

North 32nd Street Corridor

Phase 1 mapping exercise

February 27, 2014



City of Phoenix
PLANNING & DEVELOPMENT DEPARTMENT

Meeting Overview

- Summary of group efforts / history
- Mapping exercise
- Presentation of exercise results
- Future meetings
 - Paradise Valley Village Planning Committee update (3/3/14)
 - Phase 2 mapping exercise (3/27/14)



Summary of Group

- Working group formed in Spring 2012
- 17 meetings
- Three Subcommittees formed – March 2012
 - Branding and Events
 - Transportation and Street Improvements
 - Land Use Regulation
- ASU Graduate Student study – May 2012
- ULI AzTAP report – February 2013
- Corridor Visioning Draft – October 2013
- Draft Master Plan by Land Use Subcommittee – October 2013



Branding and Events ideas

- Branding and marketing of area
- Advertising (including freeway)
- Inventory of corridor amenities
- Enhanced parks and community destinations
- Trail 101 Marathon
- Street festival (art, music)
- Use Elks Lodge facility for special events
- Farmers market
- Food trucks



Transportation and Street Improvements

- Light rail and light rail access
- RAPID bus that targets riders from other areas
- Bike and bus transportation north and south on 32nd Street
- Improved road maintenance
- Bike lanes –two way with bikes on their own sidewalk
- Road diet with bike lanes and landscaped median
- Unique street lighting and signage
- Traffic study
- Paved crosswalks
- Improve street landscaping and landscaping competitions



Land Use Regulations

- Establish a design theme for corridor and major intersections
- Unique character
- Intersection nodes
- Fast track city processes for opening businesses
- Streamline city ordinances for business approval
- Proactive code enforcement
- Preservation and adaptive reuse
- Fee reduction to increase ownership and redevelopment
- Tactful signage/enhanced visibility
- Higher design standards and signage



ASU Graduate Student Study

- May 2012
- Described current conditions
- Implementation
- Broad Goals
 - Branding
 - Public art
 - Road Diet
 - Landscaping
- Future ambitions
 - Light rail
 - Recreation resort



Urban Land Institute Arizona Technical Assistance Panel (ULI AzTAP) 2/2013

- Make the NWC 32nd Street / Shea priority for redevelopment
- Redevelop commercial properties with mixed use
- Create a complete street focused on mountain preserve
- Set an identity / vision for corridor by based on community wants
- Attract youth and create opportunities for multi-generational developments, interactions and activities



North 32nd Street Corridor

ULI Arizona Technical Assistance Panel
Summary report for the City of Phoenix North 32nd Street Corridor

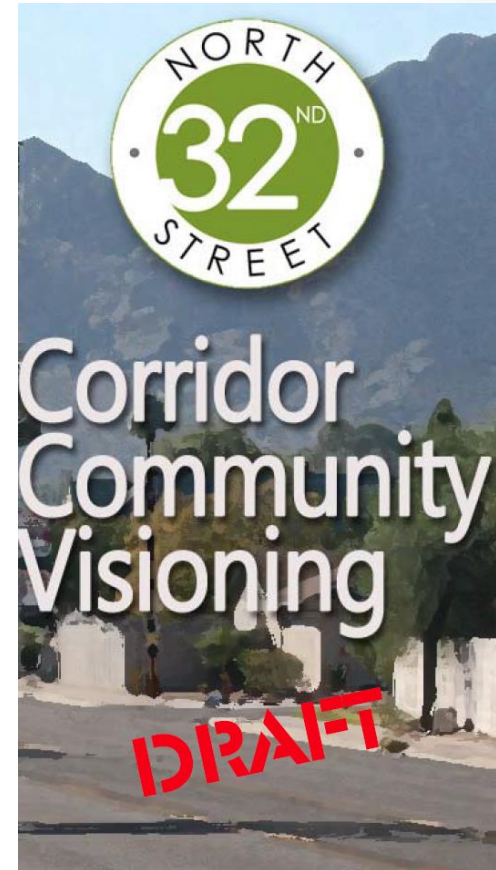
February 2013



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North 32nd Street Corridor Community Visioning Draft

- Corridor Visioning Goals and Strategies
 - Shore up the corridor for the next generation
 - Enhance the corridor to attract investment from business owners and residents
 - Make the corridor a desirable place to live, work, and play
 - Create an identity and sense of place
 - Create a corridor vision implementation tool (Area Plan)



