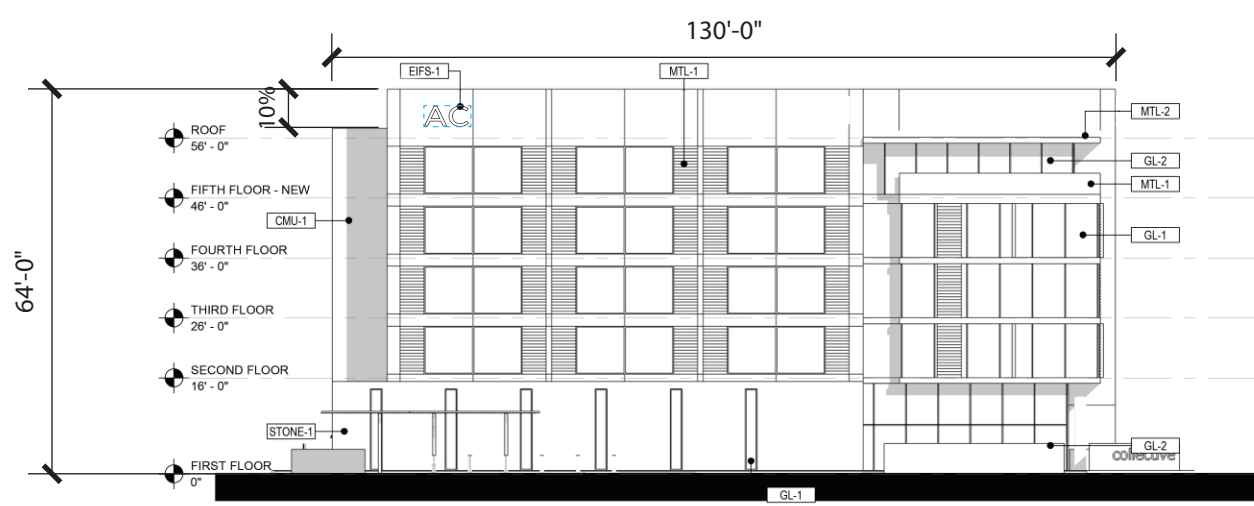
Reference Section (Camelback East Primary Core Specific Plan)	Item	Section Text (Camelback East Primary Core Specific Plan)	Demonstration of Compliance (28 th Street & Camelback Road PUD)
			<p>5. The base of the building will also be differentiated from the remainder of the building with floor to ceiling glazing that will be set back behind a ground level colonnade. There will also be a material change from the first level ceiling height and up.</p> <p>6. Pedestrian-level walls will be predominantly glazed with clear glass and therefore their opacity will not exceed thirty (30) percent of their overall areas.</p>
B. Building Design / Construction	7.1	<i>Loading and storage areas, mechanical and electrical equipment and refuse areas should face away from public streets.</i>	Loading areas have been designed to access the building from the south and do not face public streets, mechanical equipment will be roof mounted with screening from public view, and refuse areas will be screened by a minimum six (6) foot high solid wall with gates to appear integral with the building design.