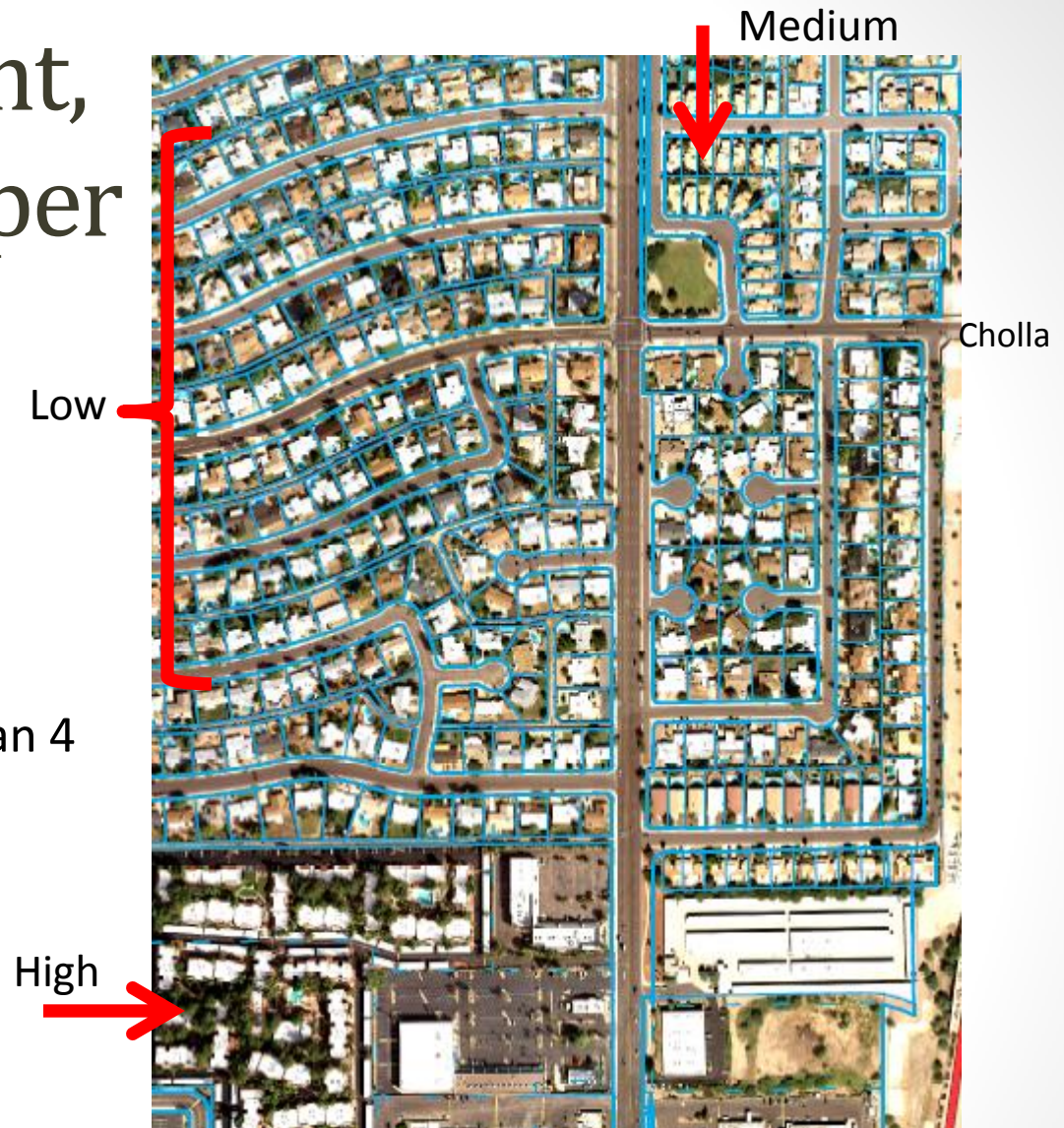
Density, Height, and Vehicles per Day

- Definitions (Section 202 of the Phoenix Zoning Ordinance)
 - Density: Number of dwelling units divided by the gross area
 - Height: The vertical distance measured from the higher of the natural grade level or finished grade level to the highest level of the roof surface of flat roofs; or to the mean height between eaves and ridge of gable, gambrel or hip roofs
- Vehicles per day: The typical number of vehicle trips per day generated by a specific use



Density, Height, and Vehicles per Day

- Needed:
 - More people
 - Density
 - Height (no more than 4 stories)



32nd Street



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Density and Height

- Cactus 42 Apartments
- Located on Cactus Road west of Target
- 200 units (studio, 1 and 2 bedroom units)
- Estimated vehicle trips per day according to Trip Generation Handbook: 1,336