M-13

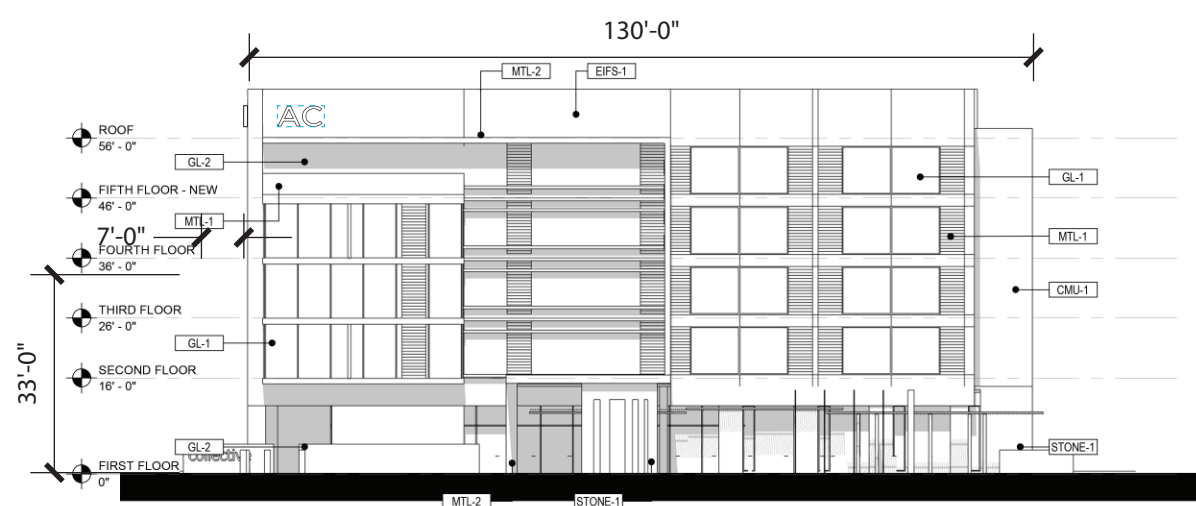


1 SITE PLAN PUD
1" = 60'-0"

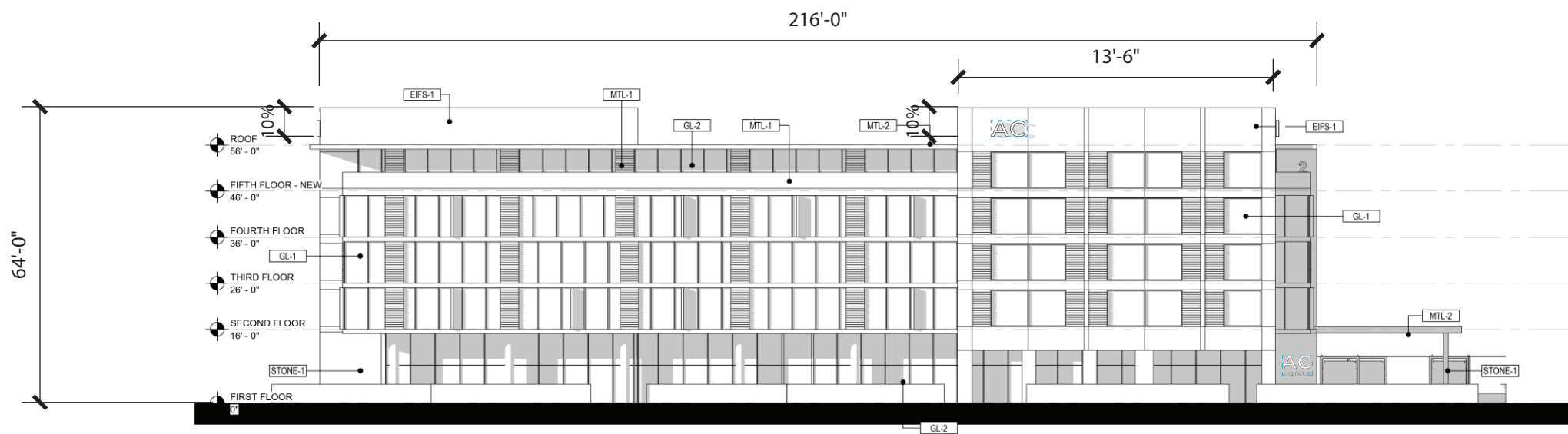




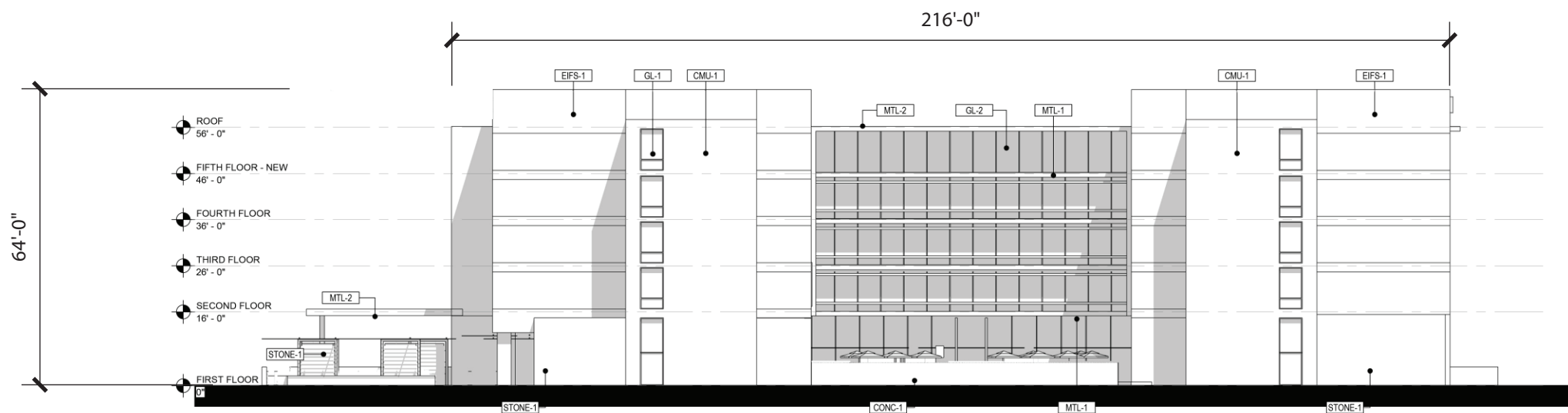
1 HOTEL EAST
1/32" = 1'-0"