Mapping Exercise

- Know what community wants, just need to identify where
- Additional traffic needed on North 32nd Street
 - Pre SR51: 58,000 vehicle per day (vpd)
 - Post SR51: 21,000 vpd
 - How do we address the 37,000 vpd difference?
- Identify the following areas:
 - Stability / Preserve (color red)
 - Repurpose / Retrofit (yellow)
 - Redevelop / Change (green)



Preserve / Stability example

- Single family residential districts
- Building/Sites with “Placemaking potential”
- Highlight the areas by filling with **RED**



Repurpose / Retrofit

- Additional landscaping
- Adaptive reuse
- 32 Shea
- Highlight the areas with the color **YELLOW**
- Keep existing buildings
- Add new buildings to vacant properties or parking lots at the same scale of surrounding neighborhood



Reconstruct frontage roads/access to improve sidewalks and landscaping.

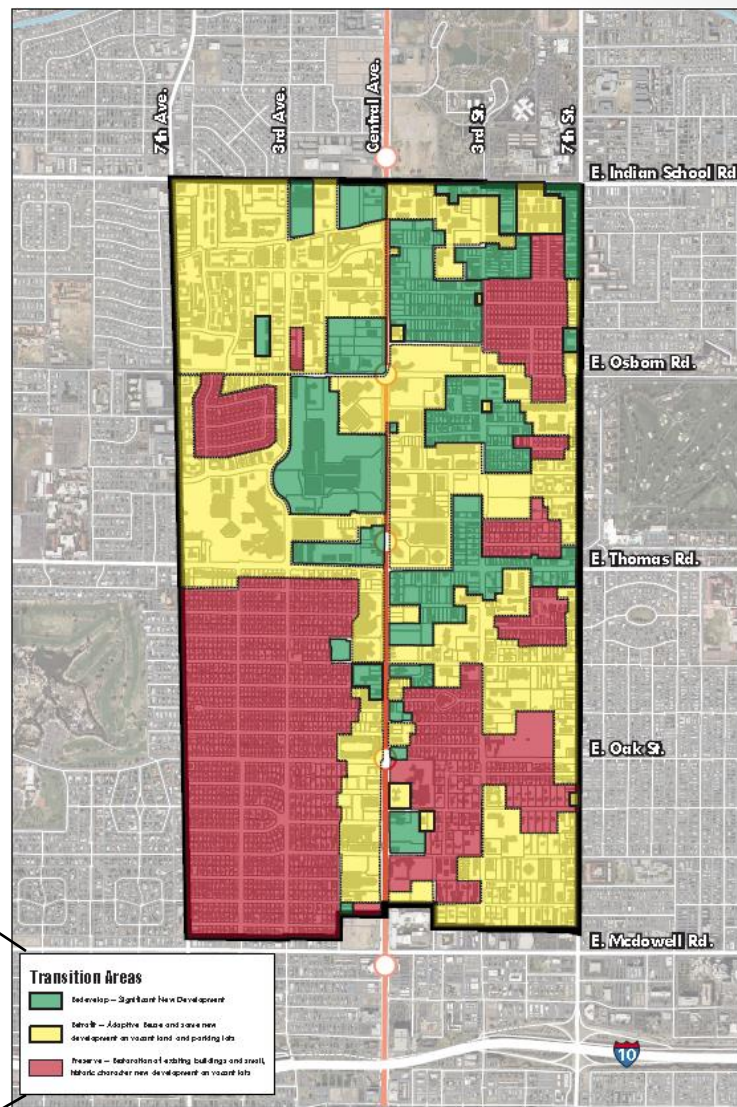


Redevelop / Change

- Completely redevelop
- Change the scale
- Increase the density and traffic per day
- Highlight these areas in **GREEN**



ReinventPHX Midtown District Map example



DRAFT

ReinventPHX
Midtown District Map

0 500 1,000 2,000 Feet

Transition Areas

-  Redevelop – Significant New Development
-  Retrofit – Adaptive Reuse and some new development on vacant land and parking lots
-  Preserve – Restoration of existing buildings and small, historic character new development on vacant lots

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Discussion of mapping results

- Spokesperson from each group present the finding
- Explain each of the three color areas
- Describe higher density developments

- This information will be used in the final policy plan for the North 32nd Street Corridor
 - Future rezoning and development decisions will utilize this plan



Transition Areas



Redevelop – Significant New Development



Retrofit – Adaptive Reuse and some new development on vacant land and parking lots



Preserve – Restoration of existing buildings and small, historic character new development on vacant lots