4 HOTEL WEST
1/32" = 1'-0"



2 HOTEL NORTH
1/32" = 1'-0"

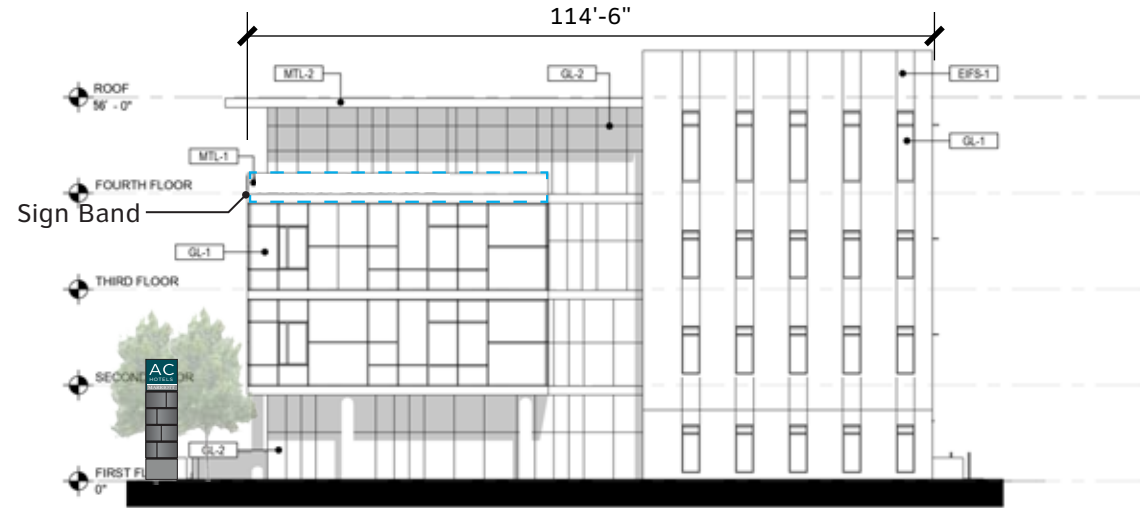


3 HOTEL SOUTH
1/32" = 1'-0"

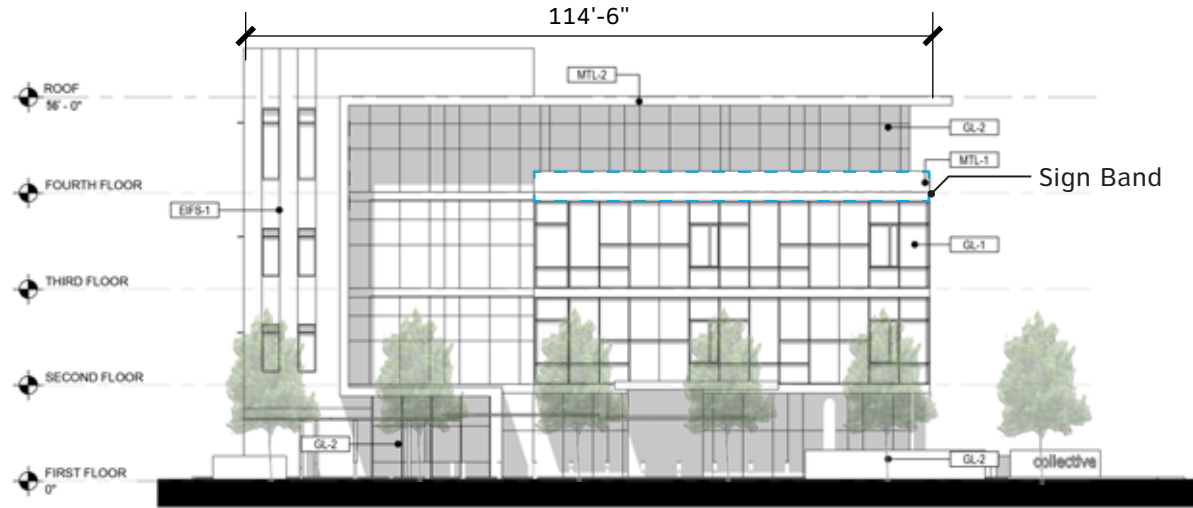
HOTEL NORTHEAST CORNER



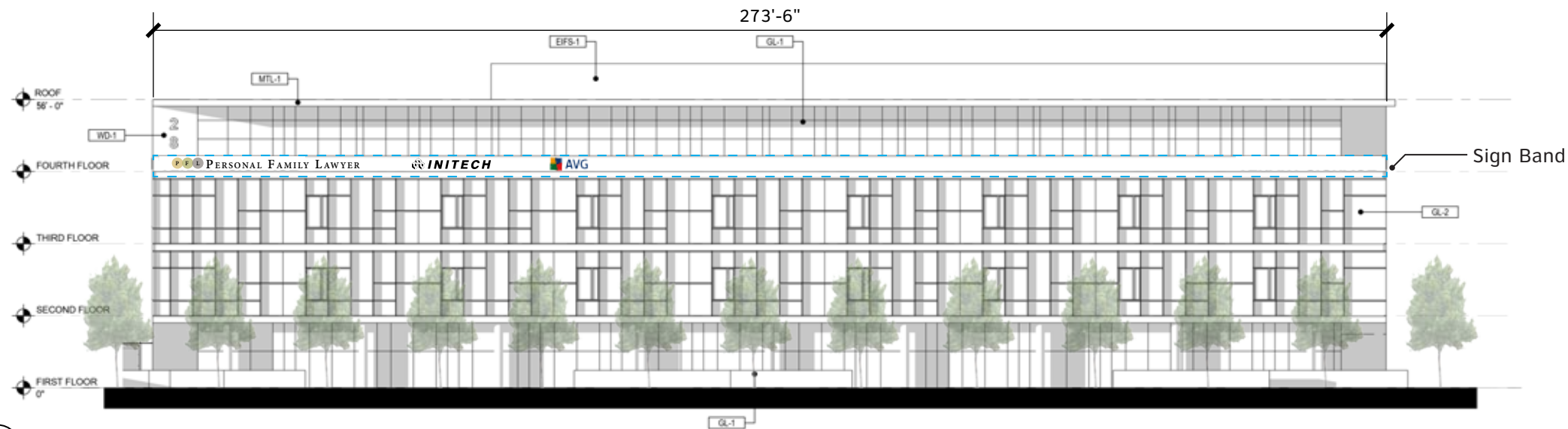
JRC DESIGN



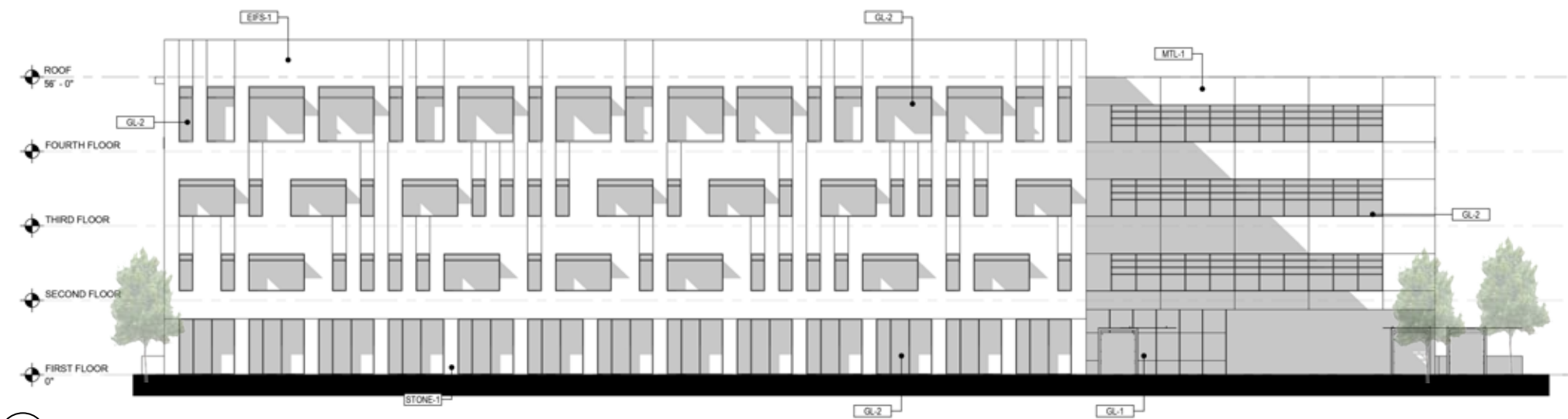
1 OFFICE WEST
1/32" = 1'-0"



2 OFFICE EAST
1/32" = 1'-0"



3 OFFICE NORTH
1/32" = 1'-0"



4 OFFICE SOUTH
1/32" = 1'-0"

MATERIAL LEGEND	
CONC-1	MASONRY
CONC-2	MASONRY
EFS-1	STAINED EIFS
GL-1	STOREFRONT GLAZING
GL-2	STOREFRONT GLAZING
MTL-1	ANNOZED ALUMINUM
MTL-2	RUSTED METAL PANEL
STONE-1	LIMESTONE
WD-1	EXTERIOR WOOD CEILING

KVA: 15-2370
PAPP: 1503638

OFFICE NORTHWEST CORNER



OFFICE NORTHWEST CORNER



JRC DESIGN

OFFICE NORTHEAST CORNER



M-14

**Traffic Impact Analysis
Submitted Under Separate Cover
to the city of Phoenix Street Transportation
Department.**

**By Southwest Traffic Engineering, LLC,
Report Dated October 28, 2015.**